

NATIONAL REGISTER OF HISTORIC PLACES INVENTORY - NOMINATION FORM

(Type all entries - complete applicable sections)

STATE: Minnesota
COUNTY: Saint Louis
FOR NPS USE ONLY
ENTRY DATE: APR 16 1975

1. NAME

COMMON:	Endion Passenger Depot
AND/OR HISTORIC:	Endion Passenger Depot

2. LOCATION

STREET AND NUMBER: 1504 South Street			
CITY OR TOWN: Duluth		CONGRESSIONAL DISTRICT: 8th	
STATE: Minnesota	CODE: 22	COUNTY: Saint Louis	CODE: 137

3. CLASSIFICATION

CATEGORY <small>(Check One)</small>	OWNERSHIP	STATUS	ACCESSIBLE TO THE PUBLIC
<input type="checkbox"/> District <input checked="" type="checkbox"/> Building <input type="checkbox"/> Site <input type="checkbox"/> Structure <input type="checkbox"/> Object	<input type="checkbox"/> Public <input checked="" type="checkbox"/> Private <input type="checkbox"/> Both	Public Acquisition: <input type="checkbox"/> In Process <input checked="" type="checkbox"/> Being Considered	<input checked="" type="checkbox"/> Occupied <input type="checkbox"/> Unoccupied <input type="checkbox"/> Preservation work in progress
PRESENT USE <small>(Check One or More as Appropriate)</small>			
<input type="checkbox"/> Agricultural <input type="checkbox"/> Commercial <input type="checkbox"/> Educational <input type="checkbox"/> Entertainment	<input type="checkbox"/> Government <input type="checkbox"/> Industrial <input type="checkbox"/> Military <input type="checkbox"/> Museum	<input type="checkbox"/> Park <input type="checkbox"/> Private Residence <input type="checkbox"/> Religious <input type="checkbox"/> Scientific	<input checked="" type="checkbox"/> Transportation <input type="checkbox"/> Other <small>(Specify)</small> <input type="checkbox"/> Comments

4. OWNER OF PROPERTY

OWNER'S NAME: Duluth, Missabe and Iron Range Railroad			
STREET AND NUMBER: 210 Missabe Building			
CITY OR TOWN: Duluth		STATE: Minnesota	CODE: 22

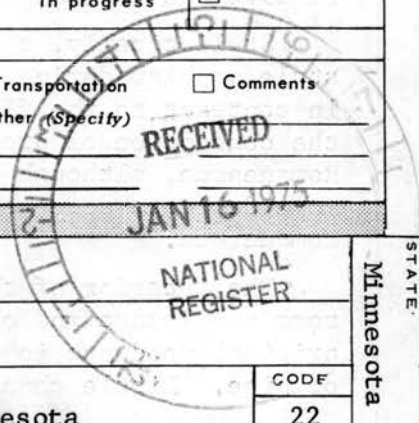
5. LOCATION OF LEGAL DESCRIPTION

COURTHOUSE, REGISTRY OF DEEDS, ETC.: Registry of Deeds - Saint Louis County Courthouse			
STREET AND NUMBER: 5th Avenue West at First Street			
CITY OR TOWN: Duluth		STATE: Minnesota	CODE: 22

6. REPRESENTATION IN EXISTING SURVEYS

TITLE OF SURVEY: Statewide Historic Sites Survey			
DATE OF SURVEY: 1974 <input type="checkbox"/> Federal <input checked="" type="checkbox"/> State <input type="checkbox"/> County <input type="checkbox"/> Local			
DEPOSITORY FOR SURVEY RECORDS: Minnesota Historical Society			
STREET AND NUMBER: Building 25, Fort Snelling			
CITY OR TOWN: Saint Paul		STATE: Minnesota	CODE: 22

SEE INSTRUCTIONS



STATE: Minnesota
COUNTY: St. Louis
ENTRY NUMBER: APR 16 1975
DATE: APR 16 1975

FOR NPS USE ONLY

7. DESCRIPTION

CONDITION	(Check One)					
	<input type="checkbox"/> Excellent	<input checked="" type="checkbox"/> Good	<input type="checkbox"/> Fair	<input type="checkbox"/> Deteriorated	<input type="checkbox"/> Ruins	<input type="checkbox"/> Unexposed
	(Check One)			(Check One)		
	<input checked="" type="checkbox"/> Altered	<input type="checkbox"/> Unaltered	<input type="checkbox"/> Moved	<input checked="" type="checkbox"/> Original Site		

DESCRIBE THE PRESENT AND ORIGINAL (if known) PHYSICAL APPEARANCE

The Endion Passenger Depot is located on South Street between Fifteenth and Sixteenth Avenues East in Duluth (Lots 1,2,3, Block 14, Endion Division). The Depot is only separated from the shore of Lake Superior by four tracks or a distance of approximately one hundred feet. It rests upon a shelf of solid rock which lies only six feet below grade.

The Depot is a small building, measuring only seventy feet by twenty-four feet. The highest point at the central gable rises nearly thirty-seven feet above ground level. The foundation and stone trim are of locally quarried Kettle River sandstone of buff color which contrasts the pressed red brick body of the building. All coping and door and window surrounds are of Kettle River sandstone. Essentially the building forms a cruciform plan at the roof due to the intersection of gables although it is basically rectangular in floor plan. A central stone bay which houses the station master's office projects on the track-side of the building. Originally all had been roofed in slate, but this roofing has recently been replaced with brown asphalt shingle roofing. Original metal ridges and caps have not been retained. A frame freight (LCL) shed formerly attached to the west facade was removed during the summer of 1974 due to its dilapidated condition.

Stylistically, the Depot may be classified as a mode of the Richardsonian style popular during the 1880's and 1890's. The rusticated stonework in contrast to the flat surface of the brick, stone window surrounds and the conception of projecting gables capped with stone is essentially Romanesque, although the smallness of scale contributes to the lighter, more delicate appearance rather than the heaviness of the earlier Romanesque.

The interior of the Depot consists of two areas: a passenger waiting room and lounge and office for the station-master. A great degree of original woodwork is retained although painted. All floors are of maple or pine. Little change, if any, is apparent in the plan.

Basically the building continues to serve the functions of the DM & IR but passenger service has been discontinued. The owners continue to maintain the building, however, deterioration is evident and the future uncertain. Mention has been made by various civic groups in Duluth, that the building will be considered for adaptive use if and when it ceases to serve as a depot.

SEE INSTRUCTIONS



6. SIGNIFICANCE

PERIOD (Check One or More as Appropriate)

- | | | | |
|--|---------------------------------------|--|---------------------------------------|
| <input type="checkbox"/> Pre-Columbian | <input type="checkbox"/> 16th Century | <input type="checkbox"/> 18th Century | <input type="checkbox"/> 20th Century |
| <input type="checkbox"/> 15th Century | <input type="checkbox"/> 17th Century | <input checked="" type="checkbox"/> 19th Century | |

SPECIFIC DATE(S) (If Applicable and Known) **1899**

AREAS OF SIGNIFICANCE (Check One or More as Appropriate)

- | | | | |
|--|---|--|--|
| <input type="checkbox"/> Aboriginal | <input type="checkbox"/> Education | <input type="checkbox"/> Political | <input type="checkbox"/> Urban Planning |
| <input type="checkbox"/> Prehistoric | <input type="checkbox"/> Engineering | <input type="checkbox"/> Religion/Philosophy | <input type="checkbox"/> Other (Specify) |
| <input type="checkbox"/> Historic | <input type="checkbox"/> Industry | <input type="checkbox"/> Science | _____ |
| <input type="checkbox"/> Agriculture | <input type="checkbox"/> Invention | <input type="checkbox"/> Sculpture | _____ |
| <input checked="" type="checkbox"/> Architecture | <input type="checkbox"/> Landscape Architecture | <input type="checkbox"/> Social/Humanitarian | _____ |
| <input type="checkbox"/> Art | <input type="checkbox"/> Literature | <input type="checkbox"/> Theater | _____ |
| <input type="checkbox"/> Commerce | <input type="checkbox"/> Military | <input checked="" type="checkbox"/> Transportation | _____ |
| <input type="checkbox"/> Communications | <input type="checkbox"/> Music | | _____ |
| <input type="checkbox"/> Conservation | | | _____ |

STATEMENT OF SIGNIFICANCE

Beginning in the 1880's the Duluth, Missabe and Iron Range Railroad provided suburban freight and passenger service along the lake shore from Duluth to Lester Park to the west. The first stop after First Avenue East in Duluth was the community of Endion (now part of the city of Duluth). The railroad commissioned the newly formed firm of Tenbusch and Hill (Gearhard A. Tenbusch and I. Vernon Hill) of Duluth to design a new passenger depot for Endion in 1899.

The depot of Kettle River sandstone and pressed brick was executed in a variant on the Romanesque style. Total cost was approximately \$10,000. The basic concept of projecting gables which crossed in a transept fashion had been used by other designers of the late nineteenth century, but was developed by Hill into a highly aggressive and personal style which marked his major works in Duluth between 1901 and 1903. The design of the depot was the first of these major works in the development of Hill's mature architectural career.

The Endion Depot is significant as the last remaining functioning small passenger depot in Duluth. Its jewel-like character of setting near the shore of Lake Superior highlights the fine use of stone and brick architectural detailing and craftsmanship. Further, it stands as an excellent example of I. Vernon Hill's early design and as a building which contributed to the development of Hill's distinctive impact on Duluth architecture in the early twentieth century.



SEE INSTRUCTIONS

9. MAJOR BIBLIOGRAPHICAL REFERENCES

Records of Saint Louis County Historical Society
 Duluth Historic Buildings Report - City Planning Department, 1974
 Records of Duluth Department of Building Inspection & Permit

10. GEOGRAPHICAL DATA

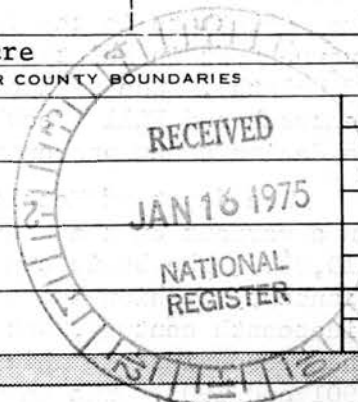
LATITUDE AND LONGITUDE COORDINATES DEFINING A RECTANGLE LOCATING THE PROPERTY			O R	LATITUDE AND LONGITUDE COORDINATES DEFINING THE CENTER POINT OF A PROPERTY OF LESS THAN TEN ACRES		
CORNER	LATITUDE	LONGITUDE		LATITUDE	LONGITUDE	
	Degrees Minutes Seconds	Degrees Minutes Seconds		Degrees Minutes Seconds	Degrees Minutes Seconds	
NW	0 . "	0 . "		46° 47' 59"	92° 04' 36"	
NE	0 . "	0 . "				
SE	0 . "	0 . "				
SW	0 . "	0 . "				

15/570460
 5183100
 CD

APPROXIMATE ACREAGE OF NOMINATED PROPERTY: **less than 1 acre**

LIST ALL STATES AND COUNTIES FOR PROPERTIES OVERLAPPING STATE OR COUNTY BOUNDARIES

STATE:	CODE	COUNTY	CODE
STATE:	CODE	COUNTY:	CODE
STATE:	CODE	COUNTY:	CODE
STATE:	CODE	COUNTY:	CODE



SEE INSTRUCTIONS

11. FORM PREPARED BY

NAME AND TITLE:
Charles W. Nelson, Historic Sites Survey and Planning

ORGANIZATION: **Minnesota Historical Society** DATE: **3 December 1974**

STREET AND NUMBER:
Building 25, Fort Snelling

CITY OR TOWN: **Saint Paul** STATE: **Minnesota** CODE: **22**

12. STATE LIAISON OFFICER CERTIFICATION

NATIONAL REGISTER VERIFICATION

As the designated State Liaison Officer for the National Historic Preservation Act of 1966 (Public Law 89-665), I hereby nominate this property for inclusion in the National Register and certify that it has been evaluated according to the criteria and procedures set forth by the National Park Service. The recommended level of significance of this nomination is:

National State Local

Name Russell W. Fridley
Russell W. Fridley,
 Title State Historic Preservation Officer
 Date 12/30/74

I hereby certify that this property is included in the National Register.

AB Mortensen
 Director, Office of Archeology and Historic Preservation

Date 4/16/75

ATTEST:
W. J. ...
 Keeper of The National Register

Date **APR 8 1975**



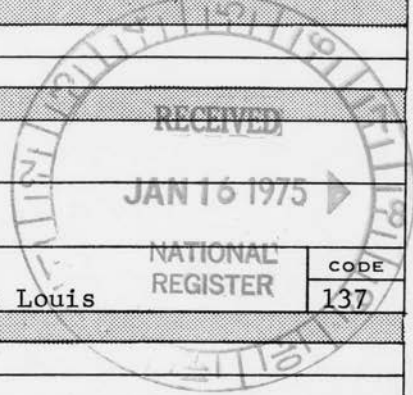
**NATIONAL REGISTER OF HISTORIC PLACES
PROPERTY PHOTOGRAPH FORM**

(Type all entries - attach to or enclose with photograph)

STATE Minnesota	
COUNTY Saint Louis	
FOR NPS USE ONLY	
ENTRY NUMBER	DATE
	APR 16 1975

SEE INSTRUCTIONS

1. NAME			
COMMON:	Endion Passenger Depot		
AND/OR HISTORIC:	Endion Passenger Depot		
2. LOCATION			
STREET AND NUMBER: 1504 South Street			
CITY OR TOWN: Duluth			
STATE:	CODE	COUNTY:	CODE
Minnesota	22	Saint Louis	137
3. PHOTO REFERENCE			
PHOTO CREDIT:	Charles W. Nelson		
DATE OF PHOTO:	November 1974		
NEGATIVE FILED AT: Minnesota Historical Society, 690 Cedar Street, St. Paul, Minnesota 55101			
4. IDENTIFICATION			
DESCRIBE VIEW, DIRECTION, ETC. Track side perspective view from Southwest			
#1082			





**NATIONAL REGISTER OF HISTORIC PLACES
PROPERTY PHOTOGRAPH FORM**

(Type all entries - attach to or enclose with photograph)

STATE Minnesota	
COUNTY Saint Louis	
FOR NPS USE ONLY	
ENTRY NUMBER	DATE
APR 16 1975	

SEE INSTRUCTIONS

1. NAME

COMMON: Endion Passenger Depot

AND/OR HISTORIC: Endion Passenger Depot

2. LOCATION

STREET AND NUMBER:
1504 South Street

CITY OR TOWN:
Duluth

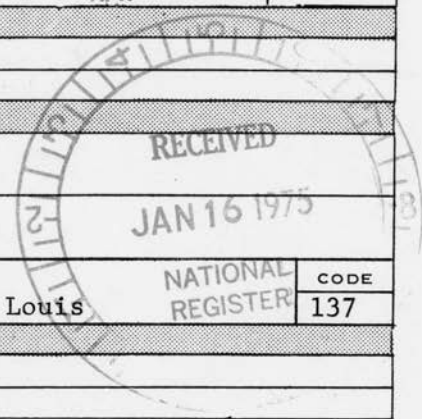
STATE:
Minnesota

CODE
22

COUNTY:
Saint Louis

NATIONAL REGISTER

CODE
137



3. PHOTO REFERENCE

PHOTO CREDIT: Charles W. Nelson

DATE OF PHOTO: November 1974

NEGATIVE FILED AT:
Minnesota Historical Society, 690 Cedar Street, St. Paul, Minnesota 55101

4. IDENTIFICATION

DESCRIBE VIEW, DIRECTION, ETC.

Rear perspective view from Northwest

#282

**NATIONAL REGISTER OF HISTORIC PLACES
PROPERTY MAP FORM**

(Type all entries - attach to or enclose with map)

STATE Minnesota	
COUNTY Saint Louis	
FOR NPS USE ONLY	
ENTRY NUMBER APR 16 1975	DATE

SEE INSTRUCTIONS

1. NAME

COMMON: Endion Passenger Depot
AND/OR HISTORIC: Endion Passenger Depot

2. LOCATION

STREET AND NUMBER:
1504 South Street

CITY OR TOWN:
Duluth

STATE: Minnesota	CODE 22	COUNTY: Saint Louis	CODE 137
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3. MAP REFERENCE

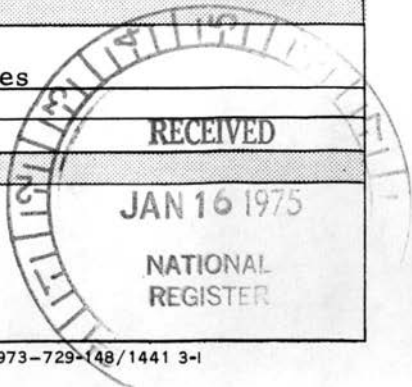
SOURCE:
USGS - Duluth Quadrangle - Minnesota, 7.5 Minute Series

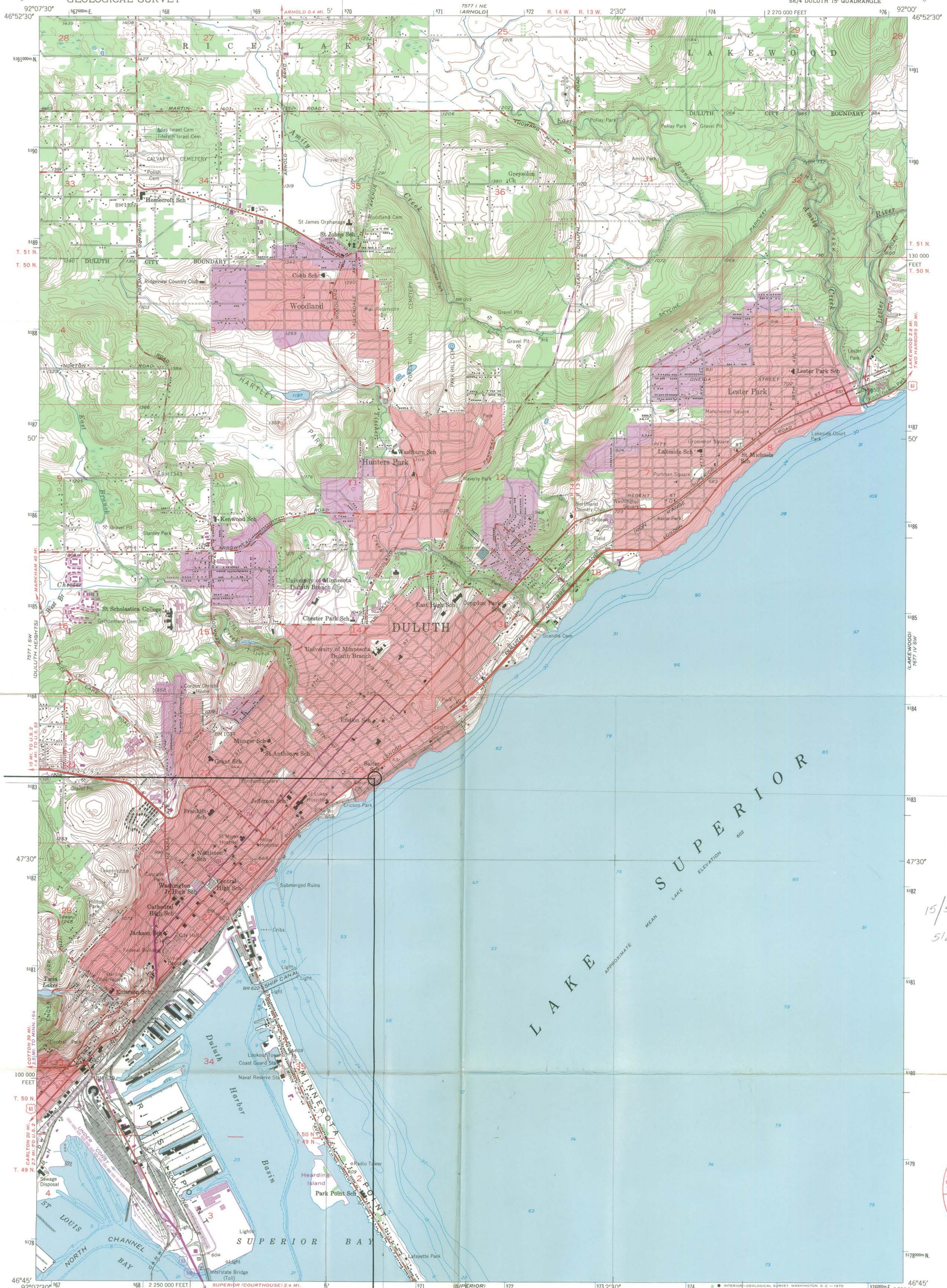
SCALE:
1:24000

DATE:
1953 photorevised 1969

4. REQUIREMENTS

- TO BE INCLUDED ON ALL MAPS
1. Property boundaries where required.
 2. North arrow.
 3. Latitude and longitude reference.





Mapped, edited, and published by the Geological Survey

Control by USGS, US&GS, and U. S. Lake Survey

Topography from aerial photographs by multiplex methods

Aerial photographs taken 1952. Field check 1953

Hydrography from U. S. Lake Survey Charts 96 (1:120 000) and 966 (1:15 000)

Polyconic projection. 1927 North American datum

10,000-foot grid based on Minnesota coordinate system, north zone

1000-meter Universal Transverse Mercator grid ticks, zone 15, shown in blue

Red tint indicates areas in which only landmark buildings are shown

Revisions shown in purple compiled in cooperation with the State of Minnesota from aerial photographs taken 1969

This information not field checked

Purple tint indicates extension of urban areas



CONTOUR INTERVAL 10 FEET
DATUM IS MEAN SEA LEVEL
DEPTH CURVES AND SOUNDINGS IN FEET—DATUM IS 601 FEET



ROAD CLASSIFICATION

Heavy-duty	4 LANE 16 LANE	Light-duty	2 LANE 16 LANE
Medium-duty	4 LANE 6 LANE	Unimproved dirt	-----
U. S. Route	□	State Route	○
		Interstate Route	⊖

DULUTH, MINN.
SE/4 DULUTH 15' QUADRANGLE
N4645-W9200/7.5

1953
PHOTOREVISED 1969

AMS 7577 I SE—SERIES V872

THIS MAP COMPLIES WITH NATIONAL MAP ACCURACY STANDARDS
FOR SALE BY U. S. GEOLOGICAL SURVEY, DENVER, COLORADO 80225 OR WASHINGTON, D. C. 20242
A FOLDER DESCRIBING TOPOGRAPHIC MAPS AND SYMBOLS IS AVAILABLE ON REQUEST

National Register of Historic Places

Note to the record

Additional Documentation: 1989

75002085

NATIONAL REGISTER OF HISTORIC PLACES
EVALUATION/RETURN SHEET

Substantive Review

Endion Passenger Depot (Proposed Move)
St. Louis County
MINNESOTA

Working No. MAR 10 1986
Fed. Reg. Date: 2/3/87
Date Due: 4/17/86 ~~4/24/86~~
Action: ACCEPT 4-17-86
 RETURN
 REJECT

Proposed move approved

- resubmission
- nomination by person or local government
- owner objection
- appeal

Federal Agency: _____

Substantive Review: sample request appeal NR decision

Reviewer's comments:

Recom./Criteria Approve move
Reviewer Gibbons
Discipline Historian
Date 4/17/86
_____ see continuation sheet

Nomination returned for: _____ technical corrections cited below
_____ substantive reasons discussed below

1. Name

2. Location

3. Classification

Category	Ownership Public Acquisition	Status Accessible	Present Use

4. Owner of Property

5. Location of Legal Description

6. Representation in Existing Surveys

Has this property been determined eligible? yes no

7. Description

Condition

<input type="checkbox"/> excellent	<input type="checkbox"/> deteriorated	Check one	Check one
<input type="checkbox"/> good	<input type="checkbox"/> ruins	<input type="checkbox"/> unaltered	<input type="checkbox"/> original site
<input type="checkbox"/> fair	<input type="checkbox"/> unexposed	<input type="checkbox"/> altered	<input type="checkbox"/> moved date _____

Describe the present and original (if known) physical appearance

- summary paragraph
- completeness
- clarity
- alterations/integrity
- dates
- boundary selection

8. Significance

Period _____ Areas of Significance—Check and justify below

Specific dates _____ Builder/Architect _____

Statement of Significance (*in one paragraph*)

- summary paragraph
- completeness
- clarity
- applicable criteria
- justification of areas checked
- relating significance to the resource
- context
- relationship of integrity to significance
- justification of exception
- other

9. Major Bibliographical References

10. Geographical Data

Acreage of nominated property _____

Quadrangle name _____

UTM References _____

Verbal boundary description and justification

11. Form Prepared By

12. State Historic Preservation Officer Certification

The evaluated significance of this property within the state is:

____ national ____ state ____ local

State Historic Preservation Officer signature

title _____ date _____

13. Other

- Maps
- Photographs
- Other

Questions concerning this nomination may be directed to _____

Signed _____ Date _____ Phone: _____

United States Department of the Interior
National Park Service

MAR 21 1989

National Register of Historic Places Continuation Sheet

Section number _____ Page _____

Note: These changes apply to
Endion Passenger Depot in St. Louis
County, Minnesota.

REFERENCE NUMBER: 75002088

STATE: MINNESOTA

COUNTY: St. Louis

RESOURCE NAME (HISTORIC): Duluth Missabe and Iron Range
Depot (Endion)

CITY:

VICINITY OF:

ADDRESS: 100 Lake Place

CERTIFICATION DATE:

REMOVED DATE:

COMMENTS:

Nina M. Archabal

Nina M. Archabal
State Historic Preservation Officer

JUN 17 1988

Date

United States Department of the Interior
National Park Service

MAR 21 1989

National Register of Historic Places Continuation Sheet

Section number _____ Page _____

Endion Passenger Depot St. Louis County, MN

Name Change Approval
to Duluth Missabe and Iron Range
Depot (Endion)

Keeper Beth Boland 5/4/89

UNITED STATES DEPARTMENT OF THE INTERIOR
NATIONAL PARK SERVICE

NATIONAL REGISTER OF HISTORIC PLACES
EVALUATION/RETURN SHEET

REQUESTED ACTION: ADDITIONAL DOCUMENTATION

PROPERTY Endion Passenger Depot
NAME:

MULTIPLE
NAME:

STATE & COUNTY: MINNESOTA, St. Louis

DATE RECEIVED: 3/21/89 DATE OF PENDING LIST:
DATE OF 16TH DAY: DATE OF 45TH DAY: 5/05/89
DATE OF WEEKLY LIST:

REFERENCE NUMBER: 75002088

NOMINATOR: STATE

REASONS FOR REVIEW:

APPEAL: N DATA PROBLEM: N LANDSCAPE: N LESS THAN 50 YEARS: N
OTHER: Y PDIL: N PERIOD: N PROGRAM UNAPPROVED: N
REQUEST: N SAMPLE: N SLR DRAFT: N NATIONAL: N

COMMENT WAIVER: N

ACCEPT RETURN REJECT 5/4/89 DATE

ABSTRACT/SUMMARY COMMENTS:

Name change is OK - the Duluth Missabe & Iron Range RR is the historic RR mentioned in the nomination. However - there is another problem. The Nat'l Reg. approved the move of this depot in 1986 and requested new photos, map & address after the completion of the move. The State has never submitted this documentation. We need to write a letter.

RECOM./CRITERIA Accept name change
REVIEWER Boland
DISCIPLINE Historian
DATE 5/4/89

DOCUMENTATION see attached comments Y/N see attached SLR Y/N

CLASSIFICATION

count resource type

STATE/FEDERAL AGENCY CERTIFICATION

FUNCTION

historic current

DESCRIPTION

architectural classification
 materials
 descriptive text

SIGNIFICANCE

Period Areas of Significance--Check and justify below

Specific dates Builder/Architect
Statement of Significance (in one paragraph)

summary paragraph
 completeness
 clarity
 applicable criteria
 justification of areas checked
 relating significance to the resource
 context
 relationship of integrity to significance
 justification of exception
 other

BIBLIOGRAPHY

GEOGRAPHICAL DATA

acreage verbal boundary description
 UTMs boundary justification

ACCOMPANYING DOCUMENTATION/PRESENTATION

sketch maps USGS maps photographs presentation

OTHER COMMENTS

Questions concerning this nomination may be directed to

_____ Phone _____

Signed _____ Date _____

Property *Emdion Passenger Depot*

St. Louis

State *Missouri*

Working Number *1.16.75.68*

TECHNICAL

Photos 2
Maps 1

CONTROL

cm
OK 1.20.75

HISTORIAN

Accept
E. Smith
1-24-75

ARCHITECTURAL HISTORIAN

ACCEPT
LEBOVICH
1-24-75

ARCHEOLOGIST

OTHER

HAER

Inventory _____
Review _____

REVIEW UNIT CHIEF

Accept
Colz
2-11-75

BRANCH CHIEF

accept
Hung
4.4.75

KEEPER

[Signature]
APR 8 1975

National Register Write-up _____
Federal Register Entry *6-3-75*

Send-back _____
Re-submit _____

Entered *APR 16 1975*

INT 106-74

National Register of Historic Places

Note to the record

Additional Documentation: 2013

UNITED STATES DEPARTMENT OF THE INTERIOR
NATIONAL PARK SERVICE

NATIONAL REGISTER OF HISTORIC PLACES
EVALUATION/RETURN SHEET

REQUESTED ACTION: ADDITIONAL DOCUMENTATION

PROPERTY NAME: Duluth Missabe and Iron Range Depot (Endion)

MULTIPLE NAME:

STATE & COUNTY: MINNESOTA, St. Louis

DATE RECEIVED: 11/01/13 DATE OF PENDING LIST:
DATE OF 16TH DAY: DATE OF 45TH DAY: 12/18/13
DATE OF WEEKLY LIST:

REFERENCE NUMBER: 75002088

NOMINATOR: STATE

REASONS FOR REVIEW:

APPEAL: N DATA PROBLEM: N LANDSCAPE: N LESS THAN 50 YEARS: N
OTHER: N PDIL: N PERIOD: N PROGRAM UNAPPROVED: N
REQUEST: N SAMPLE: N SLR DRAFT: N NATIONAL: N

COMMENT WAIVER: N

___ACCEPT ___RETURN ___REJECT _____DATE

ABSTRACT/SUMMARY COMMENTS:

Additional Documentation Approved

RECOM./CRITERIA Accept
REVIEWER Edson Beall DISCIPLINE History
TELEPHONE _____ DATE 12-18-13

DOCUMENTATION see attached comments Y/N see attached SLR Y/N

If a nomination is returned to the nominating authority, the nomination is no longer under consideration by the NPS.

**United States Department of the Interior
National Park Service**

**National Register of Historic Places
Continuation Sheet**

Duluth Missabe and Iron Range Depot

(Endion Depot)

Name of Property

St. Louis County, MN

County and State

Name of multiple listing (if applicable)

Section number 1 Page 1

Reference Number: 75002088

State: Minnesota

County: St. Louis

Resource Name (Historic): Duluth Missabe and Iron Range Depot (Endion Depot)

Location: 200 Lake Place Drive

Comments: The Duluth Missabe and Iron Range Depot (Endion Depot) was moved to its present location as part of a Memorandum of Agreement with the Advisory Council on Historic Preservation, the Minnesota State Historic Preservation Office and the Federal Highway Administration, dated May 18, 1977. Pursuant to this agreement, the National Register documentation now includes the verbal boundary description, boundary justification, and maps for the new location, as well as photos of the property at its new location. The resource name is the correct historic name.

Barbara Howard

Barbara Mitchell Howard
Deputy State Historic Preservation Officer

October 11, 2013

Date

United States Department of the Interior
National Park Service

National Register of Historic Places
Continuation Sheet

Section number 2 Page 1

Duluth Missabe and Iron Range Depot
(Endion Depot)

Name of Property
St. Louis County, MN
County and State

Name of multiple listing (if applicable)

Location

Street & number: 200 Lake Place Drive

City or town: Duluth State: MN County: St. Louis County

Comments: When first moved to its new location, the depot address was given as 100 Lake Place Drive.
Using the St. Louis County Land Explorer map, 200 Lake Place Drive is determined to be the correct address.

United States Department of the Interior
National Park Service

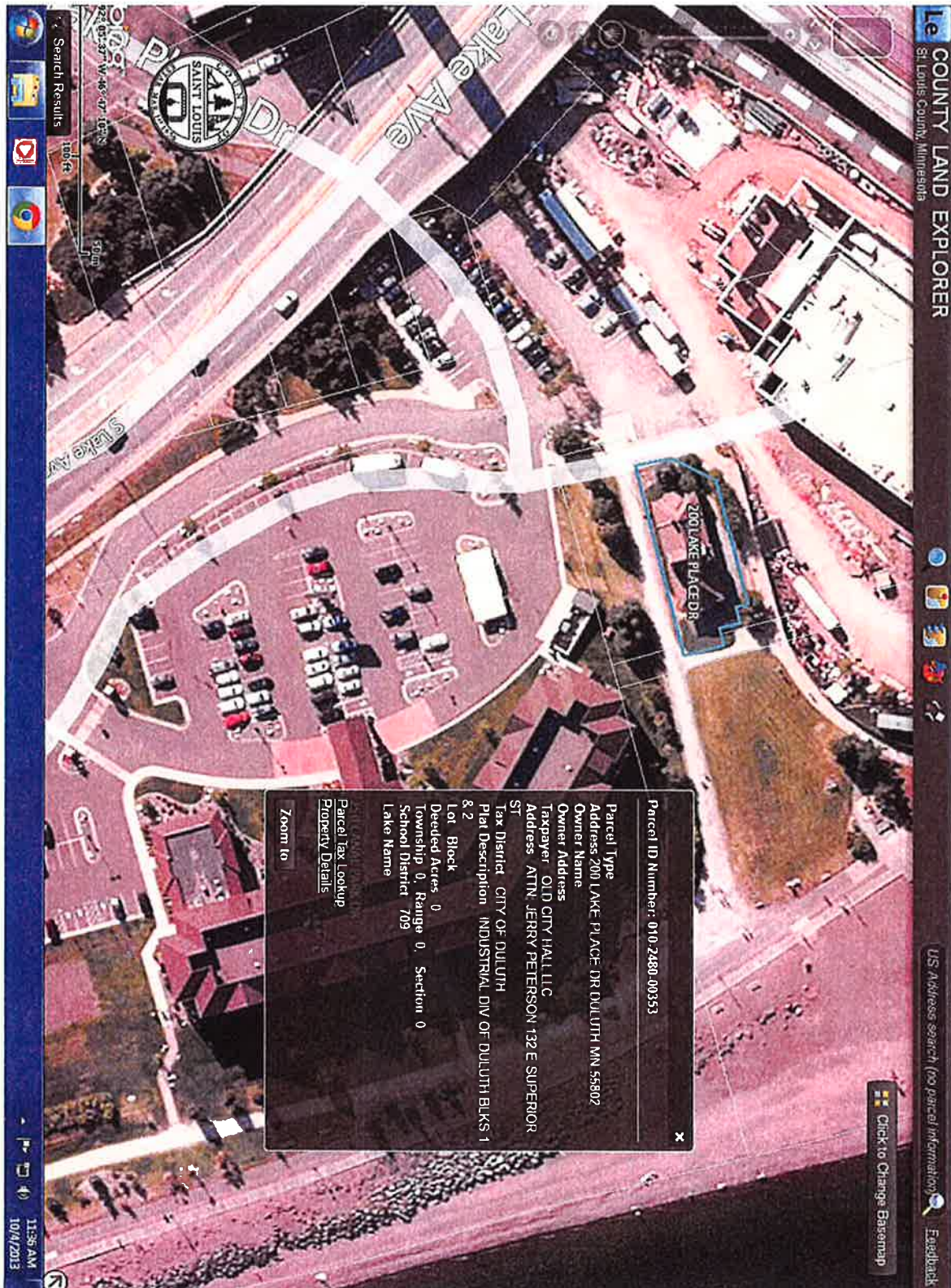
National Register of Historic Places
Continuation Sheet

Section number 2 Page 2

Duluth Missabe and Iron Range Depot
(Endion Depot)

Name of Property
St. Louis County, MN
County and State

Name of multiple listing (if applicable)



Duluth, Missabe and Iron Range Depot

gis.stlouiscountymn.gov/planningflexviewers/County_Explorer/

United States Department of the Interior
National Park Service

National Register of Historic Places
Continuation Sheet

Duluth Missabe and Iron Range Depot
(Endion Depot)

Name of Property
St. Louis County, MN
County and State

Name of multiple listing (if applicable)

Section number 10 Page 1

Latitude/Longitude Coordinates

(enter coordinates to 6 decimal places)

1. Latitude: 46.786313 Longitude: -92.095197

Verbal Boundary Description

That part of Lots 30 and 31 Block 1 described as follows: Commencing at the SW corner of Lot 30, thence on an assumed bearing of N15 DEG 55' 39"W along SW line of Block 1 25.63 feet; thence N15 DEG 55' 39"W along SW line of Block 1 50 feet; thence N 75 DEG 01' 17"E 42.65 feet to point of beginning; thence N75 DEG 01' 17"E 85.07 feet; thence S14 DEG 58' 43"E 8.47 feet; thence N75 DEG 01' 17"E 10.71 feet; thence S 14 DEG 58' 43"E 13.88 feet; thence N75 DEG 01' 17"E 11.52 feet; thence S14 DEG 58' 43"E 27.65 feet; thence S 75 DEG 01' 17"W 130.11 feet; thence N10 DEG 58' 48"W 24.77 feet; thence N24 DEG 50' 11"E 32.92 Feet to point of beginning and there terminating.

Boundary Justification

The station at its new location is 14 blocks from its original, historic location. It retains its north-facing orientation and has the same setting and general environment as the original location. (See Additional Documentation, page 3, for Carol D. Shull letter dated 2 May 1986.)

United States Department of the Interior
National Park Service

National Register of Historic Places
Continuation Sheet

Duluth Missabe and Iron Range Depot
(Endion Depot)

Name of Property

St. Louis County, MN

County and State

Name of multiple listing (if applicable)

Section number Additional Documentation Page 1

Photographs:

Name of Property: Duluth Missabe and Iron Range Depot (Endion Depot)

City or Vicinity: Duluth

County: St. Louis **State:** Minnesota

Photographer: Denis Gardner

Date Photographed: September 2013

Location of Original Digital Files: Minnesota Historical Society, 345 Kellogg Blvd., St. Paul MN 55102

Description of Photograph(s) and number:

1 of 4 .

North façade, camera facing south.

2 of 4 .

West side, camera facing east.

3 of 4 .

South rear, camera facing northeast.

4 of 4 .

East side, camera facing west.

**United States Department of the Interior
National Park Service**

**National Register of Historic Places
Continuation Sheet**

Section number Additional Documentation

Page 2

Duluth Missabe and Iron Range Depot
(Endion)

Name of Property
St. Louis County, MN
County and State

Name of multiple listing (if applicable)

SKETCH MAP



**United States Department of the Interior
National Park Service**

**National Register of Historic Places
Continuation Sheet**

Duluth Missabe and Iron Range Depot
(Endion Depot)

Name of Property
St. Louis County, MN

County and State

Name of multiple listing (if applicable)

Section number Additional Documentation Page 3



United States Department of the Interior

NATIONAL PARK SERVICE

P.O. BOX 37127

WASHINGTON, D.C. 20013-7127

IN REPLY REFER TO:

1132 (413)

MAY 2 1986

Mr. Russell W. Fridley
Director, Minnesota Historical Society
690 Cedar Street
St. Paul, Minnesota 55101

Dear Mr. Fridley:

Thank you for your request that the National Register approve the proposed move of the Endion Passenger Depot, St. Louis County, Minnesota, listed on April 16, 1975.

After reviewing the documentation, the National Register approves the move of the Endion Passenger Depot to its proposed new site within the same part of town, approximately 14 blocks from its historic location; therefore the depot will remain listed during its move. The station will have a similar setting, general environment, and north/south orientation as on its historic site, although, unfortunately, it will not face Lake Superior as it had originally. When the move has been completed, please notify us of the date of the move, and provide photographs of the depot on its new site, along revised maps and geographical information, on signed National Register continuation sheets in accordance with 36 CFR 60.14(b)(3).

We appreciate the opportunity to review this documentation.

Sincerely,

Carol D. Shull
Chief of Registration
National Register of Historic Places
Interagency Resources Division



lake place drive, duluth mn

Sign in 1 of 5



Directions

My places

Nearby

Road

Aerial

Traffic



Fullscreen

Print

Share



Duluth, Missabe and Iron Range Depot

Lake Place Dr, Duluth, MN 55802

46.786313 -92.095197

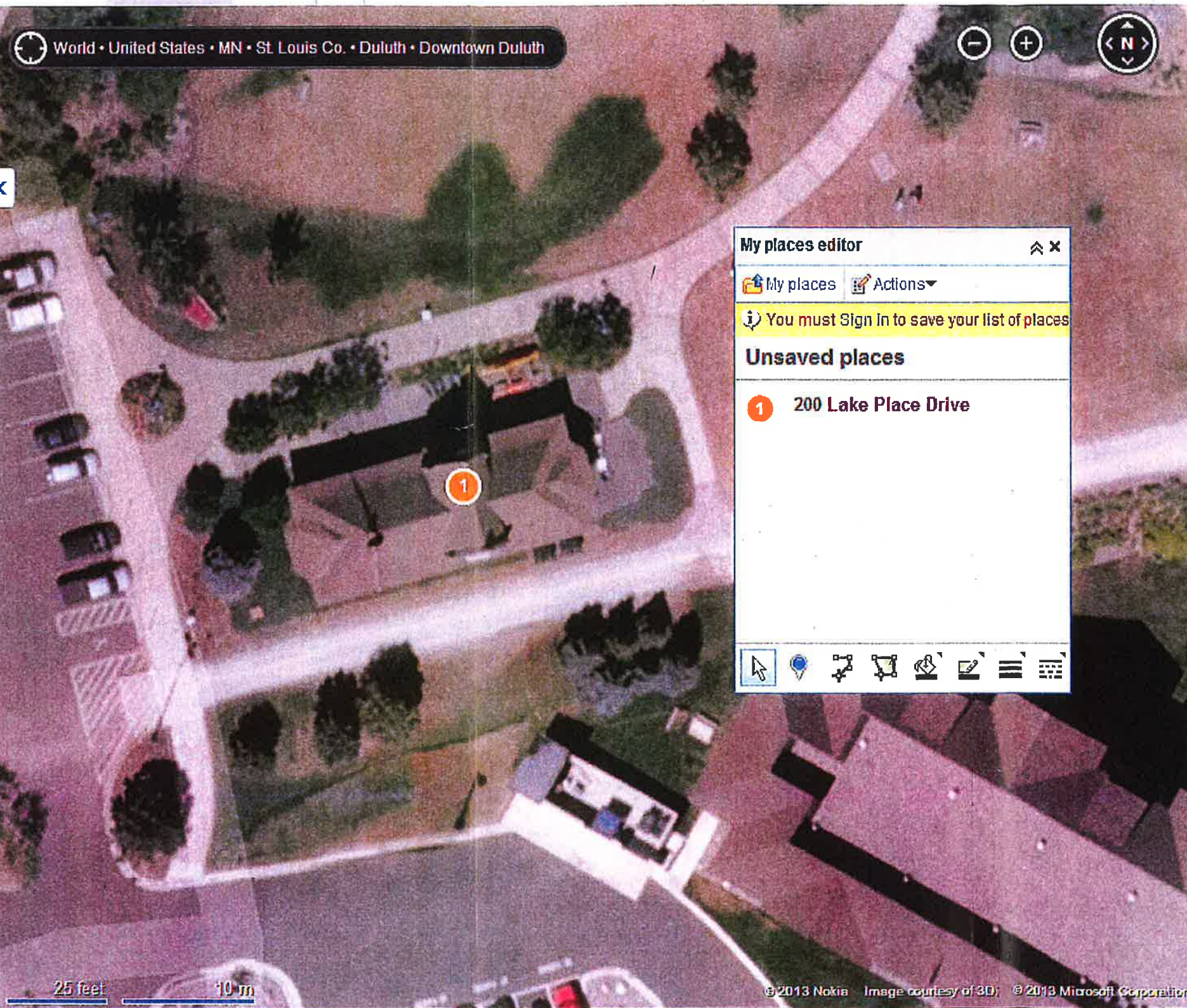
Directions Send More

Not what you wanted?

Report a problem

NEARBY

- see+do
- eat+drink
- shop
- picks for you



My places editor [Close]

My places Actions

You must Sign In to save your list of places

Unsaved places

- 1 200 Lake Place Drive

[Map navigation icons]



lake place drive, duluth mn



Sign in 1 of 5



Directions

My places

Nearby

Road

Aerial

Traffic



Fullscreen

Print

Share



Duluth, Missabe and Iron Range Depot

Lake Place Dr, Duluth, MN 55802

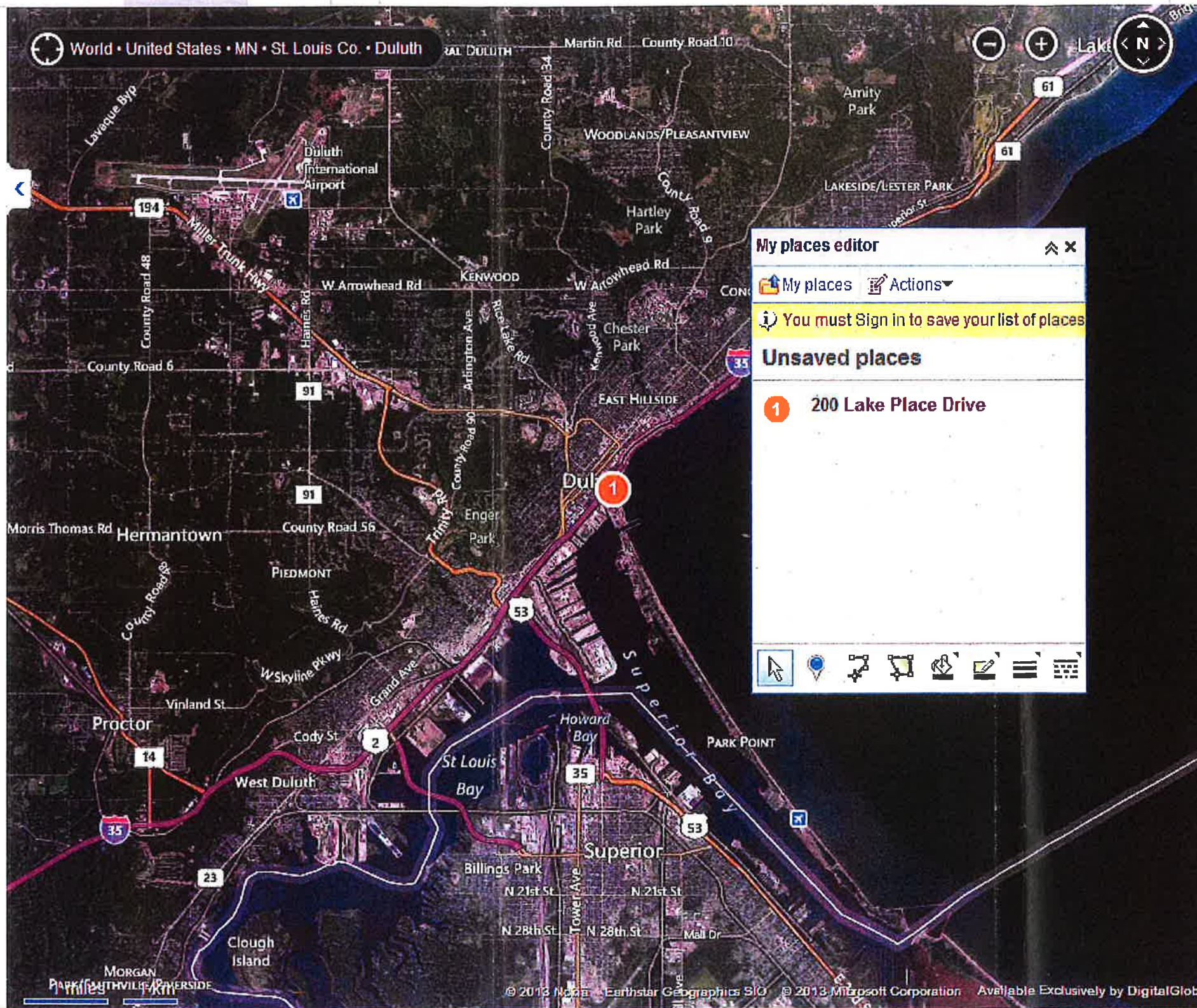
46.786313 -92.095197

Directions · Send · More
Not what you wanted?

Report a problem

NEARBY

see+do
eat+drink
shop
picks for you



My places editor

My places Actions

You must Sign in to save your list of places

Unsaved places

- 1 200 Lake Place Drive

Map navigation icons: pan, location pin, zoom, street view, etc.



lake place drive, duluth mn



Sign in 1 of 5



Directions

My places

Nearby

Road

Aerial

Traffic



Fullscreen

Print

Share



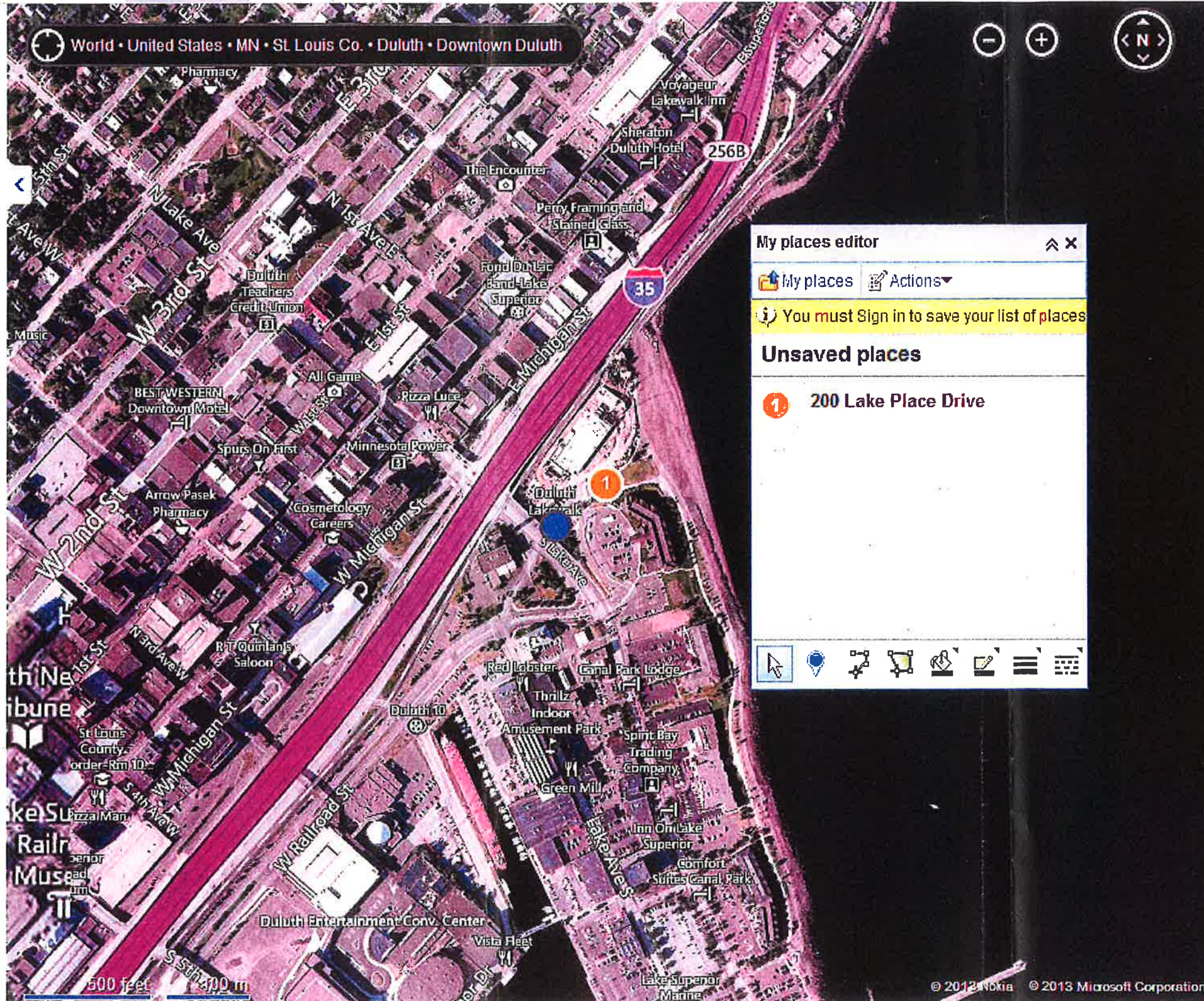
Duluth, Missabe and Iron Range Depot
Lake Place Dr, Duluth, MN 55802
46.786313 -92.095197

Directions · Send · More
Not what you wanted?

Report a problem

NEARBY

- see+do
- eat+drink
- shop
- picks for you



My places editor [Close]

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You must Sign in to save your list of places

Unsaved places

- 1 200 Lake Place Drive

[Map navigation icons]



Canal Park




HANDICAPPED
PARKING
VEHICLE ID
REQUIRED
UP TO \$200 FINE
FOR VIOLATION

NO PARKING
LOADING ZONE
7 AM TO
MIDNIGHT
MONDAY THROUGH
FRIDAY

NO PARKING
EXCEPT FOR
EMERGENCY
VEHICLES



NO
UNLICENSED
VEHICLES BEYOND
THIS POINT
BY JOHN BRADY
IN 1910 THE
CITY OF
DENVER





ENTRIES IN THE NATIONAL REGISTER

STATE MINNESOTA

Date Entered APR 16 1975

<u>Name</u>	<u>Location</u>
Endion Passenger Depot	Duluth St. Louis County
Kitchi Gammi Club	Duluth St. Louis County
Little American Mine	Island View Koochiching County

Also Notified

Hon. Walter F. Mondale
 Hon. Hubert H. Humphrey
 Hon. James Oberstar

Regional Director, Mid West
 Region

State Historic Preservation Officer
 Mr. Russell W. Fridley, Director
 Minnesota Historical Society
 690 Cedar Street
 St. Paul, Minnesota 55101

Copy to Crecco

PR

MMott/cha

4-18-75

For Register of Architects



Minnesota Department of Transportation
Transportation Building, St. Paul, MN 55155

Phone 612-296-3276

January 10, 1986

Mr. Gregory Kendrick, Historian
Cultural Resources Division
National Park Service
Rocky Mountain Region
P.O. Box 25287
Denver, Colorado 80225

In reply refer to:
NPS File H 38 (RMR-PR)
HAER MINN-9
Mn/DOT File 330
S.P. 6982-124 & 196 (I-35)
Minn. Proj. I 035-6 (216)
Endion Passenger Depot
In Duluth, St. Louis County

Dear Mr. Kendrick:

In fulfillment of stipulation 5 of the Memorandum of Agreement approved in May 1977 between the Advisory Council on Historic Preservation, the Federal Highway Administration and the State Historic Preservation Officer, as clarified by your letter of September 25, 1985, we submit the following documentation of the Endion Passenger Depot for the permanent records of the Historic American Engineering Record (HAER).

One package of unbound sheets including:

- a) Cover, title sheet
- b) Photographic documentation, including index and 18 dry-mounted photographs
- c) Historical narrative consisting of 5 typewritten pages
- d) 18 negatives packaged in individual sleeves

The historical research was conducted and documentation compiled by personnel of the Minnesota Department of Transportation. The 1985 photographs were taken by two of our department photographers, Dale R. Tresler and David R. Gonzalez, and the film was processed

Mr. Gregory Kendrick
Page 2
January 10, 1986

in our department's photo lab. The negatives have been tested and found to be free of traces of residual hypo. The documentation has been prepared in accordance with the "Guidelines for the Preparation and Transmittal of HABS/HAER Documentation."

We appreciate the guidance which you have given us on this project and hope that this documentation will meet with your acceptance. Your early review and approval would be most appreciated, since the conditions of construction along the possible moving routes require that the building be moved early this year. We intend to let a contract for this move on February 28, 1986.

Should you require anything further, please call me on 612/296-3276. Thank you for your cooperation and assistance.

Sincerely,

Clement P. Kachelmyer
Preliminary Design Engineer

Enclosures



United States Department of the Interior

NATIONAL PARK SERVICE

ROCKY MOUNTAIN REGIONAL OFFICE

655 Parfet Street

P.O. Box 25287

Denver, Colorado 80225

SEP 25 1985

IN REPLY REFER TO:
1138 (RMP-PP)

Mr. Clerent P. Kachelmyer
Preliminary Design Engineer
Minnesota Department of Transportation
Transportation Building
St. Paul, Minnesota 55155

Dear Mr. Kachelmyer:

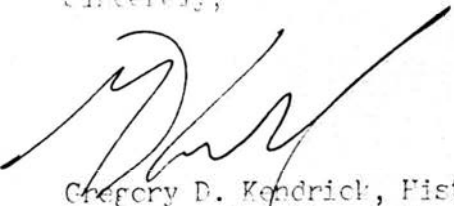
Thank you for your request regarding the appropriate Historic American Engineering Record (HAER) documentation for the Endicott Passenger Depot, Duluth, Minnesota.

The documentation outlined below will satisfy HAER standards:

1. Complete archival quality photographic documentation (including representative interior views and views of significant interior architectural features) following the guidelines outlined in the enclosed "Specifications for Contract Photographers."
2. A 1-3 page written narrative placing the structure in historical perspective. The history should follow the content and format of the enclosed HAER/HAER written documentation samples.
3. The documentation must be edited, catalogued and packaged according to the enclosed "Guidelines for the Preparation and Transmittal of HAER/HAER Documentation." The archival supplies and HAER number will be provided by this office upon request. Please allow two weeks for delivery.

Thank you for your commitment to the recordation of our Nation's endangered historic resources.

Sincerely,



Gregory D. Kendrick, Historian
Division of Cultural Resources

Enclosure

cc:
Mr. Russell Fridley, Minnesota Historical Society, St. Paul, Minnesota



MINNESOTA HISTORICAL SOCIETY

FOUNDED IN 1849

690 Cedar Street, St. Paul, Minnesota 55101 • (612) 296-6126

February 10, 1986

Ms. Carol Schull, Keeper
National Register of Historic Places
Interagency Resources Division
U. S. Department of the Interior
1100 L Street
Room 6209
Washington, D. C. 20243

Dear Ms. Schull:

Re: S. P. 6982-03, 6982-196 (I-35)
Minn. Proj. I-35-6 (216)
From Tenth Avenue East to 26th Avenue East
Endion Passenger Depot
Duluth, St. Louis County
MHS Referral File Number: P-820

Enclosed please find a report prepared by the Minnesota Department of Transportation regarding the moving of the Endion Depot, a property listed on the National Register of Historic Places. This report was prepared in accordance with Stipulation 2 of the May 1977 Memorandum of Agreement governing this project. It is our opinion that the documentation contained in this report is complete.

It is our recommendation that the Endion Depot be retained on the National Register during its move to the new site and after its placement on the new site.

Enclosed with this letter, in addition to the Report, are copies of correspondence regarding the report and a copy of documentation of the Depot submitted to the Historic American Engineering Record.

Sincerely,

Russell W. Fridley
State Historic Preservation Officer

RWF:dmb

Enclosures

cc: Clem Kachelmyer
Preliminary Design Engineer
Minnesota Department of Transportation
612H Transportation Building
St. Paul, Minnesota 55155

Rec'd 3/10/86

H32 (413)

MAY 2 1986

Mr. Russell W. Fridley
Director, Minnesota Historical Society
699 Cedar Street
St. Paul, Minnesota 55101

Dear Mr. Fridley:

Thank you for your request that the National Register approve the proposed move of the Endion Passenger Depot, St. Louis County, Minnesota, listed on April 16, 1975.

After reviewing the documentation, the National Register approves the move of the Endion Passenger Depot to its proposed new site within the same part of town, approximately 14 blocks from its historic location; therefore the depot will remain listed during its move. The station will have a similar setting, general environment, and north/south orientation as on its historic site, although, unfortunately, it will not face Lake Superior as it had originally. When the move has been completed, please notify us of the date of the move, and provide photographs of the depot on its new site, along revised maps and geographical information, on signed National Register continuation sheets in accordance with 36 CFR 60.14(b)(3).

We appreciate the opportunity to review this documentation.

Sincerely,

[Carol D. Shull (Sgd.)

Carol D. Shull
Chief of Registration
National Register of Historic Places
Interagency Resources Division

bcc: 1200-RMRO

413

Reading File
Record Center

FNP:BGrosvenor:mdj:05/01/86:343-9536

Sys. 8 (NRH) MN-SHPO

Basic File Retained in 413

H32(413)

MAY 26 1989

Ms. Nina M. Archabal
Director
Minnesota Historical Society
690 Cedar Street
St. Paul, Minnesota 55101

Dear Ms. Archabal,

We received your request to change the name of the Endion Passenger Station, St. Louis County, Minnesota, to the Duluth Missabe and Iron Range Depot (Endion). Because the new name is associated with the historic railroad for which the depot was built, we have changed the name as requested. However, our review of the documentation revealed another problem.

In 1986 your office requested approval to move the station approximately 14 blocks from its historic location. In the National Register's letter approving the move, we asked to be notified when the move was complete, and sent photographs of the depot on its new site and revised maps and geographic information on signed continuation sheets. We have never received revised documentation.

The new address on the continuation sheet with the name change indicates that the Duluth Missabe and Iron Range Depot (Endion) has been moved to its proposed new site. If that is the case, please provide us with the information specified above.

Thank you for your cooperation in this matter.

Sincerely,

Carol Shull

Carol D. Shull
Chief of Registration
National Register of Historic Place
Interagency Resources Division

cc: 413-Reading File
Record Center

Basic file retained 413



FOUNDED IN 1849

MINNESOTA HISTORICAL SOCIETY

Fort Snelling History Center, St. Paul, MN 55111 • (612) 726-1171

5 June 1989

Ms. Carol D. Shull
Chief of Registration
National Register of Historic Places
Interagency Resources Division
National Park Service
P.O. Box 37127
Washington D.C. 20013-7127

Dear Ms. Schull:

Your letter to Mrs. Nina Archabal was referred to me for response. Thank you for bringing to our attention the documentation needed to update the nomination of the Duluth Missabe and Iron Range Depot (Endion) located in Duluth, St. Louis County.

As soon as staff are in Duluth we will take photographs of the depot on its new location. The photos, the updated U.S.G.S. map, and geographic documentation will then be forwarded to your office.

Sincerely,

Susan Roth
National Register Historian
State Historic Preservation Office

cc: Nina M. Archabal, State Historic Preservation Officer

REPORT TO
THE MINNESOTA STATE HISTORIC PRESERVATION OFFICER
AND
THE KEEPER OF THE NATIONAL REGISTER OF HISTORIC PLACES

ON THE PROPOSED MOVE OF
THE ENDION PASSENGER DEPOT
DULUTH, ST. LOUIS COUNTY, MINNESOTA

BY THE
MINNESOTA DEPARTMENT OF TRANSPORTATION
ST. PAUL, MINNESOTA
IN COOPERATION WITH THE
FEDERAL HIGHWAY ADMINISTRATION

REPORT TO
THE MINNESOTA STATE HISTORIC PRESERVATION OFFICER
AND
THE KEEPER OF THE NATIONAL REGISTER OF HISTORIC PLACES

ON THE PROPOSED MOVE OF
THE ENDION PASSENGER DEPOT
DULUTH, ST. LOUIS COUNTY, MINNESOTA

BY THE
MINNESOTA DEPARTMENT OF TRANSPORTATION
ST. PAUL, MINNESOTA
IN COOPERATION WITH THE
FEDERAL HIGHWAY ADMINISTRATION

INDEX

	Page
A. Reasons for Move	1
B. Analysis of the Integrity of the New Site	1-2
C. Description of New Site	2
D. Photographs of the New Site	
Photograph Captions	2-3
Photograph Index Map	4
Photographs	5-9
E. Attachments	
1. Memorandum of Agreement	
2. Portion of Layout for Proposed I-35	
3. 8 sheets of plans for State Project No. 6982-196 (TH 35=395), Relocation of Endion Depot Building	
4. Duluth Parks & Recreation letter of September 19, 1985	
5. SHPO letter of January 15, 1986	
6. Plan for development of "Corner of the Lake" Park	

REPORT ON THE PROPOSED MOVE OF THE ENDION PASSENGER DEPOT

This report to the Minnesota State Historic Preservation Officer (SHPO) and the Keeper of the National Register of Historic Places has been prepared in accordance with Stipulation 2 of the Memorandum of Agreement (MOA) of May 1977 between the SHPO, the Federal Highway Administration (FHWA) and the Advisory Council on Historic Preservation (ACHP). A copy of the MOA is included as Attachment 1.

A. Reasons for the Move:

The Endion Passenger Depot, a property included in the National Register of Historic Places, is located at 1504 South Street on Lots 1 and 2, Block 14, Endion Division, Duluth, Minnesota. The property on which this building is situated is needed for the proposed construction of the segment of Interstate Highway 35 (I-35) between Tenth Avenue East and 26th Avenue East in Duluth.

The proposed alignment was selected following many years of study of alternative corridor locations. The Final Environmental Impact Statement (FEIS) and Section 4(f) Statement were published in 1977. The selection of this alignment was reaffirmed in 1984 when the Minnesota Legislature enacted Chapter 477, Sec. 2, Laws of Minnesota which directed that Interstate Highway 35 be constructed in this corridor.

Between 15th Avenue East and 16th Avenue East the transportation corridor occupies a narrow strip of land between South Street and Lake Superior previously occupied by the Duluth, Missabe and Iron Range Railway (DM&IR) tracks and the Endion Depot. A print of a portion of the layout for I-35 at this location is included as Attachment 2. The distance from the front face of the Depot to the top edge of the slope to Lake Superior is only about 70', which is not sufficient to allow for the construction of the Interstate freeway and retention of one set of railroad tracks without removal of the building.

Recognizing these factors, the 1977 Memorandum of Agreement concluded that moving the building would satisfactorily mitigate any adverse effect.

B. Analysis of the Integrity of the New Site:

The entire building above ground level will be moved in accordance with the plans identified as S.P. 6982-196, the eight sheets of which are included as Attachment 3. On its new site, the building will overlook Lake Superior as it does on the existing site. It will be adjacent to a bikeway, parallel to a proposed trolley bus route, and near to active railroad tracks.

As stated in a letter of September 19, 1985 from Duluth Director of Parks and Recreation James W. McCord, the City of Duluth intends to use the building for public restrooms, a convention and tourism office and information center, and as a boarding - unboarding station for the proposed Lisbon Trolley, wheeled streetcar or excursion train. A copy of that letter is included as Attachment 4.

The Minnesota State Historic Preservation Office has reviewed the proposed site and concluded that it provides an adequate setting for the structure. A copy of their letter of January 15, 1986 is included as Attachment 5.

C. Description of New Site:

The new site for the relocation of the Endion Passenger Depot is at the westerly extremity of Lake Superior. It is in an area owned by the City of Duluth. The City intends to develop this land as a park to be known as the "Corner of the Lake".

The park will be between the westerly shore of Lake Superior and present First Avenue East, and will include a beautiful view of Lake Superior, bikeways, walkways, a picnic area, and the proposed trolley bus route. It will be located along a walkway from downtown Duluth to the Duluth Ship Canal Park. The City has acquired an existing nightclub in the area and intends to demolish it this summer. The plan for the proposed park development is included as Attachment 6.

The land on which the building is to be situated is basically fill material. Soil borings were made at the site by Minnesota Department of Transportation (Mn/DOT) personnel on September 9, 1985. They revealed layers of cinders, wood and rocks in the upper 10'. Copies of the boring logs were furnished to the Minnesota Historical Society archaeologists and to the SHPO and were reviewed by them in conjunction with their surface archaeological survey. The SHPO's conclusion that the site appears to have no significant archaeological potential is stated in the letter of January 15, 1986 included as Attachment 5. Therefore, the new site will not be adversely affected by the move.

D. Photographs of the New Site:

See photograph index map for direction of views.

Photograph #1

View of relocation site from the east. Automobile is parked at the building site. Bridge #69816 connecting Lake Avenue to First Avenue East in near background crosses railroad tracks, streets and Interstate Highway 35.

Photograph #2

View of site from the north. Automobile is parked at the building site. Harbor Inn is the adjacent property which will remain. Aerial Lift Bridge (Bridge No. L6116), a property included on the National Register of Historic Places, is in the background.

Photograph #3

View of site from the southeast. Automobile is parked at the building site. City proposes to demolish the Club Saratoga building this summer. The long bridge (portion of Bridge #69816) will serve interchange ramp H for I-35. The tower in the center background is part of Duluth Central High School, a property included on the National Register of Historic Places.

Photograph #4

View of site from the south-southwest. Automobile is parked at the building site. Tan colored stone building is City sewage pumping station.

Photograph #5

View of site from the west. First Avenue East in foreground will be obliterated. Harbor Inn at right will remain. Lake Superior is in background.

Photograph #6

View of site from the northwest from Bridge #69816.

Photograph #7

View of site from the southwest from Bridge #69816. Illustrates intersection of west and north shores of Lake Superior in background.

Photograph #8

View of site from the west-southwest from Bridge #69816.

Photograph #9

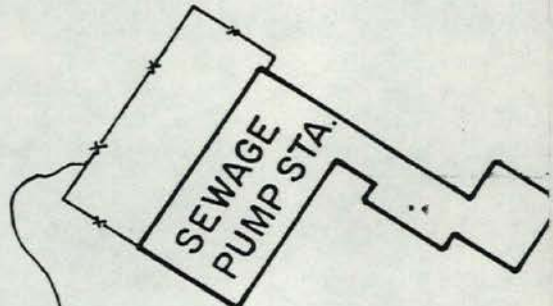
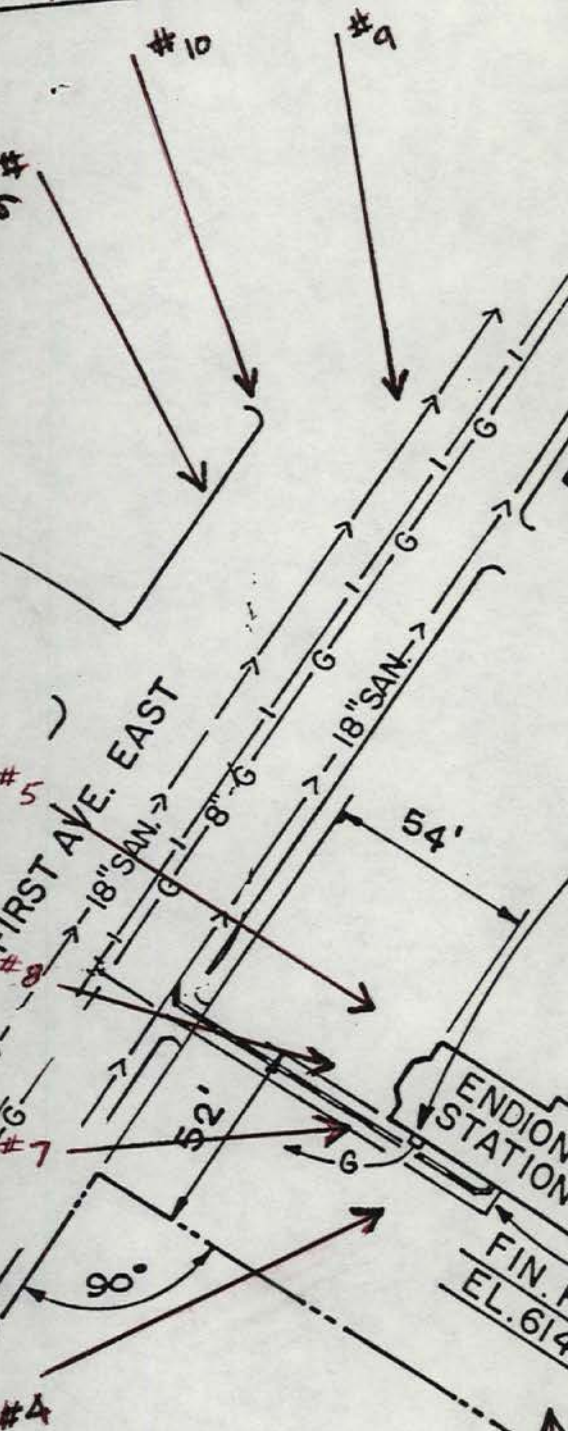
View of site from the north-northwest from Bridge #69816 leg for Ramp H.

Photograph #10

View of site from the northwest from Bridge #69816 leg for Ramp H.

RAMP "H"

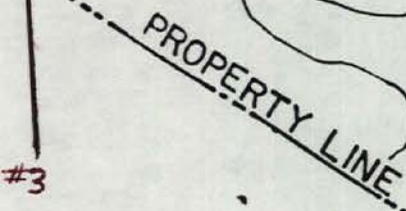
STA. 66+02.75
EL. 616.78



GAS SERVICE & METER BY GAS CO. CONTRACTOR TO CONNECT DISTRIBUTION PIPING.



CONNECT NEW 3/4" WATER SERVICE TO EXIST. DISTRIBUTION PIPING. INSTALL EXISTING WATER METER. PROVIDE AND INSTALL NEW PRESSURE REDUCING VALVE.

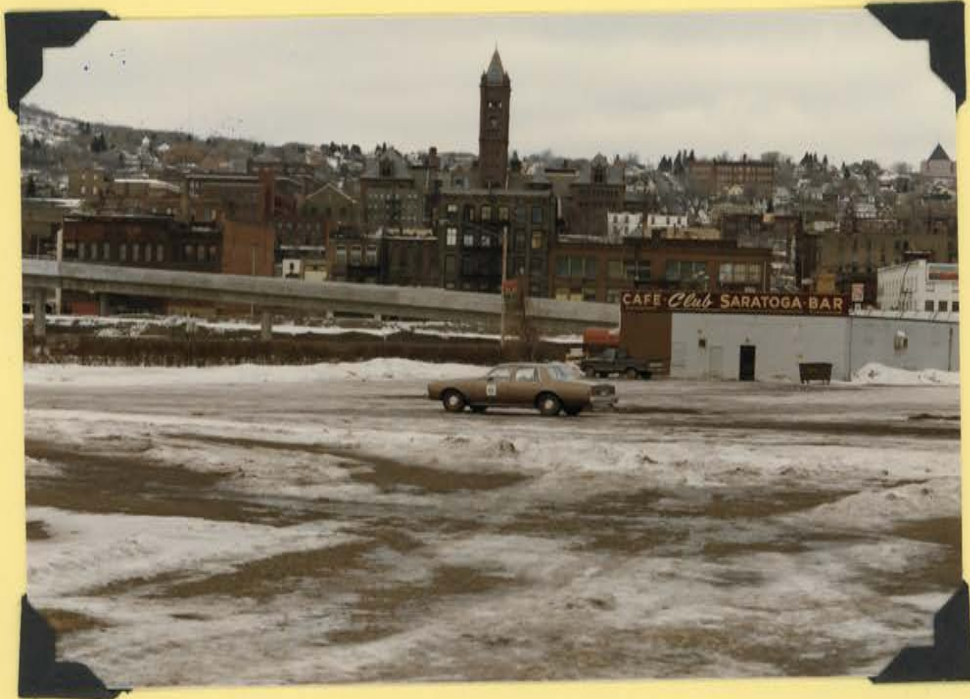




PHOTOGRAPH #1



PHOTOGRAPH #2



PHOTOGRAPH #3



PHOTOGRAPH #4



PHOTOGRAPH #5



PHOTOGRAPH #6



PHOTOGRAPH #7



PHOTOGRAPH #8



PHOTOGRAPH #9



PHOTOGRAPH #10

Advisory Council on
Historic Preservation
1522 K Street N.W.
Washington, D.C. 20005

MEMORANDUM OF AGREEMENT

WHEREAS, the U.S. Department of Transportation, Federal Highway Administration, proposes to assist the Minnesota Department of Transportation with the construction of Interstate Highway 35 from Mesaba Avenue to 26th Avenue East in the City of Duluth, Minnesota, under the Federal Aid Interstate Program; and,

WHEREAS, the Federal Highway Administration, in consultation with the Minnesota State Historic Preservation Officer, has determined that this proposal will affect the Endion Passenger Depot, a property included in the National Register of Historic Places, and Branch's Hall, a property eligible for inclusion in the National Register of Historic Places; and,

WHEREAS, pursuant to Section 106 of the National Historic Preservation Act of 1966 (16 U.S.C. 470f, as amended, 90 Stat. 1320), the Federal Highway Administration has requested the comments of the Advisory Council on Historic Preservation; and,

WHEREAS, pursuant to the Council's procedures, representatives of the Council, the Federal Highway Administration, and the Minnesota State Historic Preservation Officer have consulted and reviewed the proposal to determine the nature of the effect; now,

THEREFORE:

It is mutually agreed that implementation of the undertaking, in accordance with the following stipulations, will satisfactorily mitigate any adverse effect.

Stipulations

1. At such time as is necessary to accommodate the construction of Highway 35 Northeasterly from 10th Avenue East, the Endion Passenger Depot will be moved from its present location to a location mutually agreeable to the Federal Highway Administration and the Minnesota State Historic Preservation Officer. Such location may be northeasterly in the vicinity of 21st Avenue East or southwesterly in the vicinity of the Duluth Union Depot or at some other mutually agreeable site. Following the move, the building will be placed upon a permanent foundation and restored to a condition comparable to its condition prior to the move.

ATTACHMENT 1

The Council is an independent unit of the Executive Branch of the Federal Government charged by the Act of October 19, 1966 to advise the President and Congress in the field of Historic Preservation.

MEMORANDUM OF AGREEMENT
Endion Depot
Federal Highway Administration

2. At least 30 days prior to the date the Endion Passenger Depot is moved, the Federal Highway Administration shall submit to the Minnesota Historic Preservation Officer the following documentation:

- a. A statement of the reasons for the move;
- b. An analysis of the property's historical or architectural integrity on its new site;
- c. A description of the new setting and general environment of the proposed site, including evidence that the new site will not be adversely affected by the move;
- d. Photographs of the new site.

Within 15 days of the receipt of the above documentation, the Minnesota Historic Preservation Officer shall forward this documentation to the National Register of Historic Places, along with his recommendation as to whether the property should remain in the National Register. The National Register will file and maintain this documentation until after the move.

3. Within 30 days after the date the Endion Passenger Depot is moved, the Federal Highway Administration shall submit additional documentation showing the property on its new site to the Historic Preservation Officer and the National Register. The Endion Passenger Depot shall remain in the National Register prior to, during, and subsequent to the move, unless documentation submitted by the Federal agency shows evidence of irreparable damage to the historical or architectural integrity of the property so that it no longer meets National Register criteria.

4. The moving of the Endion Passenger Depot shall be conducted in consultation with the Office of Archeology and Historic Preservation, National Park Service, relative to appropriate moving techniques.

5. The Federal Highway Administration shall provide documentation to Historic American Building Survey standards of the Endion Passenger Depot on its current site.

Page Three

MEMORANDUM OF AGREEMENT
Endion Depot
Federal Highway Administration

6. Federal Highway Administration shall perform archeological surveys to professional standards on both the current site of the Endion Passenger Depot and its new location. Sites determined to be eligible for inclusion in the National Register of Historic Places will be salvaged in accordance with the Council's "Guidelines for Making 'Adverse Effect' and 'No Adverse Effect' Determinations for Archeological Resources in Accordance with 36 C.F.R. part 800".

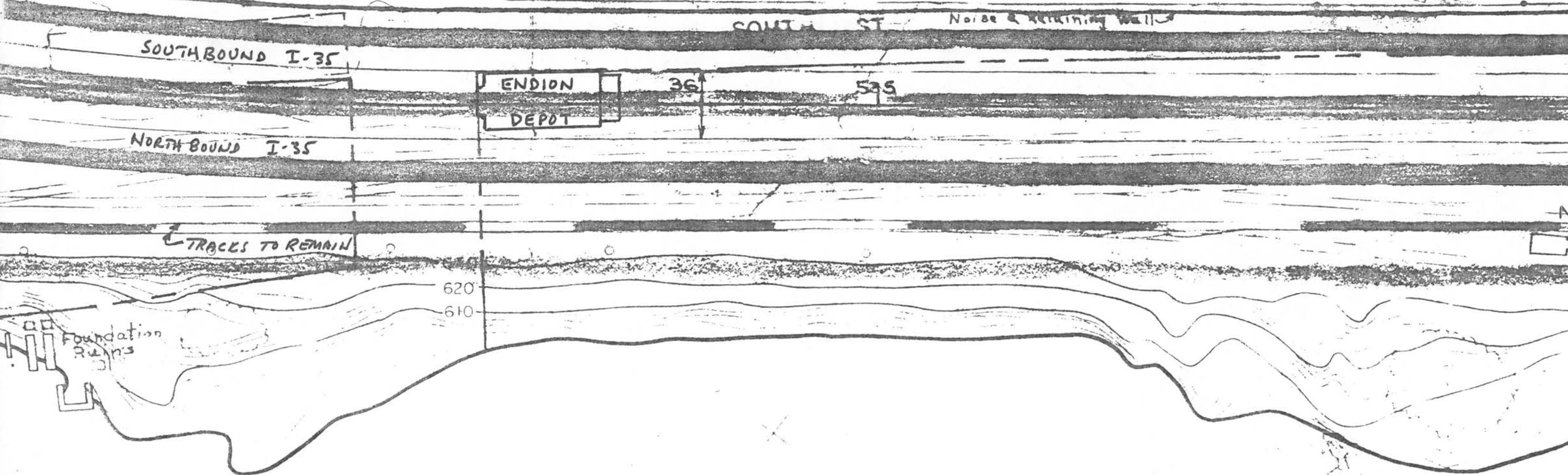
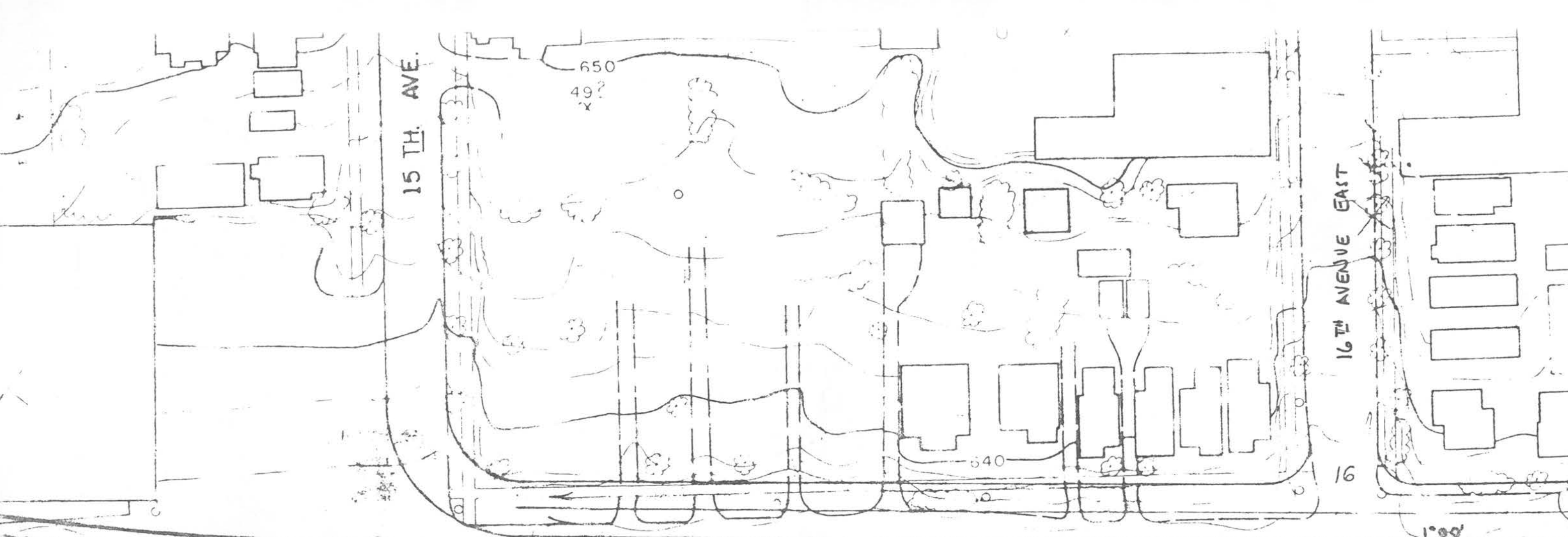
7. Prior to the demolition of Branch's Hall, the Federal Highway Administration and the Minnesota Department of Transportation will obtain photographs of the building acceptable to the Minnesota State Historic Preservation Officer and will salvage significant objects for the archives of the Minnesota Historical Society.

Robert R. Danneberg (date) April 29, 1977
Advisory Council on Historic Preservation

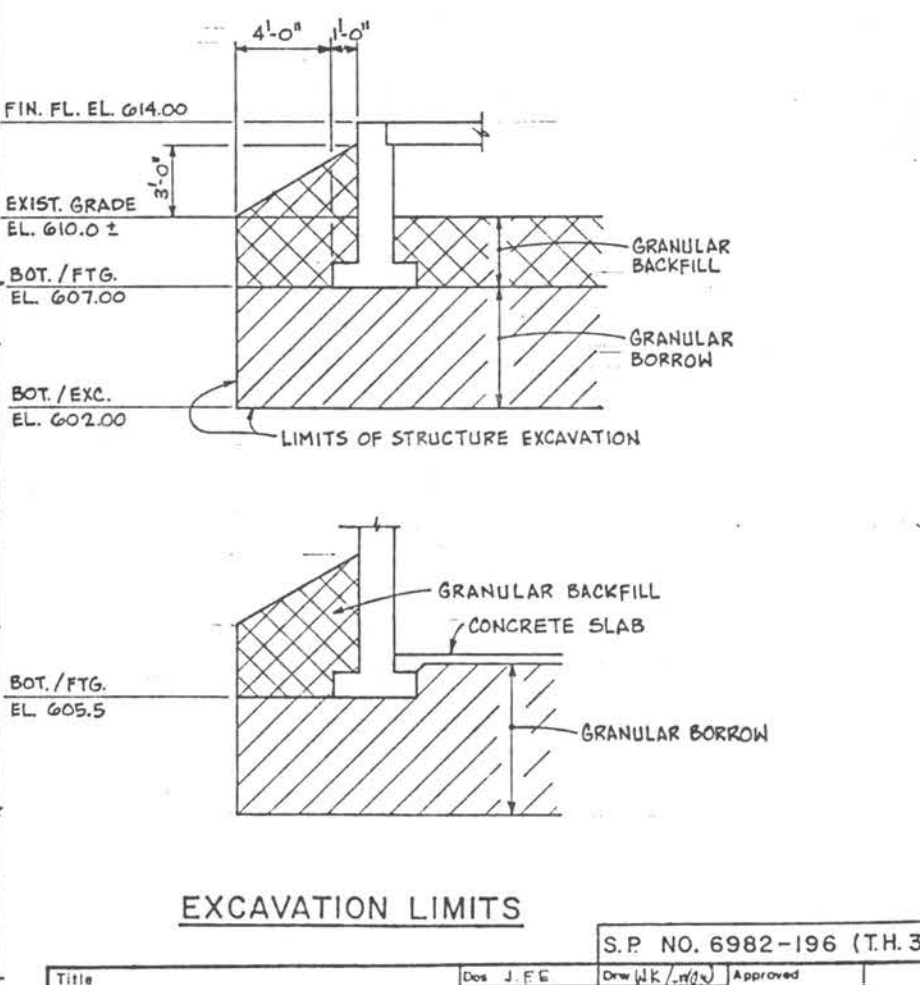
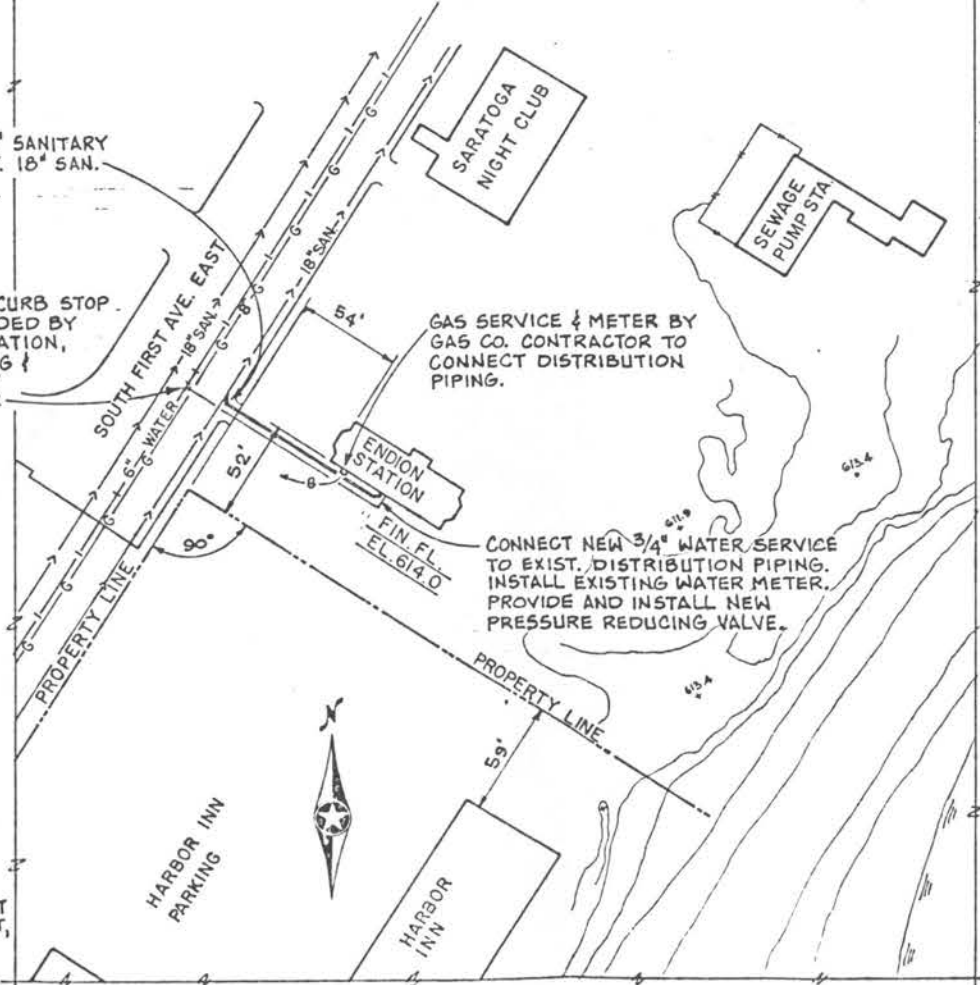
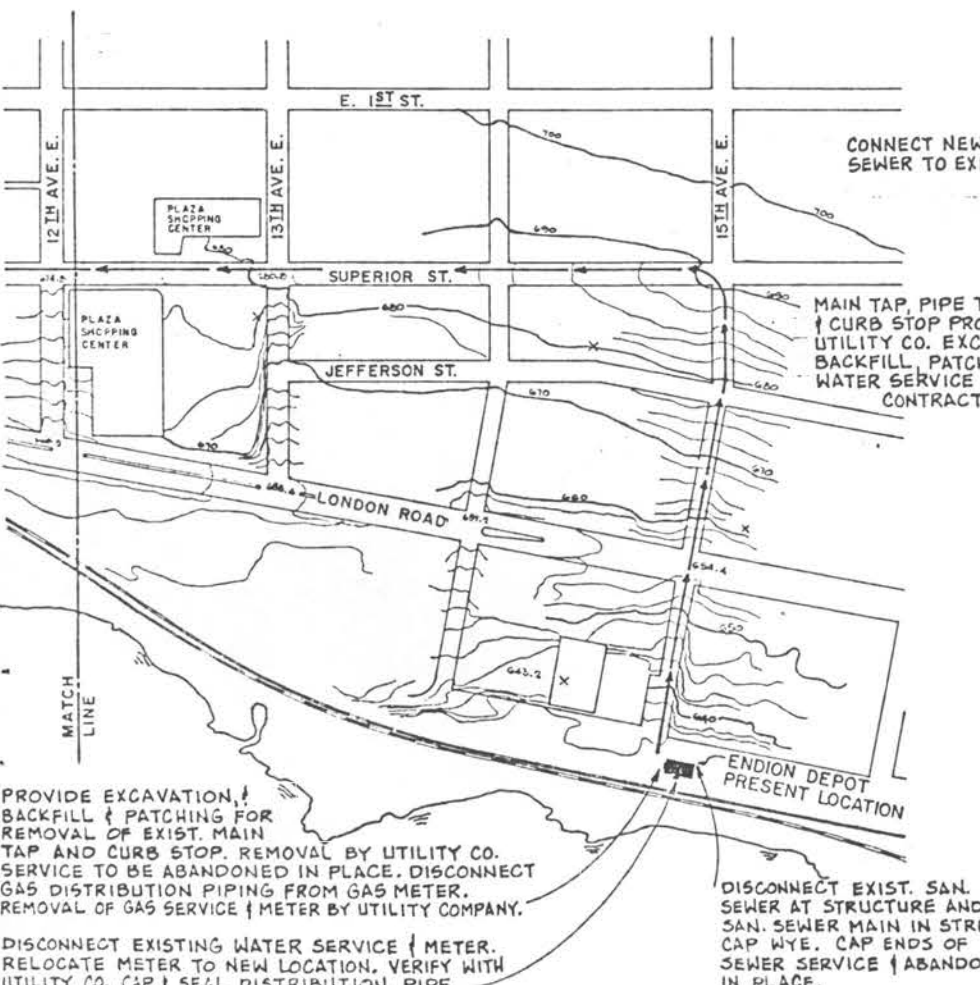
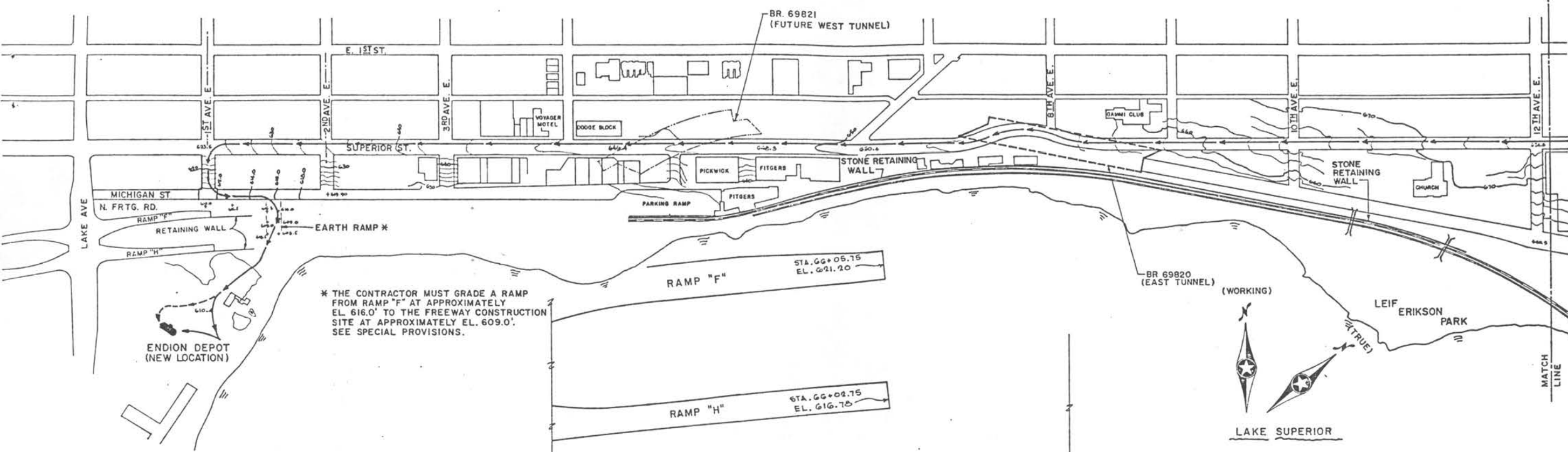
E. Dean Carlson (date) 5-3-77
Federal Highway Administration

Lucille W. Fillingim (date) 5-3-77
Minnesota State Historic Preservation
Officer

J. K. Wilson (date)
Chairman
Advisory Council on Historic Preservation



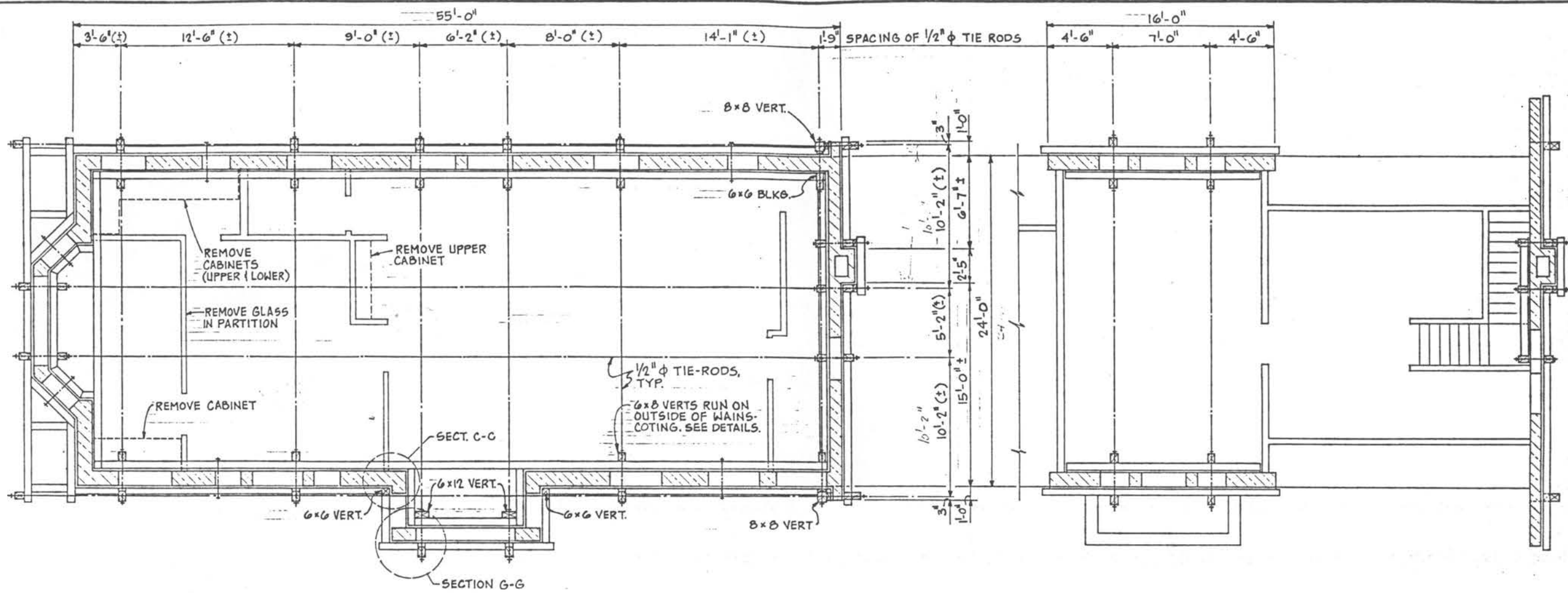
PROPOSED INTERSTATE HIGHWAY 35



SITE PLAN

Title
ROUTE MAP &
SITE PLAN

S.P. NO. 6982-196 (T.H. 35-395)	
Des J.F.G.	Draw J.K./R.G.
CHK	CHK
Approved	
Sheet No. 2 of 8 Sheets	



1ST FLOOR PLAN

2ND FLOOR PLAN

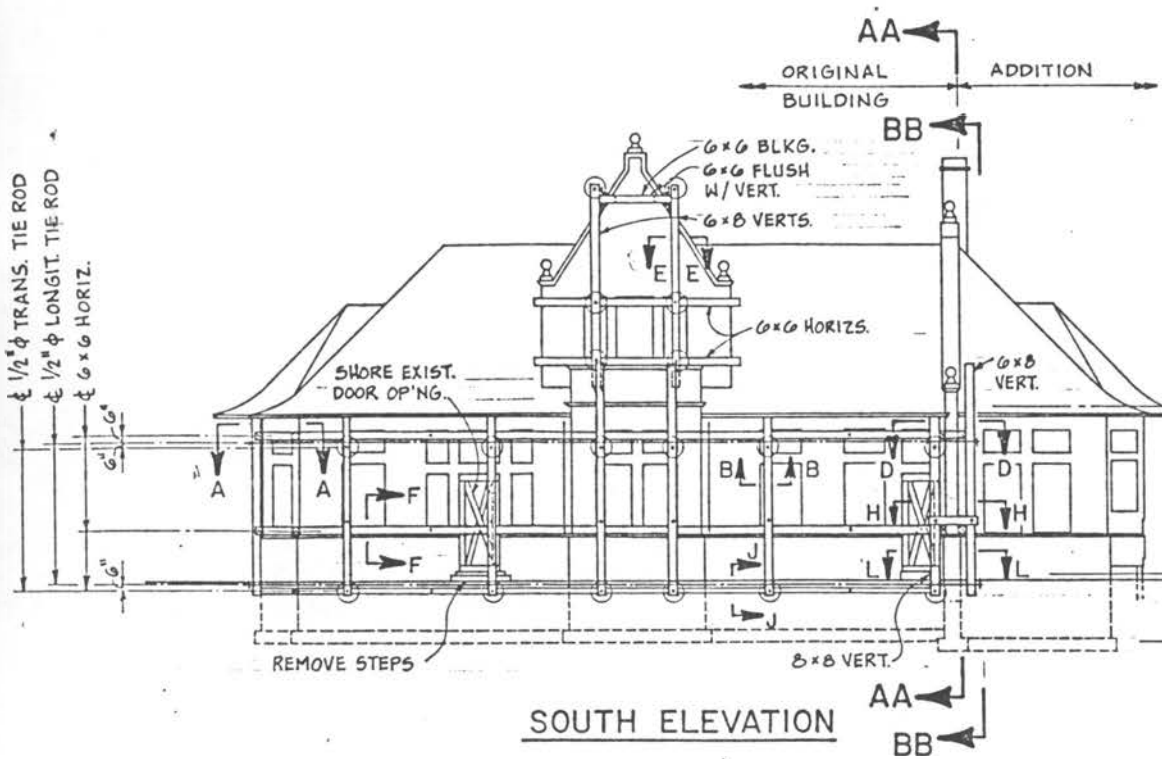
NOTES:

- ① SEE SHEET 4 FOR BRACING ELEVATIONS.
- ② SEE SHEET 5 FOR BRACING DETAILS.
- ③ CONTRACTOR MUST VERIFY EXISTING CONDITIONS WHERE THEY AFFECT THIS WORK.
- ④ SEE SHEET 8 FOR STRUCTURAL NOTES.
- ⑤ SEE SPECIAL PROVISIONS FOR PROCEDURE FOR INSTALLING TEMPORARY BRACING

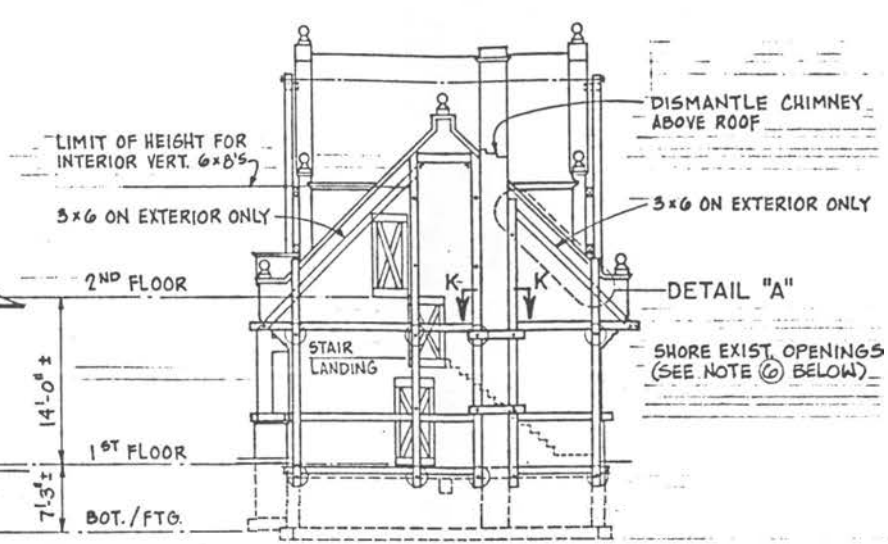
SCHEDULE OF QUANTITIES

TEM NO	2031.501	2103.501	2103.511	2451.501	2451.503	2105.521	2411.501	2411.501	2411.541	2021.501
ITEM	FIELD OFFICE, TYPE D	BUILDING REMOVAL	BASEMENT FILL	STRUCTURE EXCAVATION CLASS E	GRANULAR BACKFILL	GRANULAR BORROW	STRUCTURE CONCRETE (B43)	STRUCTURE CONCRETE (B46)	REINFORCEMENT BARS	MOBILIZATION
UNIT	EACH	LUMP SUM	CU.YD.	CU.YD.	CU.YD.	CU.YD.	CU.YD.	CU.YD.	POUND	LUMP SUM
QUANTITY	1	ONE	180	820	310	520	33	145	6890	ONE
ITEM NO.	0100.602	0100.603	0100.603	0100.603	0105.607	0504.601	0503.601	0100.603	0563.601	
ITEM	MASONRY RESTORATION	TEMPORARY BRACING	RELOCATE BUILDING	ELECTRICAL TELEPHONE CONNECTIONS	SITE GRADING	WATER MAIN SYSTEM	SANITARY SEWER SYSTEM	HEATING SYSTEM	TRAFFIC CONTROL	
UNIT	SQ. FT.	LUMP SUM	LUMP SUM	LUMP SUM	LUMP SUM	LUMP SUM	LUMP SUM	LUMP SUM	LUMP SUM	
QUANTITY	500	ONE	ONE	ONE	ONE	ONE	ONE	ONE	ONE	

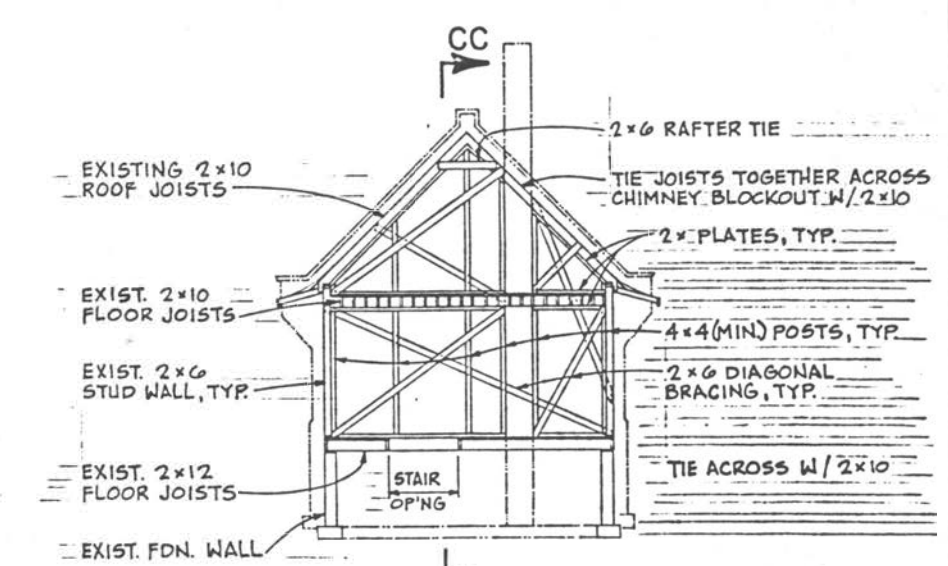
S.P. NO. 6982-196 (T.H. 35-395)



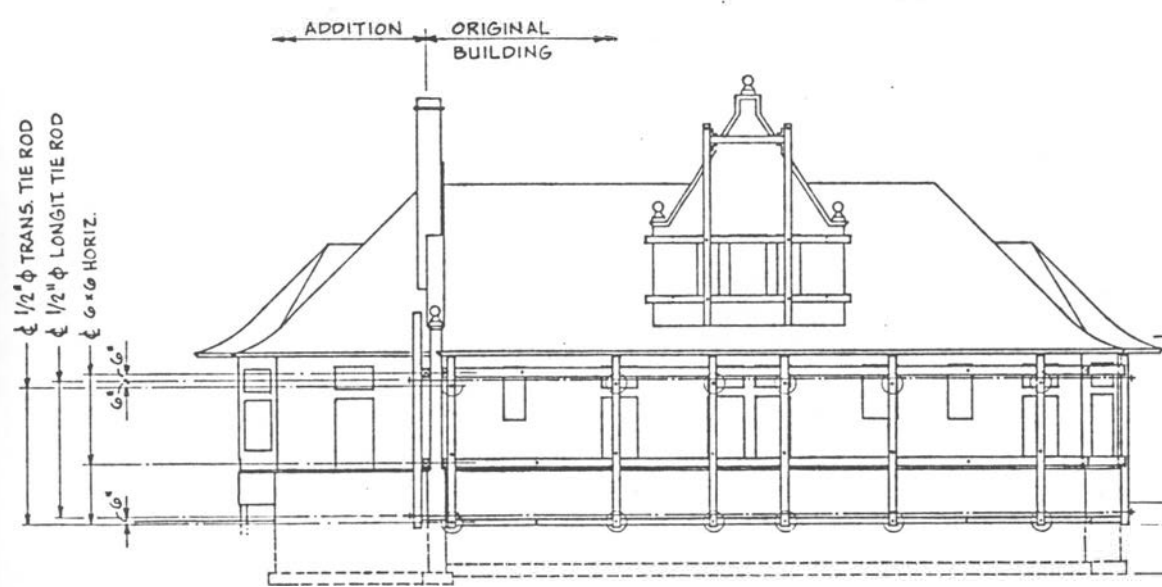
SOUTH ELEVATION



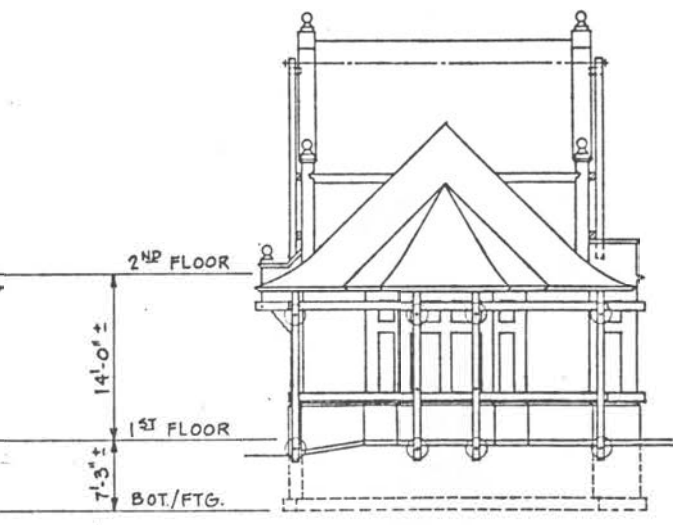
EAST ELEVATION-SECTION AA-AA



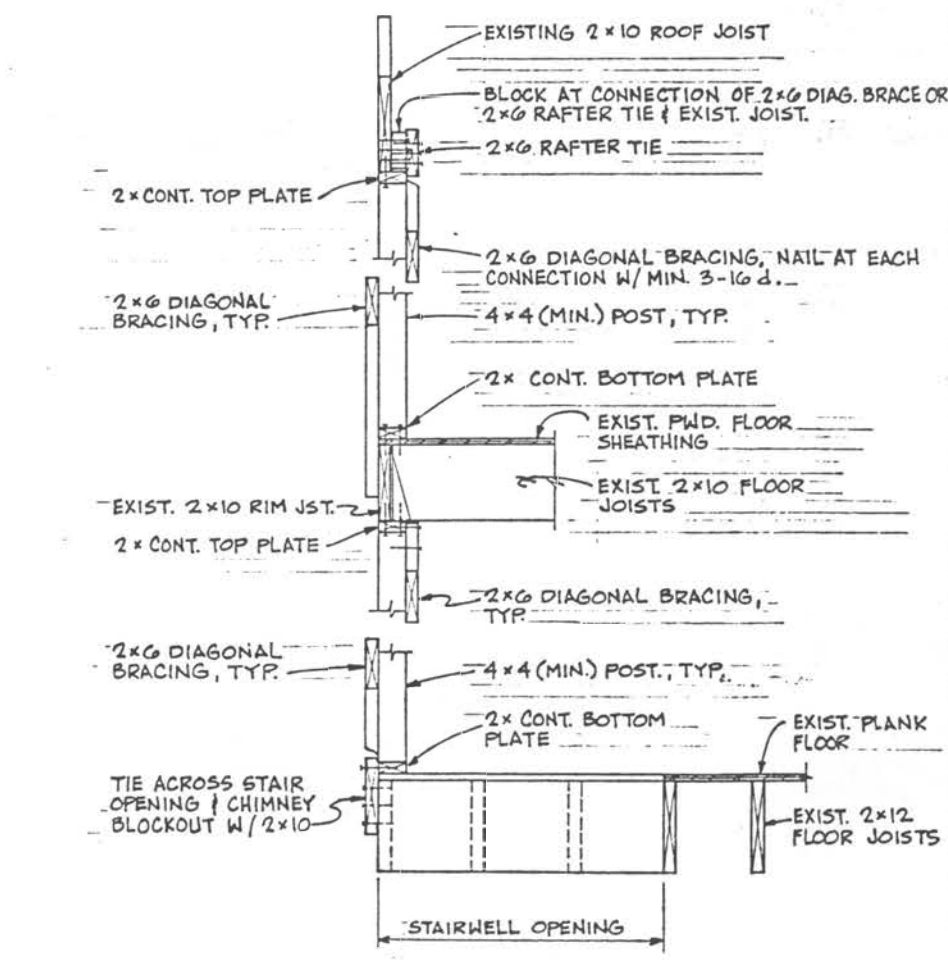
BUILDING SECTION BB-BB



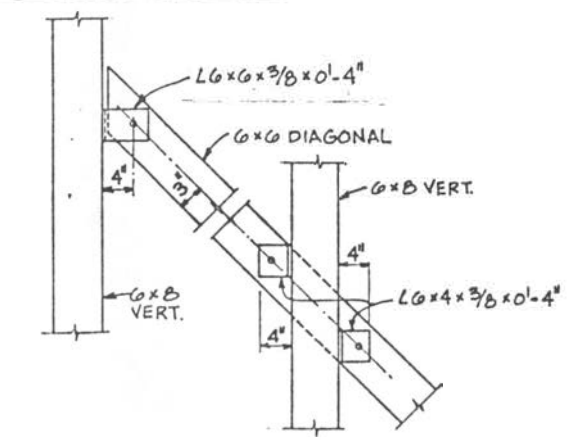
NORTH ELEVATION



WEST ELEVATION



SECTION CC-CC

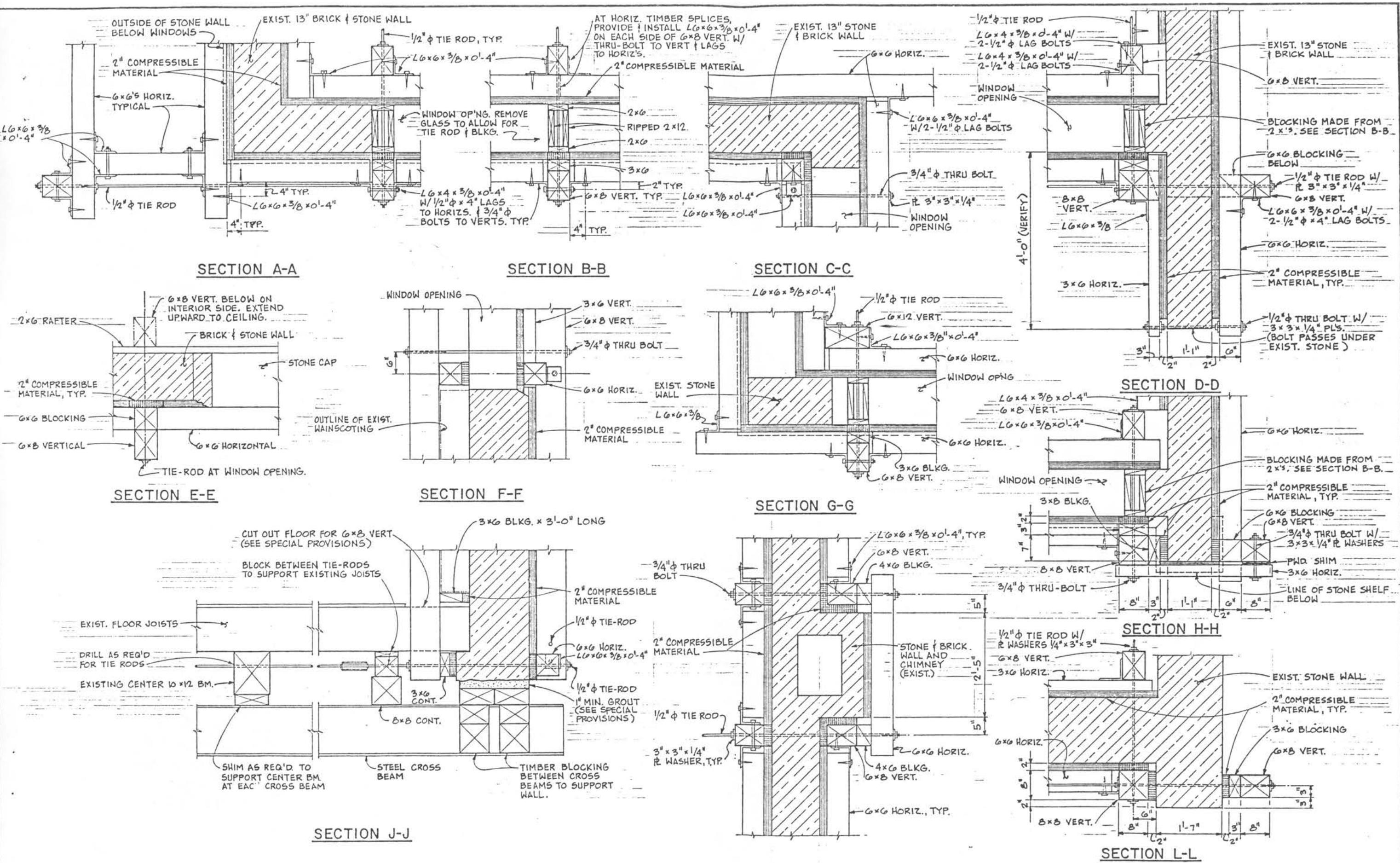


DETAIL 'A'

- NOTES:**
- WHERE THRU-BOLTS OR TIE-RODS PENETRATE BRICK WALL, REMOVE BRICK IN EXTERIOR WYTHE BEFORE DRILLING HOLE. SAVE BRICKS TO BE REINSTALLED AFTER MOVE.
 - WHERE THRU-BOLTS OR TIE-RODS PENETRATE STONE WALL, LOCATE HOLE SUCH THAT IT FALLS IN A JOINT. DRILL HOLE AS CLOSE TO BOLT SIZE AS POSSIBLE. DO NOT OVERSIZE.
 - ALL VERTICAL BRACES ARE 6x8 (MIN.) UNLESS OTHERWISE NOTED, AND ARE CONNECTED TO THE STRUCTURE WITH THRU-BLOG. TIE-RODS AND/OR THRU-WALL BOLTS.
 - ALL HORIZONTAL BRACES ARE 6x6 (MIN.) UNLESS OTHERWISE NOTED AND ARE CONNECTED TO THE STRUCTURE WITH THRU-WALL BOLTS.
 - BLOG. CUT LINE SHALL BE BELOW FIRST FLOOR JOISTS AS INDICATED IN SECTION J-J, SHEET 4.
 - SHORE OPENINGS AS SHOWN WITH 6x6 MIN. TIMBERS, AND PROVIDE 2x4 DIAGONAL BRACING ACROSS OPENING, EACH DIRECTION.

- BRACE END WALL OF ADDITION WITH 4x4 (MIN.) VERTS. AS SHOWN IN SECTION BB-BB (THIS SHEET) AND PROVIDE 2x6 DIAGONAL BRACING ACROSS OPNG. APPLY OUTSIDE BRACE AFTER ADDITION AND MAIN BUILDING HAVE BEEN SEPARATED.
- SEE SHEET 4 FOR SECTIONS AND DETAILS.
- SEE SHEET 2 FOR BRACING PLANS.
- NOTES 1/2" ϕ TIE-ROD.
- DENOTES 3/4" ϕ THRU-BOLT.

Title ELEVATIONS OF TIMBER BRACING			S.P. NO. 6982-196 (T.H. 35=395)	
Date J. F. E.	Drawn M. B.	Approved		
Chk	Chk			
Sheet No. 4 of 8 Sheets				



SECTION A-A

SECTION B-B

SECTION C-C

SECTION D-D

SECTION E-E

SECTION F-F

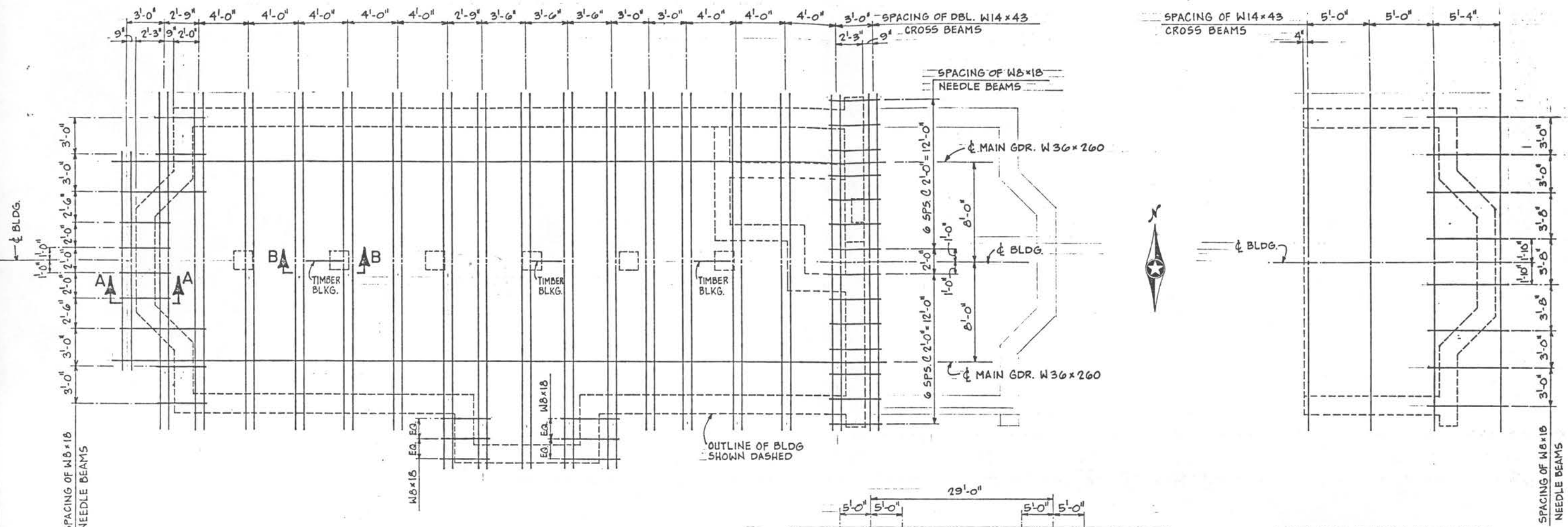
SECTION G-G

SECTION H-H

SECTION J-J

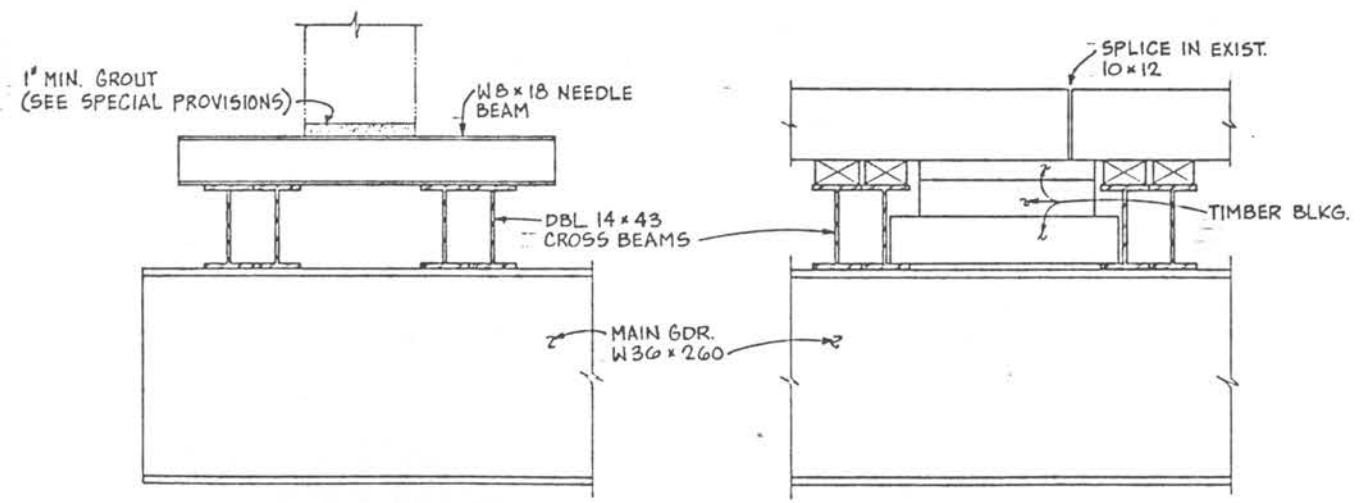
SECTION K-K

SECTION L-L



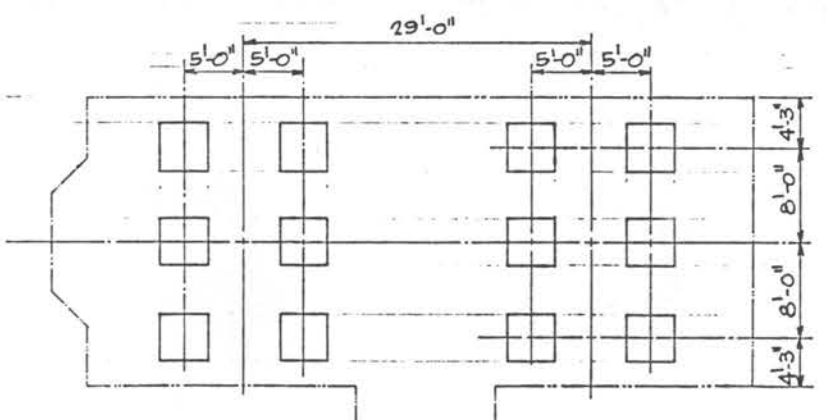
MOVING BEAM FRAMING PLAN

MOVING BEAM FRAMING PLAN FOR ADDITION

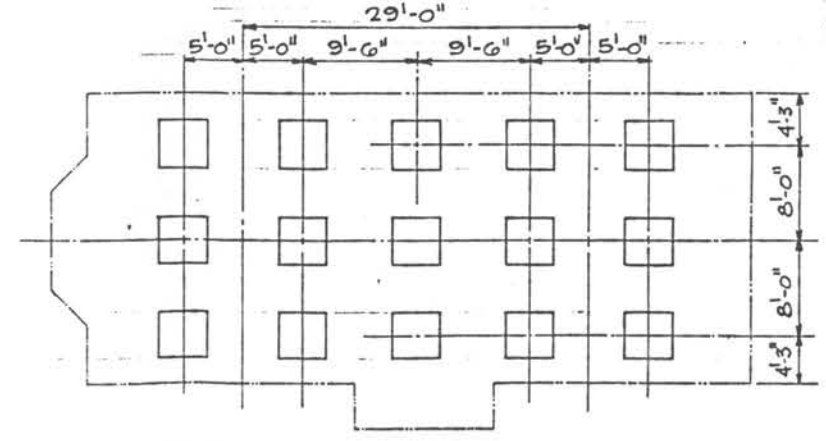


SECTION A-A

SECTION B-B



DOLLY PLAN "A"



DOLLY PLAN "B"

NOTES:

- ① SEE SPECIAL PROVISIONS FOR MOVING SEQUENCE.
- ② SEE SPECIAL PROVISIONS FOR ALTERNATE FRAMING CRITERIA.
- ③ DOLLY PLAN "A" IS SUGGESTED FOR MOVING THE MAIN STRUCTURE IN AREAS OF SIGNIFICANT GRADE CHANGE AND AROUND CORNERS.
- ④ DOLLY PLAN "B" IS REQUIRED FOR CROSSING THE EAST TUNNEL STRUCTURE.

STRUCTURAL NOTES:

DESIGN STRESSES:

- STRUCTURAL STEEL F_y = 36,000 P.S.I. MN/DOT 3306
- REINFORCING STEEL F_y = 60,000 P.S.I. MN/DOT 3301
- CONCRETE F'_c = 3000 P.S.I. FOR FOOTINGS. MN/DOT 1843
- F'_c = 3000 P.S.I. FOR WALLS & SLAB ON GRADE. MN/DOT 1846

SOIL BEARING:

2000 P.S.I.

DESIGN CODES: (ALL CODES LATEST EDITIONS)

- UNIFORM BUILDING CODE
- MINNESOTA STATE BUILDING CODE
- AMERICAN INSTITUTE OF STEEL CONSTRUCTION
- AMERICAN CONCRETE INSTITUTE
- CRSI MANUAL OF STANDARD PRACTICE (FOR PLACING AND DETAILING OF ALL REINFORCING)
- AMERICAN INSTITUTE OF TIMBER CONSTRUCTION
- NATIONAL DESIGN SPECIFICATIONS FOR STRESS GRADE LUMBER AND ITS FASTENINGS.

FOOTINGS:

WALL FOOTINGS ARE CONTINUOUS Poured CONCRETE WITH CONTINUOUS REINFORCING PLACE 3" CLEAR OF BOTTOM AND SIDES. PROVIDE 24" DIAMETER LAP AT SPLICES AND FULL CROSSING LAP AT CORNERS AND INTERSECTIONS. UNLESS OTHERWISE NOTED, WALL FOOTINGS ARE CENTERED UNDER WALLS AND PIER FOOTINGS UNDER PIERS. FOOTING ELEVATIONS SHOWN ON PLAN ARE TO TOP OF FOOTING.

POURED CONCRETE WALLS:

PROVIDE #4 x 4'-0" (2'-0" EACH LEG) CORNER BARS @ 12" O.C. HORIZONTAL REINFORCING AT OUTSIDE CORNER OF WALL WITH 3-#4 VERTICAL SUPPORT BARS. ALL OPENINGS IN WALLS LARGER THAN 12" SHALL HAVE 2-#5 AT ALL SIDES - EXTEND 2'-0" BEYOND EACH EDGE OF OPENING.

CONCRETE COVER ON REINFORCING:

- FOOTINGS: -3" CLEAR BOTTOM AND SIDES
- WALLS: -1 1/2" CLEAR OUTSIDE FACE AND 1" CLEAR INSIDE FACE.

TEMPORARY BRACING:

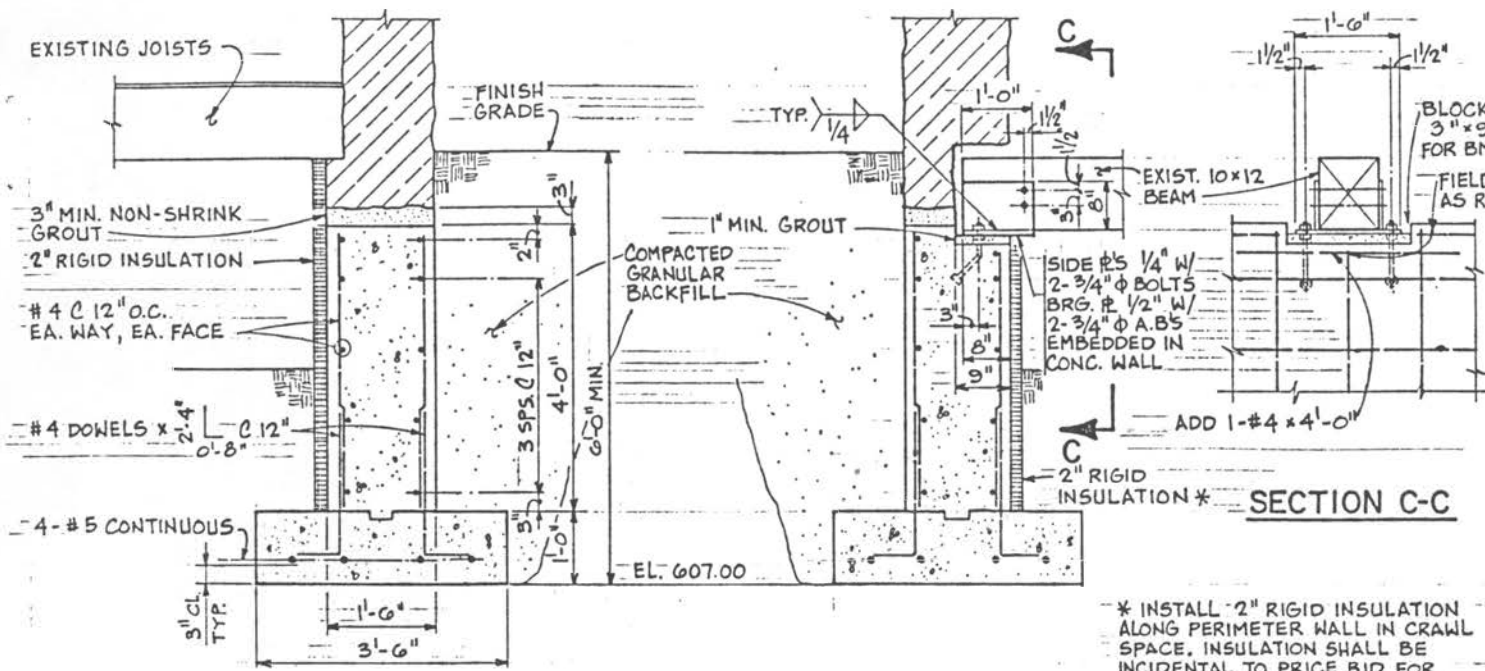
PROVIDE TEMPORARY LATERAL SUPPORT FOR ALL WALLS WHERE GRADE VARIES ON THE TWO SIDES UNTIL SLAB HAS REACHED ITS DESIGN STRENGTH.

DIMENSION LUMBER:

DIMENSION LUMBER SHALL BE NO. 2 DOUGLAS FIR OR EQUAL FOR BRACING, SHORING, ETC. ALL MEMBER SIZES GIVEN ON DRAWINGS ARE NOMINAL DIMENSIONS. ALL BOLTS FOR CONNECTION OF WOOD MEMBERS SHALL BE ASTM A307.

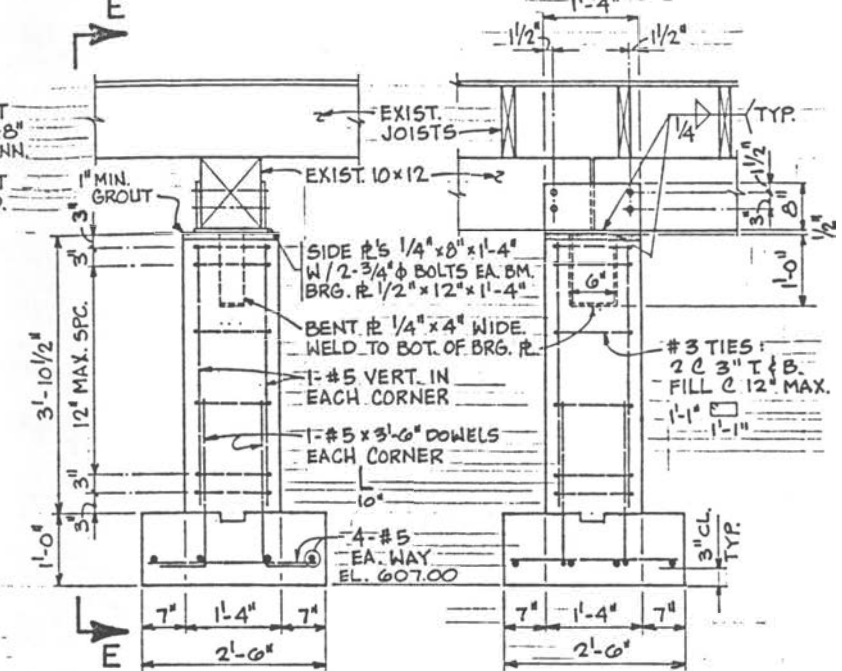
EXISTING CONDITIONS:

VERIFY ALL DIMENSIONS, ELEVATIONS, AND DETAILS OF EXISTING STRUCTURE WHERE THEY AFFECT THIS CONSTRUCTION.



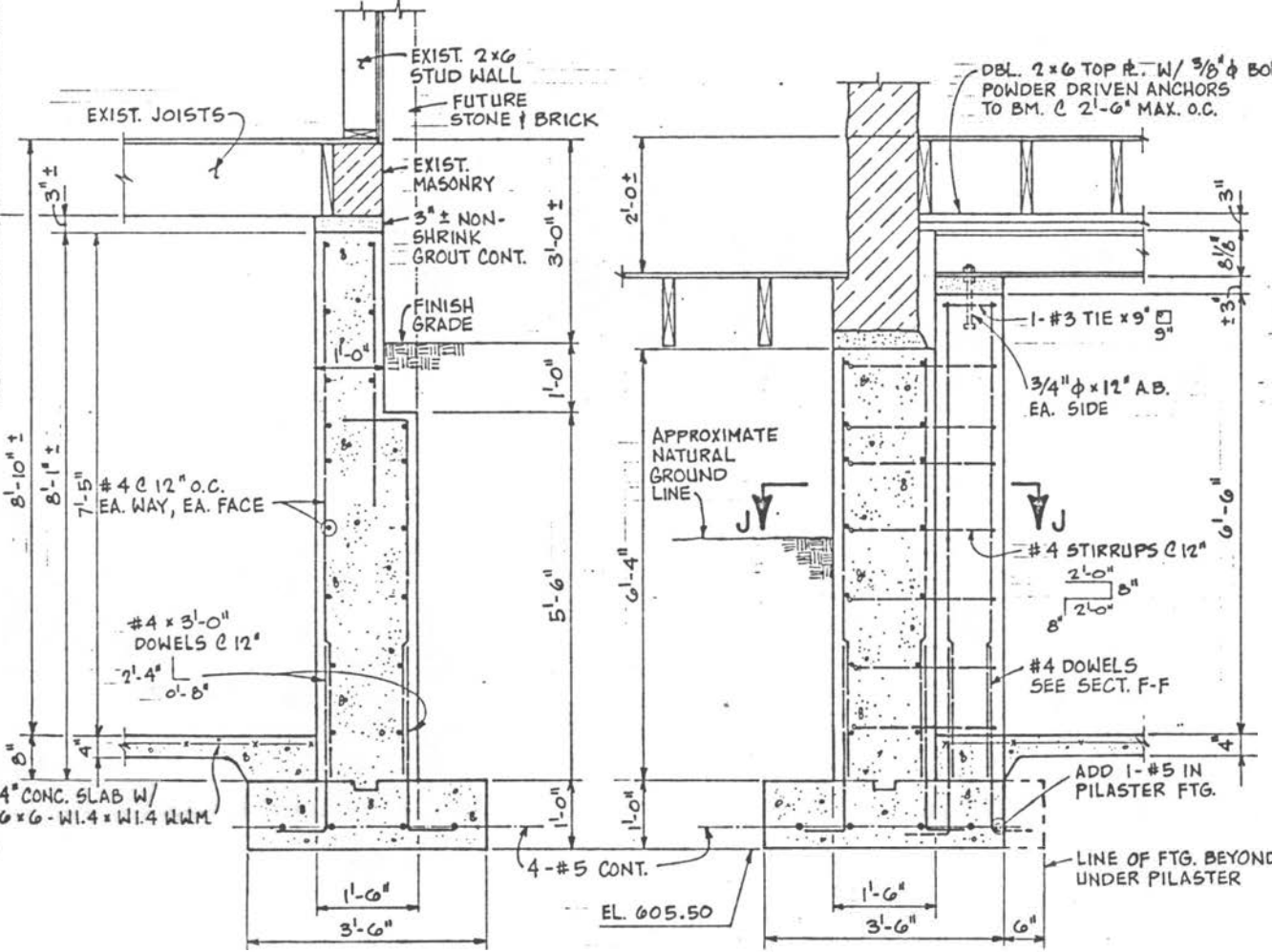
SECTION A-A

SECTION B-B



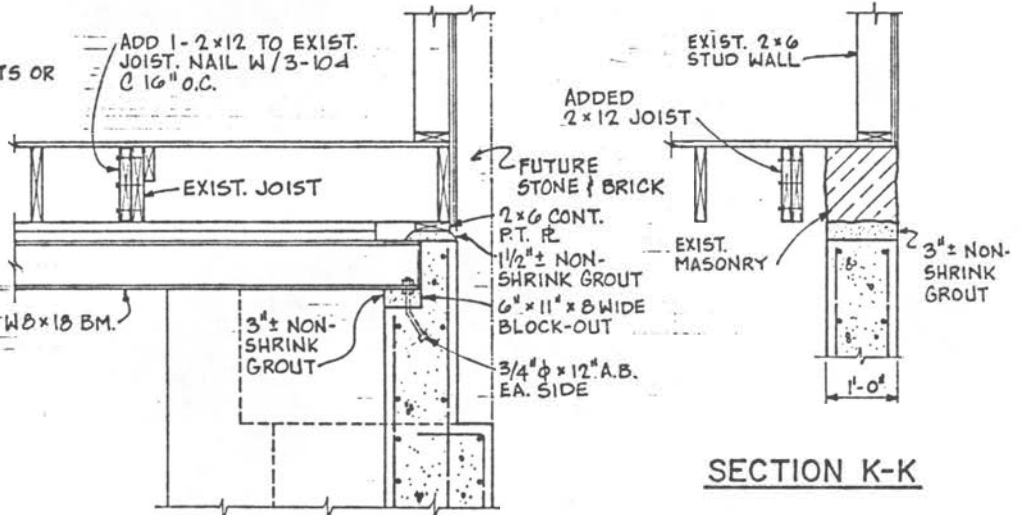
SECTION D-D

SECTION E-E



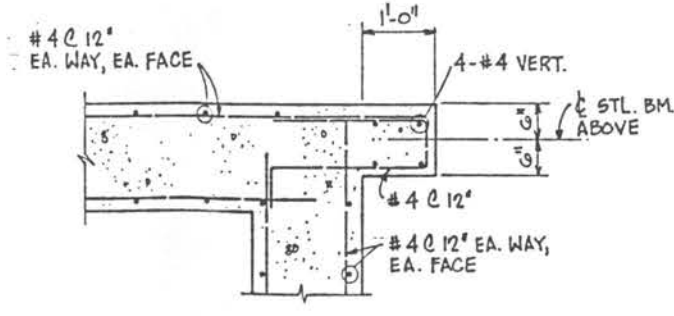
SECTION F-F

SECTION G-G



SECTION H-H

SECTION K-K



SECTION J-J

* INSTALL 2" RIGID INSULATION ALONG PERIMETER WALL IN CRAWL SPACE. INSULATION SHALL BE INCIDENTAL TO PRICE BID FOR OTHER ITEMS.

MINNESOTA DEPARTMENT OF TRANSPORTATION

CONSTRUCTION PLAN FOR RELOCATION OF ENDION DEPOT BUILDING

LOCATED ON T.H. 35 = 395 FROM FIRST AVENUE EAST TO FIFTEENTH AVENUE EAST

STATE PROJ. NO. 6982-196 (T.H. 35=395)

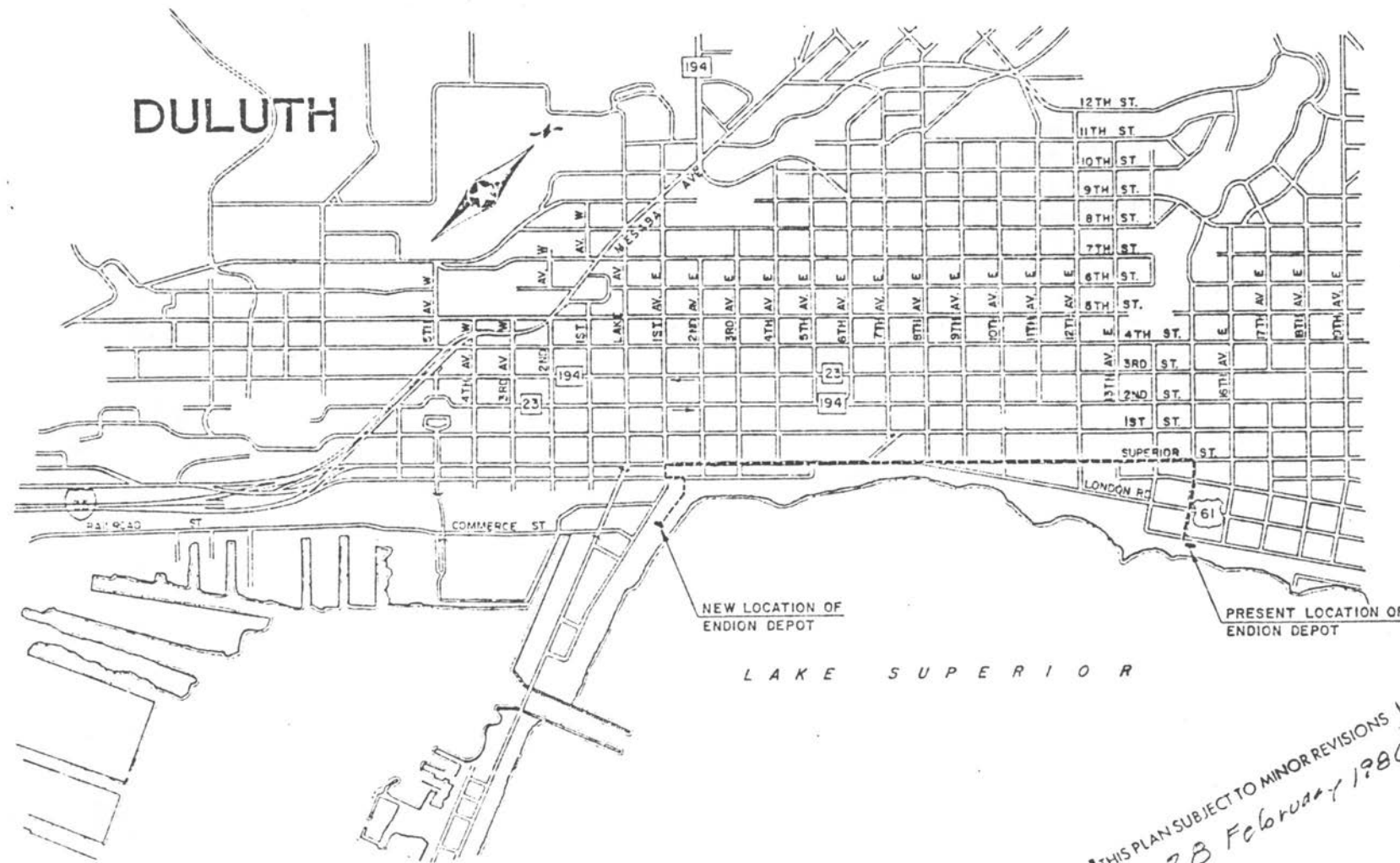
MINN. PROJ. NO. _____

GROSS LENGTH _____ FEET _____ MILES
 BRIDGES-LENGTH _____ FEET _____ MILES
 EXCEPTIONS-LENGTH _____ FEET _____ MILES
 NET LENGTH _____ FEET _____ MILES
 MILE POINT _____ TO MILE POINT _____

STATE PROJ. NO. _____

MINN. PROJ. NO. _____

GROSS LENGTH _____ FEET _____ MILES
 BRIDGES-LENGTH _____ FEET _____ MILES
 EXCEPTIONS-LENGTH _____ FEET _____ MILES
 NET LENGTH _____ FEET _____ MILES
 MILE POINT _____ TO MILE POINT _____



*THIS PLAN SUBJECT TO MINOR REVISIONS
 28 February 1986*

FED. PROJ. NO. _____

GOVERNING SPECIFICATIONS

THE 1983 EDITION OF THE MINNESOTA DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS FOR CONSTRUCTION" SHALL GOVERN

INDEX

SHEET NO.	DESCRIPTION
1	TITLE SHEET
2	ROUTE MAP & SITE PLAN
3	1ST & 2ND FLOOR BRACING & QUANTITIES
4	ELEVATIONS OF TIMBER BRACING
5	BRACING DETAILS
6	MOVING BEAM FRAMING PLAN & DETAILS
7	FOUNDATION PLANS
8	FOUNDATION DETAILS

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY REGISTERED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

DATE 12/13/85 REG. NO. 15382 ENGR. ph J. Skell

DESIGN SQUAD _____

Right of Way Approval _____	DIRECTOR, RIGHT OF WAY OPERATIONS	19
Recommended for Approval _____	TRAFFIC ENGINEER	19
Recommended for Approval _____	DISTRICT ENGINEER	19
Recommended for Approval _____	TRANSPORTATION PLANS ENGINEER	19
Recommended for Approval _____	DESIGN SERVICES DIRECTOR	19
Approved _____	ASSISTANT DIVISION DIRECTOR TECHNICAL SERVICES DIVISION	19

DEPARTMENT OF TRANSPORTATION
 FEDERAL HIGHWAY ADMINISTRATION
 APPROVED

 DIVISION ADMINISTRATOR DATE

I HEREBY CERTIFY THAT THE FINAL FIELD REVISIONS, IF ANY, OF THIS PLAN WERE MADE BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY REGISTERED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

DATE _____ REG. NO. _____

SCALES
 PLAN _____
 PROFILE _____
 INDEX MAP _____
 GENERAL LAYOUT _____

PLAN REVISIONS		
DATE	SHEET NO.	APPROVED BY

DESIGN DESIGNATION
 = _____ Design Speed _____ MPH
 = _____ Based on _____ Sight Distance
 = _____ Height of eye _____ Height of object
 = _____ % Design Speed not achieved at: _____
 = _____ % STA. _____ TO STA. _____ MPH
 = _____ % STA. _____ TO STA. _____ MPH



PROJECT LOCATION
 ST. LOUIS COUNTY
 DULUTH DISTRICT

FOR PLANS AND UTILITIES SYMBOLS SEE TECHNICAL MANUAL

STATE PROJ. NO.	AREA	JOB
	15	278



CITY OF DULUTH

DEPARTMENT OF PARKS & RECREATION
208 City Hall • Duluth, Minnesota 55802-1193
218/723-3337

September 19, 1985


John Sandahl
Minnesota Department of Transportation
1123 Mesaba Avenue
Duluth, Minnesota 55811

Dear John:

The City of Duluth intends to use the former Endion Depot at a location in the "Corner of the Lake" - near the present Saratoga Club location.

Uses include: public restrooms, convention and tourism office, tourist information center, office location for future programmers of planned activities, special events and promotions for Canal Point Area, and as a boarding-unboarding station for proposed Lisbon Trolley, wheeled streetcar or excursion train.

Sincerely,


JAMES W. McCORD
Director of Parks and Recreation

JWM:bof:c



FOUNDED IN 1849

MINNESOTA HISTORICAL SOCIETY

690 Cedar Street, St. Paul, Minnesota 55101 • (612) 296-6126

January 15, 1986

Mr. Clement P. Kachelmyer
Preliminary Design Engineer
Minnesota Department of Transportation
612-H Transportation Building
St. Paul, Minnesota 55155

Dear Mr. Kachelmyer:

Re: Endion Depot, Move to New Site
Duluth, St. Louis County
MHS Referral File Number: P-820

Thank you for providing this office with the revised site plan for the Endion Depot at its proposed new location. It has been reviewed pursuant to the responsibilities given the State Historic Preservation Officer by the National Historic Preservation Act of 1966 and the Procedures of the National Advisory Council on Historic Preservation (36CFR800), and according to the provisions of the 1977 Memorandum of Agreement governing this project.

It is our opinion that the location and revised site plan provides an adequate setting for the structure. Its orientation vis-a-vis the proposed roads and trolley line recalls the original trackside setting.

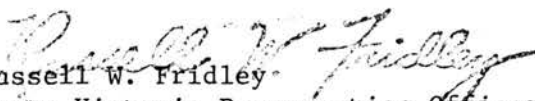
This office has also reviewed the archaeological potential of the original location and the proposed new location. The depot was originally built on bare rock on the floor of a new railroad cut. This means there are no deposits earlier than those occasioned by the construction of the depot and the adjacent rail line. We do not believe that such deposits are likely to yield significant information about the construction of the depot, and therefore conclude there is no reason to conduct archaeological studies either before the excavations preparatory for the move or after the move. Also, the new site appears to have no significant archaeological potential. Test borings show a deep layer of fill composed mostly of sawmill trash. We do not believe this fill has any significant archaeological value. There is no reason to expect archaeological materials beneath the fill, which was placed in Lake Superior shallows.

In summary, we believe the new location and siting provide an acceptable new setting for the Depot, and the move will not affect significant archaeological resources.

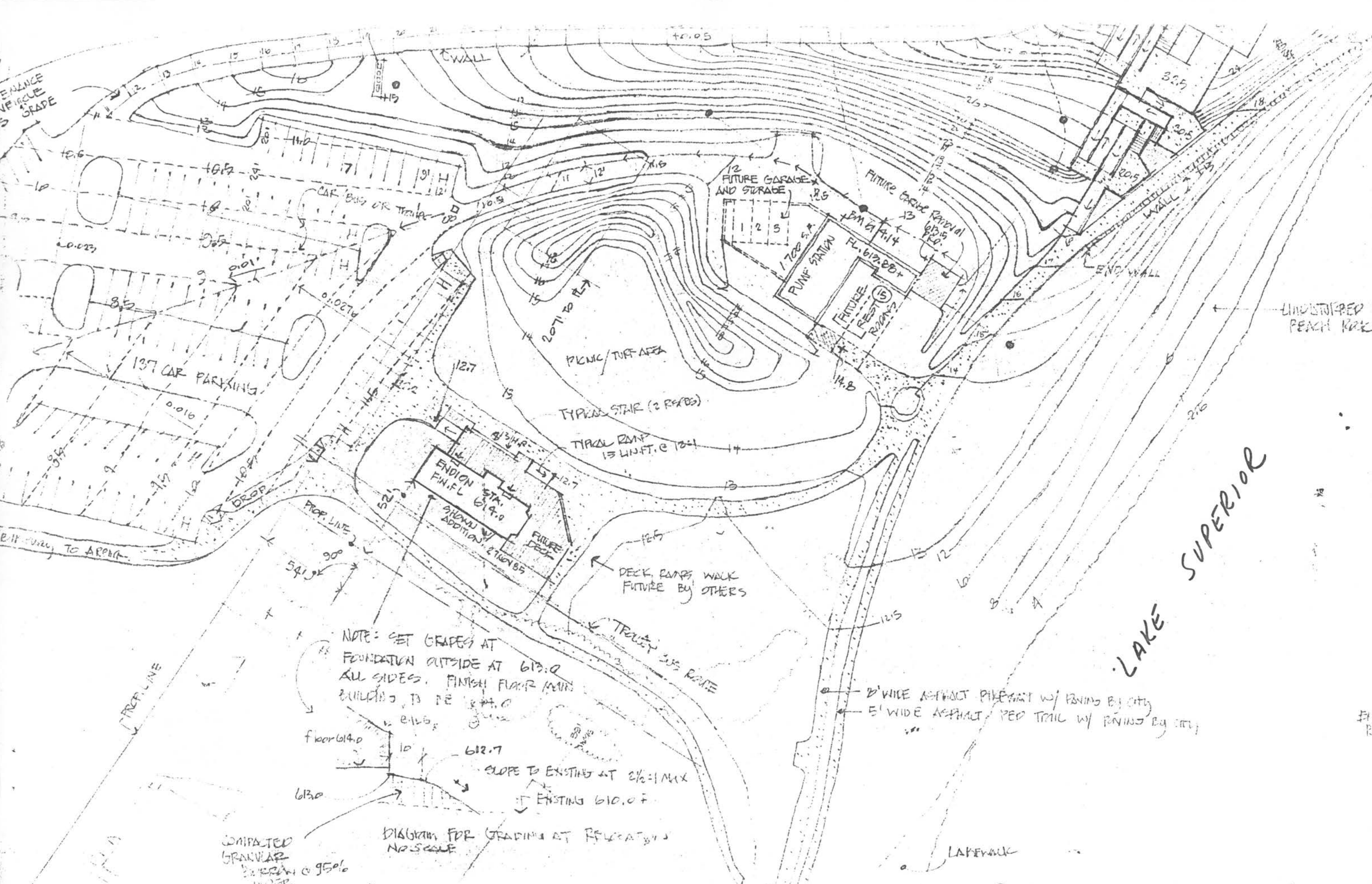
If you have any questions regarding this project, please contact Ted Lofstrom, Fort Snelling History Center, St. Paul, Minnesota 55111, (612) 726-1171.

Thank you for your continued close attention to historical values in this project.

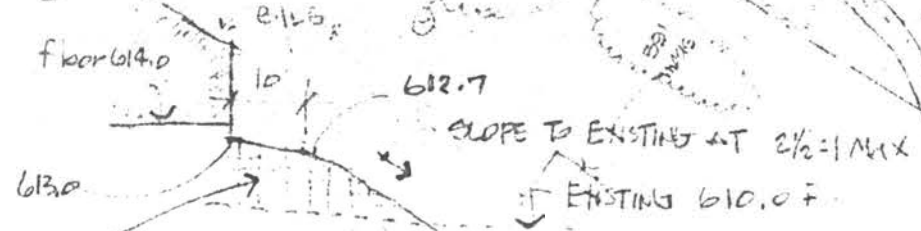
Sincerely,


Russell W. Fridley
State Historic Preservation Officer

RWF:dmb



NOTE: SET GRADES AT FOUNDATION OUTSIDE AT 613.0 ALL SIDES. FINISH FLOOR MAIN BUILDING TO BE 614.0



COMPACTED GRANULAR SUBGRADE @ 95%

DIAGRAM FOR GRADING AT RELEVATIONS NO SCALE

PROPOSED CORNER OF THE LAKE PARK

ENDION PASSENGER DEPOT
1504 South Street
Duluth
St. Louis County
Minnesota

HAER NO. MINN-9

PHOTOGRAPHS
HISTORICAL AND DESCRIPTIVE DATA

Historic American Engineering Record
National Park Service
Rocky Mountain Regional Office
Department of the Interior
P.O. Box 25287
Denver, Colorado 80225

Endion Passenger Depot
1504 South Street
Duluth
St. Louis County
Minnesota

HAER NO. MINN-9

PHOTOGRAPHS

HISTORICAL AND DESCRIPTIVE DATA

Historic American Engineering Record
National Park Service
Rocky Mountain Regional Office
Department of the Interior
P.O. Box 25287
Denver, Colorado 80225

HISTORIC AMERICAN ENGINEERING RECORD

Endion Passenger Depot

Location: 1504 South Street at 15th Ave. East in
Duluth, Minnesota
Latitude: 46° 47' 59"
Longitude: 92° 04' 36"
USGS Quadrangle: Duluth Minnesota

Present Owner: Minnesota Department of Transportation
St. Paul, Minnesota 55155

Present Occupant: Vacant

Statement of Significance: The Endion Depot is significant as the last of the small passenger depots to function in Duluth. Its jewel-like character results from the fine use of stone and brick architectural detailing and craftsmanship. Further, it stands as an excellent example of I. Vernon Hill's early design and as a building which contributed to the development of Hill's distinctive impact on Duluth architecture in the early twentieth century.

Documentation Prepared by: Paul J. LaTour, Clement P. Kachelmyer
Minnesota Department of Transportation,
January 1986

ROCKY MOUNTAIN REGION
HISTORIC AMERICAN ENGINEERING RECORD
INDEX TO PHOTOGRAPHS

ENDION PASSENGER DEPOT
1504 South Street
Duluth
St. Louis County
Minnesota

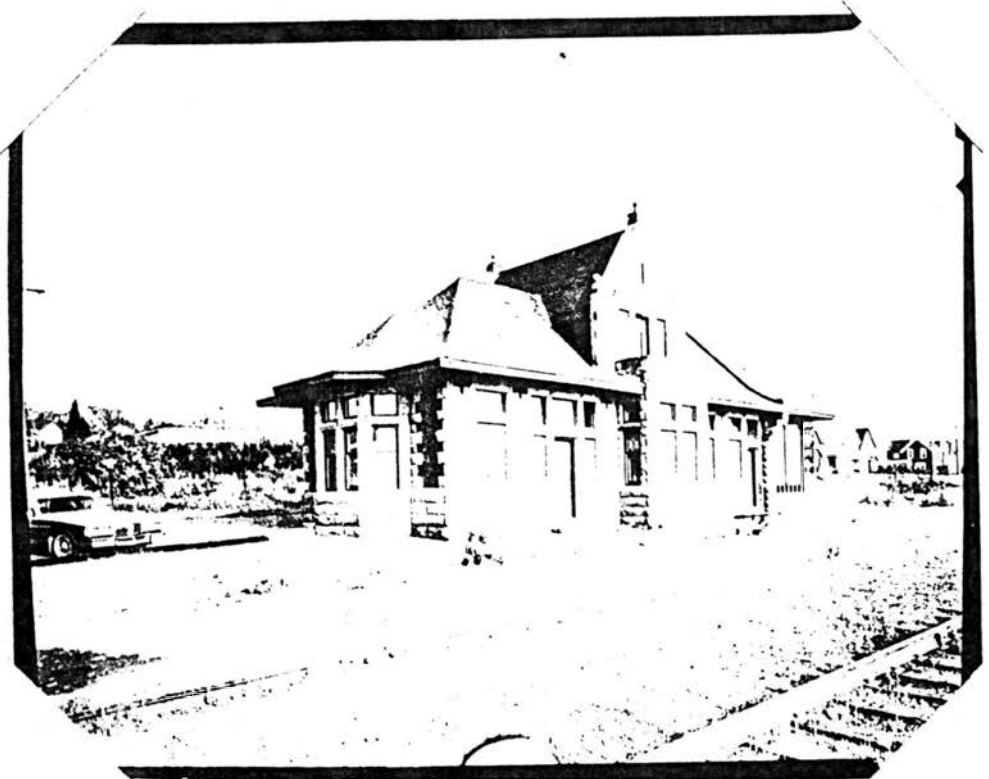
HAER NO. MINN-9

Documentation: 14 photos of building (1985)
4 photocopies of historic photos (1910 to 1982)
5 data pages (1986)

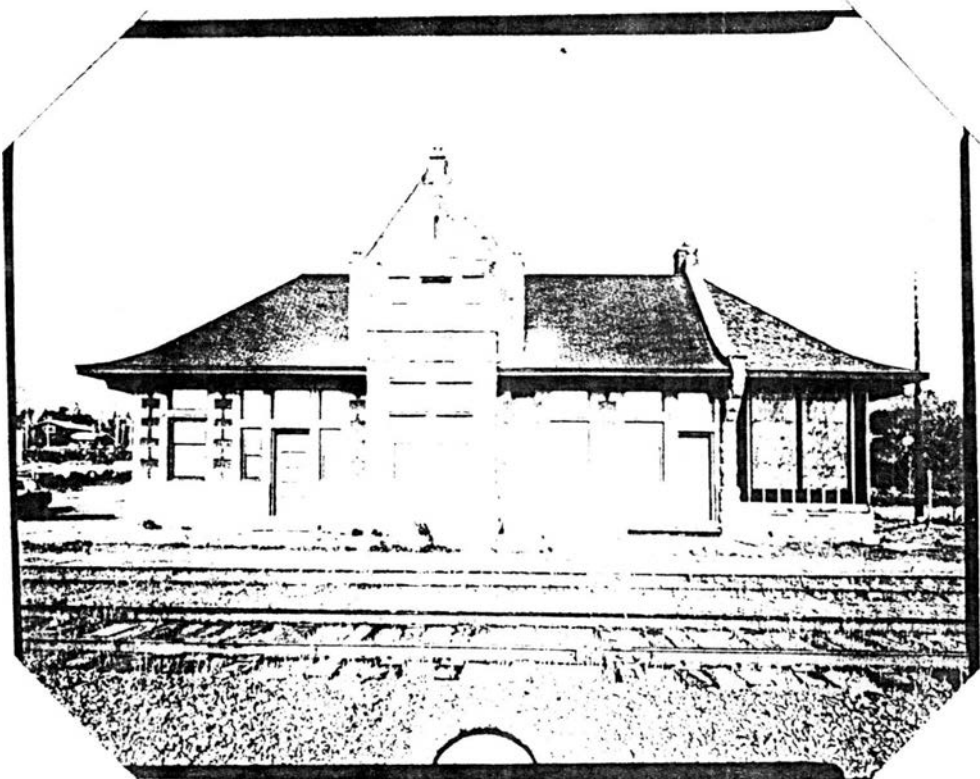
Dale R. Tresler, Photographer - exterior October 1985
David R. Gonzalez, Photographer - interior November 1985

MINN-9-1 VIEW TO NORTH OF EAST FRONT.
MINN-9-2 VIEW TO NORTHWEST OF EAST FRONT.
MINN-9-3 VIEW TO EAST OF WEST REAR.
MINN-9-4 VIEW TO SOUTHEAST OF WEST REAR.
MINN-9-5 VIEW TO SOUTH OF NORTH ADDITION.
MINN-9-6 VIEW TO SOUTHWEST OF NORTH ADDITION.
MINN-9-7 VIEW TO WEST OF SOUTHWEST ROOM, FORMERLY PORTION OF
PASSENGER WAITING ROOM.
MINN-9-8 VIEW TO NORTHWEST IN CENTER ROOM, FORMERLY PORTION
OF PASSENGER WAITING ROOM.
MINN-9-9 VIEW TO SOUTHWEST IN MAIN ROOM, FORMERLY PORTION OF
DEPOT OFFICE. CENTER DOOR OPENS TO CENTER ROOM.
MINN-9-10 VIEW TO NORTHWEST IN MAIN ROOM, FORMERLY PORTIONS OF
DEPOT OFFICE AND TRAINMEN'S WAITING ROOM.
MINN-9-11 VIEW TO NORTHWEST IN NORTH ADDITION.
MINN-9-12 VIEW TO NORTHEAST IN BASEMENT FURNACE ROOM.
MINN-9-13 VIEW TO SOUTHEAST TOWARDS OLD COAL ROOM.
MINN-9-14 VIEW TO SOUTHEAST IN OLD COAL ROOM.
MINN-9-15 Photocopy of photograph (original print property of
Edward Schafer Associates, Inc., Duluth, Minnesota)
ca. 1910
DULUTH & IRON RANGE RAILROAD LOCOMOTIVE #46 (BUILT
IN 1888) WITH LOG TRAIN AT ENDION DEPOT.

- MINN-9-16 Photocopy of photograph (original print property of Minnesota Department of Transportation) ca. 1981
VIEW TO NORTH, SHOWING ENDION DEPOT WITH ATTACHED
WOOD FRAME FREIGHT HOUSE.
- MINN-9-17 Photocopy of photograph (original print property of Minnesota Department of Transportation) ca. 1981
VIEW OF EAST PINNACLE OF ENDION DEPOT SHOWING YEAR
BUILT.
- MINN-9-18 Photocopy of photograph (original print property of Edward Schafer Associates, Inc., Duluth, Minnesota)
February 1982
VIEW TO NORTH OF EAST FRONT OF ENDION DEPOT.



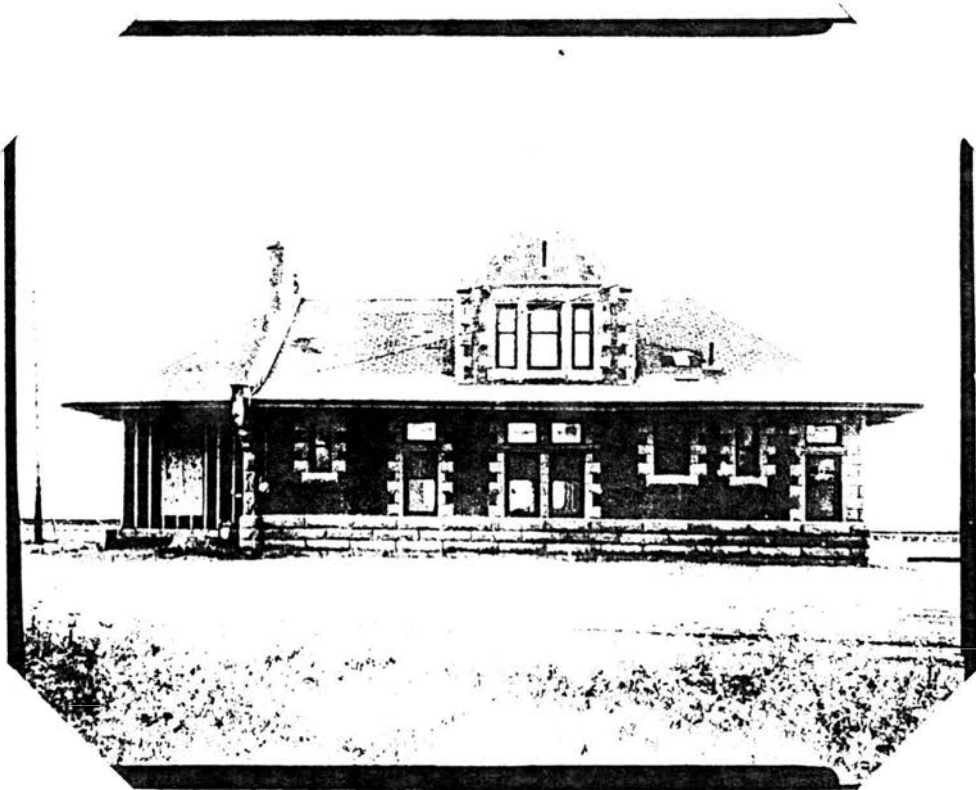
HISTORIC AMERICAN ENGINEERING RECORD
SEE INDEX TO PHOTOGRAPHS FOR CAPTION
HAER NO. MINN-9-1



HISTORIC AMERICAN ENGINEERING RECORD
SEE INDEX TO PHOTOGRAPHS FOR CAPTION
HAER NO. MINN-9-2



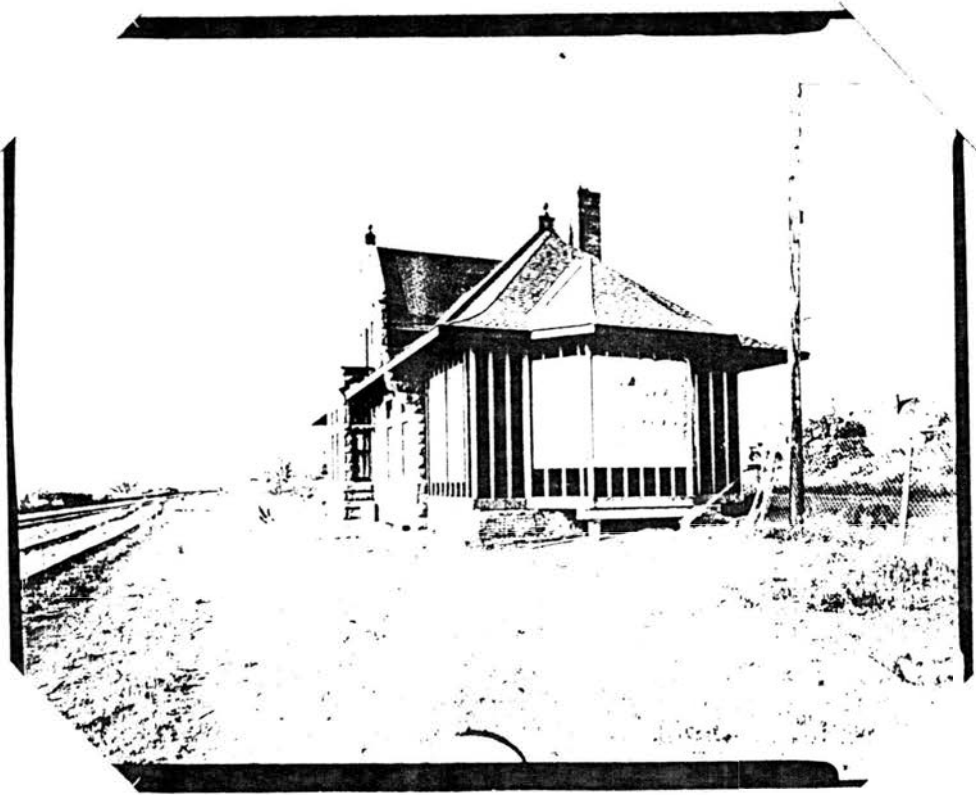
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SEE INDEX TO PHOTOGRAPHS FOR CAPTION
HAER NO. MINN-9-3



HISTORIC AMERICAN ENGINEERING RECORD
SEE INDEX TO PHOTOGRAPHS FOR CAPTION
HAER NO. MINN-9-4



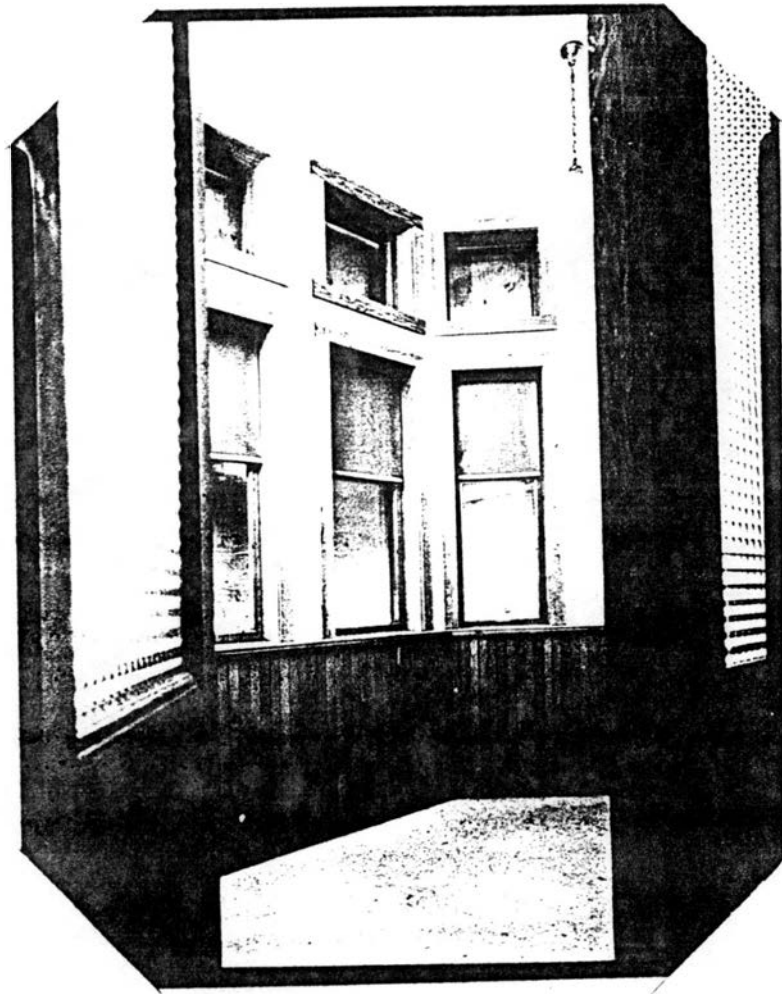
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SEE INDEX TO PHOTOGRAPHS FOR CAPTION
HAER NO. MINN-9-5

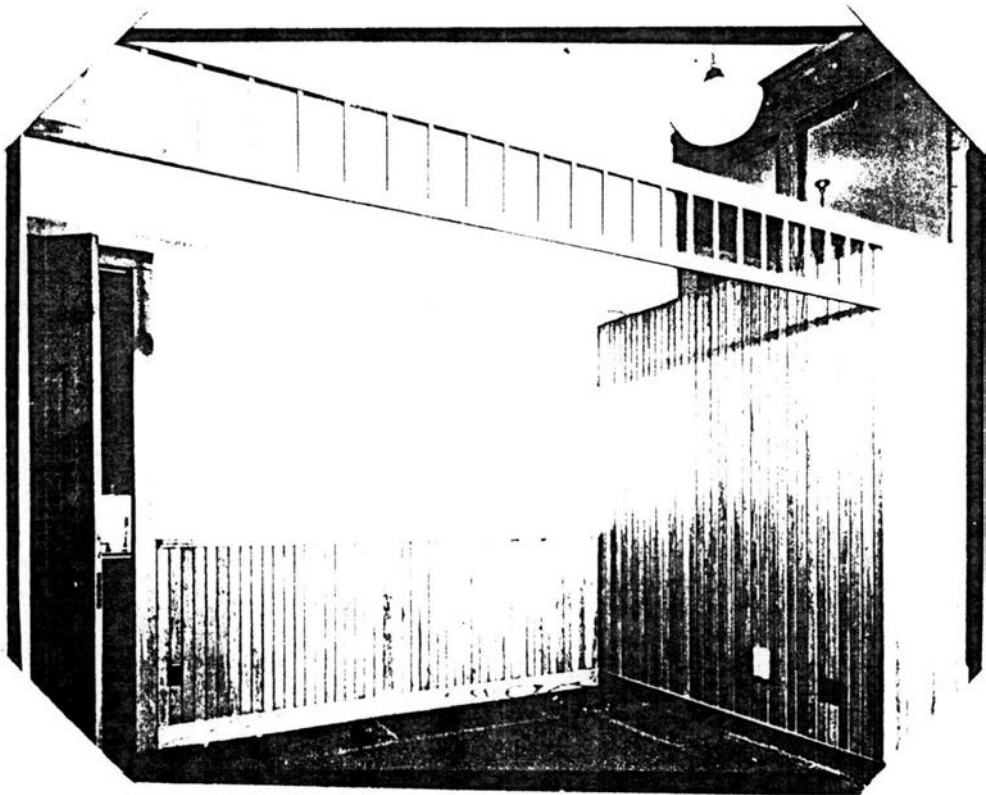


HISTORIC AMERICAN ENGINEERING RECORD
SEE INDEX TO PHOTOGRAPHS FOR CAPTION
HAER NO. MINN-9-6

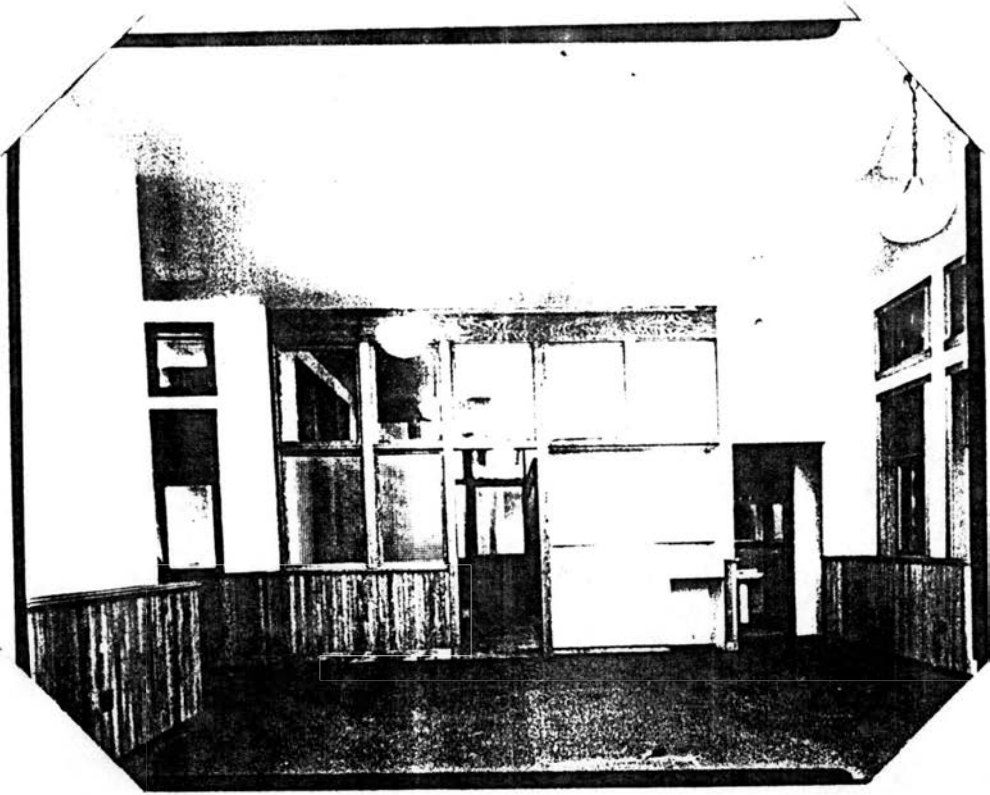
HISTORIC AMERICAN ENGINEERING RECORD
SEE INDEX TO PHOTOGRAPHS FOR CAPTION

HAER NO. MINN-9-7

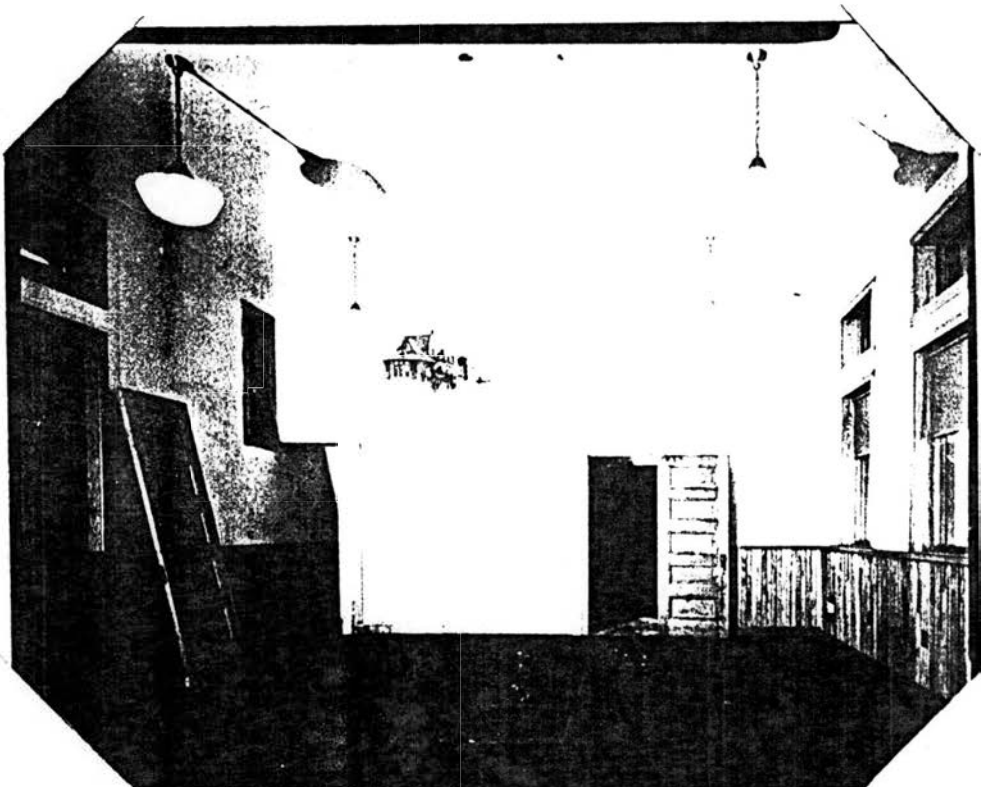




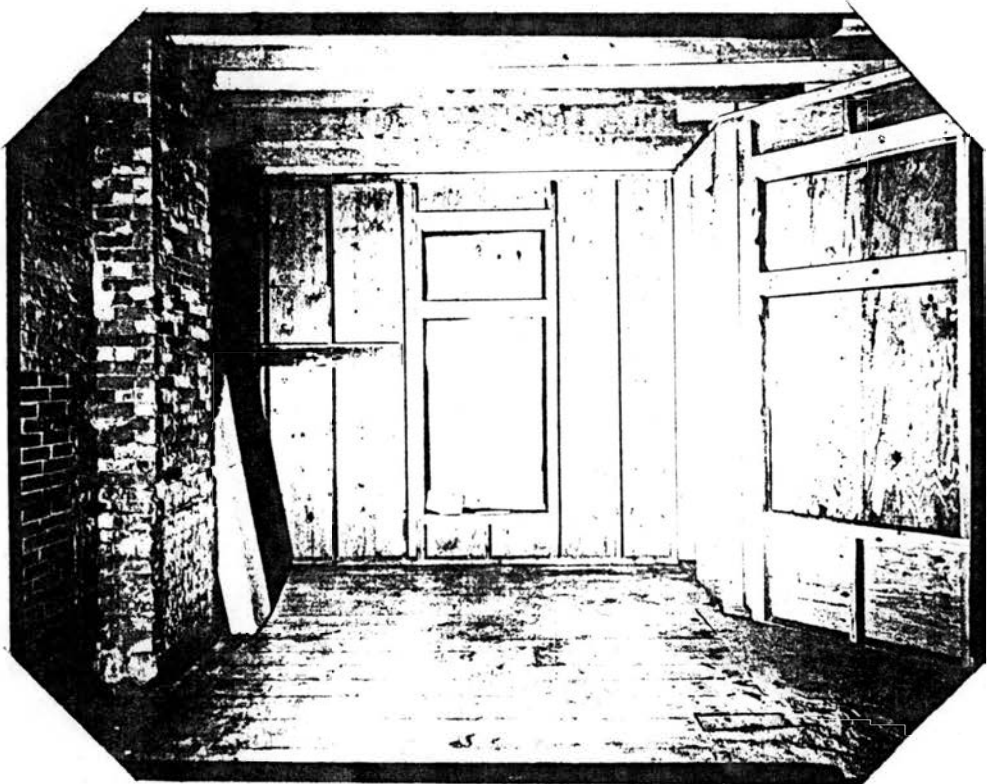
HISTORIC AMERICAN ENGINEERING RECORD
SEE INDEX TO PHOTOGRAPHS FOR CAPTION
HAER NO. MINN-9-8



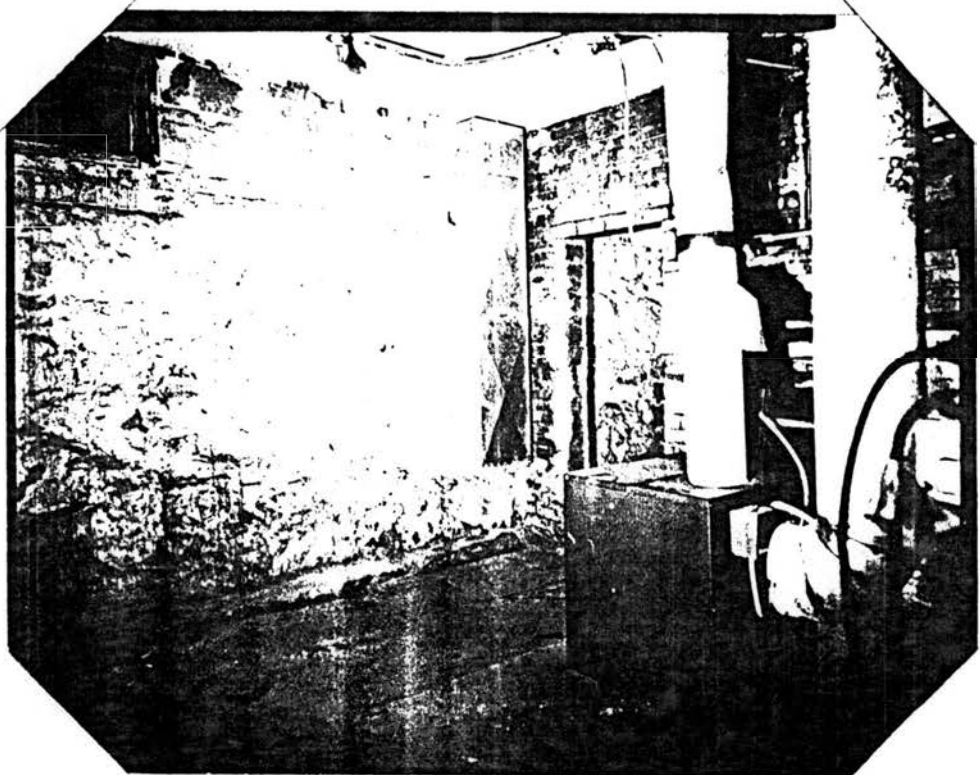
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HAER NO. MINN-9-10



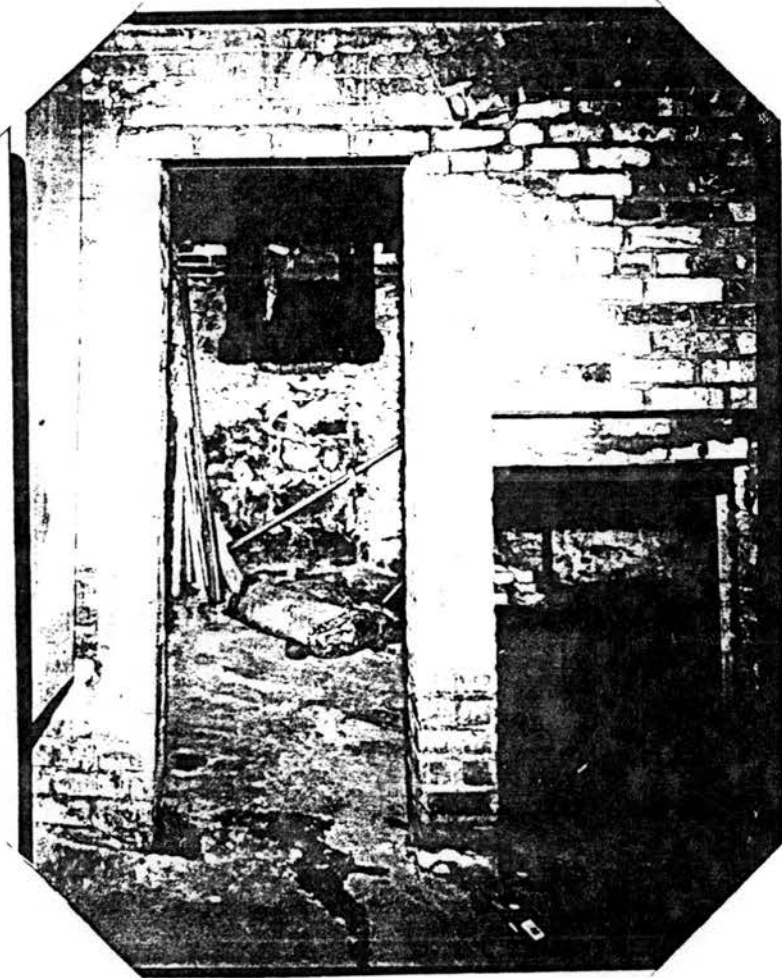
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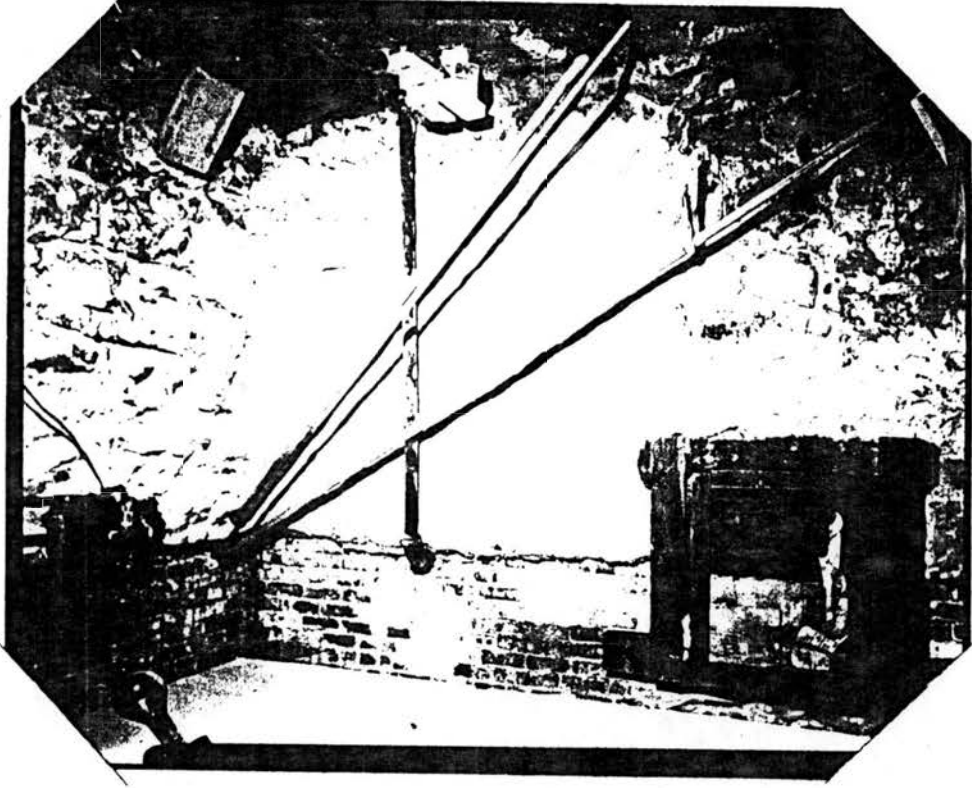
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HAER NO. MINN-9-14





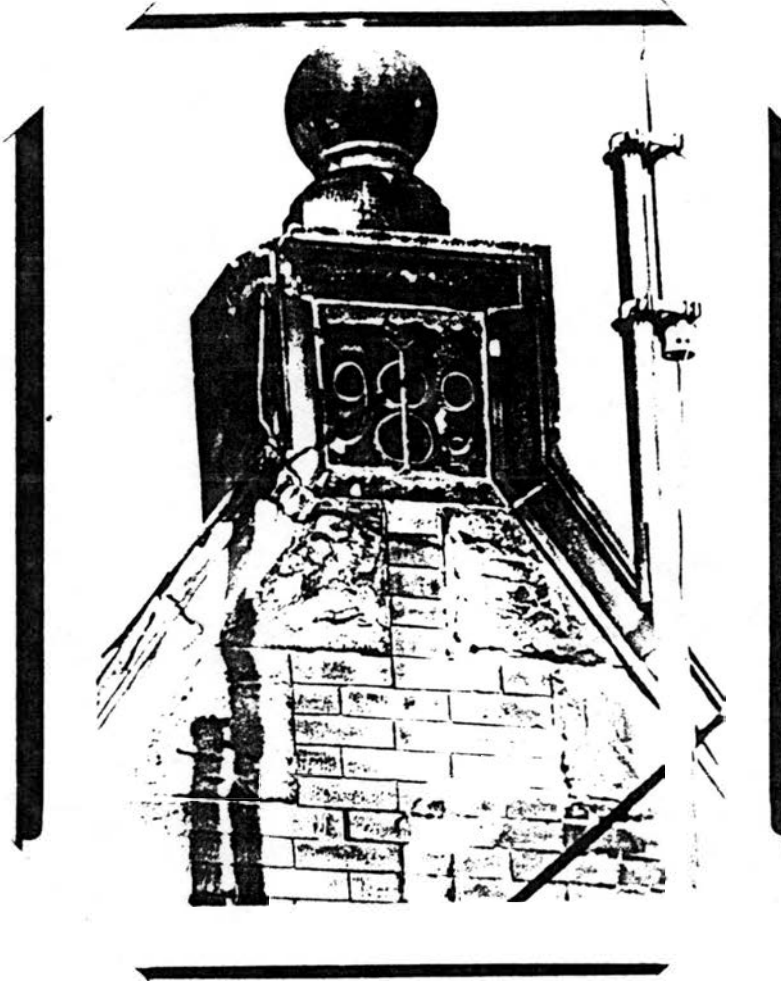
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HISTORIC AMERICAN ENGINEERING RECORD
SEE INDEX TO PHOTOGRAPHS FOR CAPTION
HAER NO. MINN-9-16

HISTORIC AMERICAN ENGINEERING RECORD
SEE INDEX TO PHOTOGRAPHS FOR CAPTION

HAER NO. MINN-9-17



HISTORIC AMERICAN ENGINEERING RECORD
SEE INDEX TO PHOTOGRAPHS FOR CAPTION

HAER NO. MINN-9-18



HISTORICAL INFORMATION

Date of Erection: 1899

Architect: Tenbusch and Hill (Gearhard A. Tenbusch and I. Vernon Hill)

Historical Narrative:

The City of Duluth, in its growth and expansion, absorbed several small communitites that were individual entities in the early days at the head of the lake. One of these early settlements was the Town of Endion, originally platted by the surveyor Elias Martin and filed on record December 5, 1856. (Endion is a Chippewa Indian name meaning "my", "yours", or "his home".) A portion of the Town of Endion was sub-divided and purchased by various owners in November 1870, the original private owner of lots 1 and 2 being William Branch, a prominent businessman of the time. Through sales and title transfers, the fledgling railroad known as the Duluth and Iron Range (D&IR) accuired the ownership to lot 1 in May 1886, and lot 2 in August 1886.

The railroad company laid track and began servicing the North Shore communities and Two Harbors in late 1886. One of the stops along the route was the Town of Endion (now part of the City of Duluth), which was the first stop outside of downtown Duluth. This railroad service performed an important role in the development of the City's east end neighborhoods. To satisfy the passenger and freight needs of the community of Endion the railroad decided in 1899 to construct a passenger depot with an attached freight shed. To design it, they commissioned the newly formed firm of Tenbusch and Hill (Gearhard A. Tenbusch and I. Vernon Hill) of Duluth.

The depot of Kettle River sandstone and pressed brick was executed in a variant on the Romanesque style. The basic concept of projecting gables which crossed in a transept fashion had been used by other designers of the later nineteenth century, but was developed by Hill into a highly aggressive and personal style which marked his major works in Duluth between 1901 and 1903. The design of the depot was the first of these major works in the development of Hill's mature architectural career.

The building was constructed by David Hood at a cost of approximately \$10,000 and it was put into service at the turn of the century.

At this time in Duluth's history, railroads proliferated throughout the region to serve the expanding industries and communities of northeastern Minnesota. Among these was the Duluth, Missabe and Northern (DM&N) serving the lumbering and iron ore industries. Consolidation of the DM&N and the D&IR railroad companies was considered for several years and a lease-merger arrangement was finally worked out and fulfilled on January 10, 1930. At the time of the merger the name for the new company resulted from the combination of names of the two former railroads. This was the birth of the Duluth, Messabe and Iron Range Railroad Company (DM&IR). The ownership of the depot transferred to the new company with the merger.

When rail passenger travel was at its peak, six trains arrived and departed daily from Endion depot and four offered cafe-parlor service. Regularly scheduled passenger service on the Missabe ended

with the departure of train No. 6 from Endion on July 15, 1961. The building was used for freight until 1978, when it was closed.

Due to its architectural style, the renowned designers of the building, and its ties to early Minnesota rail transportation, the Endion Passenger Depot was nominated to the National Register of Historic Places. After review by the appropriate authorities, the nomination of the building was accepted and it was entered onto the National Register on April 16, 1975.

The DM&IR petitioned the Minnesota Department of Transportation to retire the depot building from railroad use in 1977 because business at the station had declined to a point that was uneconomical. Following a hearing, the petition was granted and the building closed in March 1978.

On January 17, 1983 the DM&IR Railroad Co. sold the Endion Depot to Edward Schafer Associates, Inc., a Duluth architectural firm, for use as their office. After reviewing Schafer's proposal for the use of the depot as offices for the architectural firm, Charles Nelson, historical architect for the Minnesota Historical Society wrote "I am impressed with your proposal as being extremely sensitive to the historic character of the depot," and concluded, "the Endion Depot will remain the jewel which architects Tenbusch and Hill intended it to be".

The Minnesota Department of Transportation purchased the depot and its site (Northerly 35 feet of Lots 1, 2 and 3, Block 14, Endion Division) from Edward Schafer Associates, Inc. on September 27, 1985 with the land to be used for right-of-way for Interstate Highway 35.

ARCHITECTURAL INFORMATION

The Depot is a small building, measuring only seventy feet by twenty-four feet. The highest point at the central gable rises nearly thirty-seven feet above ground level. The foundation and stone trim are of locally quarried Kettle River sandstone of buff color which contrasts the pressed red brick body of the building. All coping and door and window surrounds are of Kettle River sandstone. Essentially the building forms a cruciform plan at the roof due to the intersection of gables although it is basically rectangular in floor plan. A central stone bay which housed the station master's office projects on the east front of the building. Originally it had been roofed in Bangor slate, but this roofing has been replaced with brown asphalt shingle roofing. Original metal ridges and caps have not been retained. A frame freight (LCL) shed formerly attached to the northeast facade was removed during the summer of 1974 due to its dilapidated condition.

Stylistically, the Depot may be classified as a mode of the Richardsonian style popular during the 1880's and 1890's. The rusticated stonework in contrast to the flat surface of the brick, stone window surrounds and the conception of projecting gables capped with stone is essentially Romanesque, although the smallness of scale contributes to the lighter, more delicate appearance rather than the heaviness of the earlier Romanesque.

The interior of the Depot originally consisted of two areas: a passenger waiting room with rest room facilities and the station master's office, which was partitioned to also include a vault and a trainmen's waiting room.

After Edward Schafer purchased the Depot he carefully repaired, restored and remodeled the building with concern for its character. The first floor was altered slightly by construction of partitions to provide a reception area, a conference room, offices and drafting area. Schafer had the original woodwork and wainscoting stripped of the accumulation of varnish and salvaged as much as he could to decorate the offices as they once were. Windows were replaced with energy-efficient triple-glazed windows, but because they were of the same type of double-hung windows with bronze trim, the original appearance was retained. The exterior was cleaned by a specialist approved by the Historical Society and new mortar applied. On the east end of the building, where a long warehouse was once attached, Schafer built an extension which complements the west end of the building with the same style hip roof and bay window.

BIBLIOGRAPHY

- Final Interstate Highway 35 Section 4(F) Statement and Historic and Cultural Preservation Report - October 1977
- National Register of Historic Places Inventory-Nomination Form - December 1974
- Missabe Iron Ranger - Spring 1980
- Minneapolis Tribune - July 7, 1980
- Duluth Herald - August 21, 1979

92W
67 NR Data Sheet

DATE: 3/30/77
 Reviewer INITIALS: [Signature]
 NR DOE April 16, 1975

NAME AS IT APPEARS IN FEDERAL REGISTER: Endion Passenger Depot

OTHER NAMES:

LOCATION:

STREET & NUMBER 1504 South St.

CITY/TOWN Duluth

CONGRESSIONAL DISTRICT 8th

STATE Minnesota

VICINITY OF

COUNTY code
 St. Louis 137

OWNER OF PROPERTY: (Circle) PRIVATE STATE LOCAL GOV'T MUNICIPAL COUNTY OTHER

ADMINISTRATOR (underline)

FEDERAL (AGENCY NAME):

NPS REGION: (CIRCLE) N.ATLANTIC MID ATLANTIC SOUTHEAST MIDWEST
 SOUTHWEST ROCKY MOUNTAIN WEST PACIFIC NORTHWEST

FEATURES:

<u>INTERIOR</u>	<u>EXTERIOR</u>	<u>ENVIRONS</u>
- Substantially intact-1	- Substantially intact-2	- Substantially intact-3
- unknown - 4	- unknown - 5	- unknown - 6
- not applicable - 7	- not applicable - 8	- Not applicable - 9

-Interior, exterior, environs not intact-0

CONDITION -	-EXCELLENT	-DETERIORATED	-UNALTERED	-ORIGINAL SITE
	-GOOD	-RUINS	-ALTERED	-MOVED
	-FAIR	-UNEXPOSED	-Reconstructed	-Unknown
		-Unexcavated	-Excavated	

ACCESS - Yes-restricted Yes-unrestricted No access Unknown

historic district? YES NO

WITHIN NATIONAL REGISTER HISTORIC DISTRICT? YES NO
 IF YES, NAME:

WITHIN NATIONAL HISTORIC LANDMARK? YES NO
 IF YES, NAME:

ADAPTIVE USE: YES NO Saved? YES NO

FUNCTION(S): (use vocabulary words)

then- transportation
 now- transportation

SIGNIFICANCE:

- | | | | | |
|--------------------------|---------------|-------------------------|----------------------|-----------------|
| -ARCHAEOLOGY-PREHISTORIC | -CONSERVATION | -LANDSCAPE ARCHITECTURE | -RELIGION | -entertainment |
| -ARCHAEOLOGY-HISTORIC | -ECONOMICS | -LAW/Gov't/politics | -SCIENCE | -health |
| -AGRICULTURE | -EDUCATION | -LITERATURE | | -recreation |
| -ARCHITECTURE | -ENGINEERING | -MILITARY | -SOCIAL/HUMANITARIAN | -settlement |
| -ART | -EXPLORATION | -MUSIC | | -socio/cultural |
| -COMMERCE | -INDUSTRY | -PHILOSOPHY | -TRANSPORTATION | -urban & commun |
| -COMMUNICATIONS | -INVENTION | -POLITICS/GOVERNMENT | -OTHER (SPECIFY) | planning |

Claims

"first" YES NO "oldest" YES NO "only" YES NO

Remaining small passenger station in Duluth is general

Jacobellman

ARCHITECTURAL STYLE:

2

architect/m.builder: Gearhard A Tenbusch,

engineer:

landscape/garden designer: ~~and~~ I. Vernon Hill

artist/artisan:

interior decorator:

3

builder/contractor:

ETHNIC GROUP:

NAMES:

personal

(label role & appropriate date)

events

institutional

DATES:

DATE OF CONSTRUCTION (Specific date or 1/4 of century):

1899

DATE(S) OF "MAJOR" ALTERATIONS:

HISTORICALLY SIGNIFICANT DATE(S):

SOURCE:

(OF NOMINATION)

PRIVATE

STATE

LOCAL GOV'T

MUNICIPAL

COUNTY

OTHER

FEDERAL AGENCY:

ACREAGE:

(to nearest tenth of an acre)

less than 1

COMMENTS:

(include architectural information here)

Brick, sandstone; 1--2 stories, rectangular, gable on flared hipped roof, projecting front and side polygonal stone bays, 2-story center gabled section with parapet and ball finials, prominent stone quoins; original interior woodwork. Jacobellman.

SIGNIFICANCE:

(maximum two sentences)

Dakota's last remaining small passenger station in operation; early design of noted area architect I. Vernon Hill.

Minnesota Historical Society
State Historic Preservation Office
345 Kellogg Blvd West, St. Paul, Minnesota 55102
651/259-3451



TO: Carol Shull, Keeper
National Register of Historic Places

FROM: Denis P. Gardner

DATE: October 10, 2013

NAME OF PROPERTY: Duluth, Missabe and Iron Range Depot

COUNTY AND STATE: St. Louis County, Minnesota

SUBJECT: National Register:

- Nomination
- Multiple Property Documentation Form
- Request for determination of eligibility
- Request for removal (Reference No.)
- Nomination resubmission
- Boundary increase/decrease (Reference No.)
- Additional documentation (Reference No. 75002088)

DOCUMENTATION:

- Original National Register of Historic Places Registration Form
- Multiple Property Documentation Form
- Continuation Sheets
- Removal Documentation
- Photographs
- CD w/ image files
- Original USGS Map
- Sketch map(s)
- Correspondence
 - Owner Objection
 - The enclosed owner objections
 - Do Do not constitute a majority of property owners

STAFF COMMENTS:

The following is an amendment to the National Register nomination for the Duluth, Missabe and Iron Range Depot (Ref. No. 75002088). The depot was accepted into the National Register in 1975. This amendment was necessary to fulfill a request the Keeper made in the 1980s.

October 14, 2013

Dr. Carol Shull
Keeper, National Register of Historic Places
Mail Stop 2280, 8th Floor
1201 Eye Street NW
Washington, DC 20005

Re: Duluth Missabe and Iron Range Depot (Endion Depot), Duluth, St. Louis County, Minnesota (Ref. No. 75002088)

Dear Dr. Shull:

Enclosed is an amendment to the National Register nomination for the Duluth, Missabe and Iron Range Depot in Duluth, Minnesota. The depot was placed in the National Register of Historic Places in 1975. It was subsequently relocated to accommodate an extension of Interstate Highway 35 through Duluth.

In a Memorandum of Agreement (MOA) dated May 1977, the Advisory Council on Historic Preservation approved the move. Nine years later, in a letter dated May 1986, the Keeper of the National Register also approved the move. The move took place on June 25, 1986.

The 1986 letter from the Keeper requested that the Minnesota State Historic Preservation Office (SHPO) notify the National Park Service (NPS) of the date of the move and amend the nomination, which was to include photographs of the depot at its new location and a new map. It does not appear that this amendment was ever sent to the NPS from the MN SHPO. This issue recently came to our attention when we were informed that the NPS' National Register website listed the depot at its old address, not its present address. As a result, we completed an amendment to the National Register nomination for the depot. The amendment notes the MOA and includes the updated address of the depot, the verbal boundary description, the boundary justification, and latitude/longitude coordinates. New maps (bing) are included as well, as are photographs of the depot on its new site, photographic log, photographic key, and gold disk of digital photographs. We include also the Keeper's May 1986 letter.

If you have questions concerning this amendment, please let us know.

Sincerely,



Barbara Mitchell Howard
Deputy State Historic Preservation Officer
MN State Historic Preservation Office
barbara.howard@mnhs.org
651-259-3466