

NOV 13 2015

948

# National Register of Historic Places Registration Form

This form is for use in nominating or requesting determinations for individual properties and districts. See instructions in National Register Bulletin, *How to Complete the National Register of Historic Places Registration Form*. If any item does not apply to the property being documented, enter "N/A" for "not applicable." For functions, architectural classification, materials, and areas of significance, enter only categories and subcategories from the instructions.

### 1. Name of Property

Historic name: Bridge A 249—Cloudcroft, New Mexico

Other names/site number: N/A

Name of related multiple property listing:  
N/A

(Enter "N/A" if property is not part of a multiple property listing)

### 2. Location

Street & number: 65 East Little Mexican Avenue

City or town: Cloudcroft State: New Mexico County: Otero

Not For Publication:  Vicinity:

### 3. State/Federal Agency Certification

As the designated authority under the National Historic Preservation Act, as amended,  
I hereby certify that this X nomination     request for determination of eligibility meets  
the documentation standards for registering properties in the National Register of Historic  
Places and meets the procedural and professional requirements set forth in 36 CFR Part 60.

In my opinion, the property X meets     does not meet the National Register Criteria. I  
recommend that this property be considered significant at the following  
level(s) of significance:

    national     statewide   X   local

Applicable National Register Criteria:

  X   A     B   X   C     D

Dr. Jeff Pappas, New Mexico States Historic Preservation Officer

Signature of certifying official/Title: \_\_\_\_\_ Date \_\_\_\_\_

State or Federal agency/bureau or Tribal Government

In my opinion, the property     meets     does not meet the National Register criteria.

Signature of commenting official: \_\_\_\_\_ Date \_\_\_\_\_

Title : \_\_\_\_\_ State or Federal agency/bureau or Tribal Government

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**4. National Park Service Certification**

I hereby certify that this property is:

- entered in the National Register
- determined eligible for the National Register
- determined not eligible for the National Register
- removed from the National Register
- other (explain:)

*Joe Elson W. Beall*  
Signature of the Keeper

*12-29-15*  
Date of Action

**5. Classification**

**Ownership of Property**

(Check as many boxes as apply.)

- Private:
- Public – Local
- Public – State
- Public – Federal

**Category of Property**

(Check only **one** box.)

- Building(s)
- District
- Site
- Structure
- Object

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**Number of Resources within Property**

(Do not include previously listed resources in the count)

Contributing	Noncontributing	
<u>0</u>	<u>0</u>	buildings
<u>0</u>	<u>0</u>	sites
<u>2</u>	<u>2</u>	structures
<u>0</u>	<u>0</u>	objects
<u>2</u>	<u>2</u>	Total

Number of contributing resources previously listed in the National Register 0

**6. Function or Use**

**Historic Functions**

(Enter categories from instructions.)

Transportation: rail-related

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

**Current Functions**

(Enter categories from instructions.)

Transportation: pedestrian-related

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

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## 7. Description

### Architectural Classification

(Enter categories from instructions.)

Other: wood railroad trestle

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**Materials:** (enter categories from instructions.)

Principal exterior materials of the property: Wood, Metal

### Narrative Description

(Describe the historic and current physical appearance and condition of the property. Describe contributing and noncontributing resources if applicable. Begin with a **summary paragraph** that briefly describes the general characteristics of the property, such as its location, type, style, method of construction, setting, size, and significant features. Indicate whether the property has historic integrity.)

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### Summary Paragraph

Bridge A 249 is a 46-foot-long open-deck, single-tier, wood-frame trestle located along the defunct Alamogordo & Sacramento Mountains Railroad line northeast of the village of Cloudcroft in the Sacramento Mountains in Otero County, New Mexico. Completed in 1899, the trestle is located inside a 30-degree curve between the Mexican Canyon Trestle to the north and, to the south, the collapsed “S” Trestle. Bridge A 249 is supported by four vertical bents and surmounted by stringers, on which ties and rails once rested. The trestle includes much of its original hardware. The National Register boundary includes 340-feet of intact railbed to the southwest and two noncontributing structures: a deteriorated water control structure below the bridge and a small pedestrian bridge located alongside Bridge A 249.

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### Narrative Description

Bridge A 249 is a 46-foot-long open-deck, single-tier, wood-frame trestle located along the defunct Alamogordo & Sacramento Mountains Railroad line west of U.S. Highway 82 northeast of the village of Cloudcroft in the Sacramento Mountains in Otero County, New Mexico.

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Completed in 1899, the trestle spans a small drainage located inside a 30-degree curve in the rail line between the Mexican Canyon Trestle to the north and, to the south, the collapsed “S” Trestle. Bridge A 249 is supported by four vertical bents surmounted by stringers, on which ties and rails once rested. The trestle includes much of its original hardware. The National Register boundary includes the trestle and 340-feet of intact railbed to the southwest. The railbed to the north was demolished when, in private ownership, it was converted to a vehicular drive leading to a cabin, which was constructed on the railbed. The two noncontributing structures include a deteriorated water control structure below the bridge and a small pedestrian bridge, built in 2014 and located alongside Bridge A 249.

Bridge A 249 has four bents (vertical support structures). The outer two bents serve as abutments for the trestle. The two inner bents are freestanding. All four bents include center plombs (vertical timber) and outer batters (slanted timber), characteristic of other trestles on the A&SM. The sills are 12-inch-by-12-inch timbers and the caps (horizontal beams that span the bents) are 12-inch by 14-inch timbers. The sills (horizontal beams that support the bents) are 12-inch-by-12-inch timbers. The sills have “mud blocks” below them, and in its current state, both the sills and mud blocks are in contact with the soil. The stringers (horizontal beams that span the bents) are sets of five 7-inch-by-14-inch timbers, typically about 15-feet long. On an operating railroad, ties are placed across the stringers and rails are set atop the ties. The main span between bent numbers 2 and 3 is about 12 feet above the bottom of the channel.

The A&SM ceased to operate in 1948, and it is likely that no maintenance on Bridge A 249 has been carried out since about 1940. The opening U.S. Highway 82 in 1950, the construction of a nearby Forest Service road, and the increased runoff from the village of Cloudcroft have increased the amount of water that flows under Bridge A 249. Severe erosion has occurred up and slope from Bridge A 249. Since 1940, trees and brush have grown around the trestle, diminishing the amount of sunlight and increasing wet conditions near the wood trestle.

Bridge A 249 has been damaged over decades by moisture, but is structurally sound. The sills and mud blocks are mostly buried in silt, but they retain structural integrity. Bents 2 and 3 remain dry and show few signs of rotting. The diagonal braces show rotting at their upper and lower ends. The New Mexico Rails to Trails Association is working to preserve and restore Bridge A 249. Important features, such as the sills, vertical supports, caps, and iron components, such as drift pins, bolts, washers, and spacers have been retained. There are no plans to place ties and rails on the trestle. The nonhistoric deck planks have been removed and the severely deteriorated stringers have been replaced. The wooden bulkheads at the abutments have deteriorated and will be replaced.

The T5001 Cloud-Climbing Trestle Trail is a hiking trail maintained by the Lincoln National Forest. It is a roughly north-to-south trail that begins at the Mexican Canyon Trestle to the north and continues south past the “S” Trestle site. Bridge A 249, which was privately owned until 2007, is located in a sharp bend in the middle of the trail. Bridge A 249 and the 340 feet of intact railbed are included as part of the trail. In 2014, NMRTA constructed a small wood pedestrian bridge so hikers could safely bypass Bridge A 249.

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The remains of a water control structure are located immediately down slope from Bridge A 249. This structure was intended to control erosion due to runoff from thunderstorms. The water descended approximately 15 feet in elevation across a horizontal distance of 30 feet. Although parts of this water control structure remain buried under silt, it is estimated that only about 5 to 10 percent of the original timbers remain. A temporary water control structure was constructed under the trestle to insure that the runoff from summer rains does not damage the trestle.

#### Contributing and Noncontributing Resources

Bridge A 249 is counted as one contributing structure.

The 340 feet of railbed is counted as one contributing structure.

The pedestrian bridge is counted as one noncontributing structure.

The water control structure is counted as one noncontributing structure.

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## 8. Statement of Significance

### Applicable National Register Criteria

(Mark "x" in one or more boxes for the criteria qualifying the property for National Register listing.)

- A. Property is associated with events that have made a significant contribution to the broad patterns of our history.
- B. Property is associated with the lives of persons significant in our past.
- C. Property embodies the distinctive characteristics of a type, period, or method of construction or represents the work of a master, or possesses high artistic values, or represents a significant and distinguishable entity whose components lack individual distinction.
- D. Property has yielded, or is likely to yield, information important in prehistory or history.

### Criteria Considerations

(Mark "x" in all the boxes that apply.)

- A. Owned by a religious institution or used for religious purposes
- B. Removed from its original location
- C. A birthplace or grave
- D. A cemetery
- E. A reconstructed building, object, or structure
- F. A commemorative property
- G. Less than 50 years old or achieving significance within the past 50 years

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**Areas of Significance**

(Enter categories from instructions.)

Engineering

Industry

**Period of Significance**

1899-1947

**Significant Dates**

1899—Bridge A 249 completed

**Significant Person**

(Complete only if Criterion B is marked above.)

N/A

**Cultural Affiliation**

N/A

**Architect/Builder**

Sumner, H. A. (Chief Engineer, A&SM Railroad)

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**Statement of Significance Summary Paragraph** (Provide a summary paragraph that includes level of significance, applicable criteria, justification for the period of significance, and any applicable criteria considerations.)

Bridge A 249 is a single-tier, wood-frame trestle located west of U.S. Highway 82 northeast of the village of Cloudcroft in the Sacramento Mountains in Otero County, New Mexico. Bridge A 249 is significant at the local level under National Register Criterion A in the area of industry because it represents the efforts of the Alamogordo & Sacramento Mountains Railroad to establish rail lines in the Sacramento Mountains for the harvest of timber. The A&SM operated from 1899 to 1947 and constructed roughly 26 miles of rail lines from Alamogordo to the spruce and fir forests throughout the Sacramento Mountains. Bridge A 249 is among the seven surviving trestles of the 51 constructed by the railroad. Bridge A 249 is significant at the local level under National Register Criterion C in the area of engineering because it is a representative design by Chief Engineer H. A. Sumner of small trestles required to continue the rail line over small drainages in the upper elevations of the A&SM Railroad.



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**Narrative Statement of Significance** (Provide at least **one** paragraph for each area of significance.)

Bridge A 249 is located west of U.S. Highway 82 northeast of the village of Cloudcroft in the Sacramento Mountains in Otero County, New Mexico. The trestle is significant at the local level under National Register Criterion A in the area of industry because it represents the efforts of the Alamogordo & Sacramento Mountains Railroad to establish rail lines in the Sacramento Mountains for the harvest of timber. The A&SM operated from 1899 to 1948 and constructed roughly 26 miles of rail lines from Alamogordo to the spruce and fir forests throughout the Sacramento Mountains. Bridge A 249 is among the seven surviving trestles of the 51 constructed by the railroad.

Bridge A 249 is significant at the local level under National Register Criterion C in the area of engineering because it is a representative design by Chief Engineer H. A. Sumner of small trestles required to continue the rail line over small drainages in the upper elevations of the A&SM Railroad. Bridge A 249, a single-tier, wood-frame trestle, is an important structure in one of the most challenging sections of the A&SM, which includes the Mexican Canyon Trestle and the site of the “S” Trestle.

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**Developmental history/additional historic context information (if appropriate)**

In the late 1890s, brothers Charles and John Eddy sought to exploit the gold fields near White Oaks, New Mexico, 12 miles northeast of Carrizozo. The brothers planned to connect a rail line with the Rock Island Railroad, which was building west across Kansas. As the owners of a railroad with freight and passenger service from the Midwest to the Southern Pacific Railroad at El Paso, they saw the potential for large profits. They built north from El Paso and founded the town of Alamogordo. Building and operating the railroad required timbers for ties and trestles and coal for the steam locomotives. The brothers established the Alamogordo and Sacramento Mountains Railroad (A&SM) to transport timber from the Sacramento Mountains to saw mills in Alamogordo. They built a spur to Carrizozo to carry coal mined near Capitan, New Mexico.

The rim of the Sacramento Mountains at Cloudcroft is approximately 4,600 feet above Alamogordo and 8,600 feet above sea level. The Eddys hired H.A. Sumner, who had worked as a “locating engineer” on multiple Colorado railroads, to lay out a 26-mile standard-gauge railroad that could transport logs to Alamogordo. As chief engineer, he laid out what became known as the “railroad of curves” with an average grade of 3.5 percent. In the last seven miles before Cloudcroft, he constructed a famous “switchback” to gain altitude and engineered 13 trestles to cross canyons and drainages. The “S” Trestle was 338-feet long, 42-feet high, and incorporated two reverse 30-degree curves. The Mexican Canyon Trestle, north of Cloudcroft, is 323-feet long, 60-feet high, with a 30-degree curve. Sumner’s previous railroad in Colorado, the Florence and Cripple Creek Railroad, had an average gradient of only 2.5 percent. The steep grades of the A&SM pushed the limits of railroad engineering.

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After constructing the A&SM, H.A. Sumner was hired by Colorado railroad entrepreneur David Moffett as Chief Engineer for the Denver and Northwest Pacific Railroad. The “Moffett Road” ran west from Denver across the Rockies. Sumner masterfully laid out a grade of less than 2% and became renowned for the engineering of this difficult route.

In 1899, logging companies started shipping timber to the Southwest Lumber Company in Alamogordo. They extended spurs south of Cloudcroft into Cox Canyon, Benson Canyon, and Agua Chiquita Canyon. To the north, logging spurs ran several miles into the Mescalero Apache Reservation. In 1905, the line became part of the Phelps Dodge El Paso and Southwestern Railroad and was leased by Southern Pacific Railroad in 1924.

In the 1930s and 1940s, trucks began to compete with railroads for hauling timber. Improved roads and bigger trucks alleviated the high cost of rail maintenance along the A&SM. The A&SM discontinued service in 1947, and by 1948, the rails and ties had been removed. Trestles located on private property were often left in place. Most trestles were either taken down by private contractors or disassembled for their timber by local citizens.

Bridge A 249 was sold to Dr. Bonnie Brooks in the early 1950s as part of a six-acre tract that included 800 feet of railbed. In the early 1960s, Dr. Brooks nailed planks across the stringers to allow vehicles to drive across the trestle. She also built a house on the railbed north of Bridge A 249. In 2007, Dr. Brooks donated the property to the New Mexico Rails to Trails Association.

The New Mexico Rails to Trails Association was formed in the mid-1990s. The conversion of the Cloudcroft section of the A&SM to a pedestrian trail was one of its first projects. This section is designated as the T5001 Cloud Climbing Trestle Trail. The Forest Service developed the top of the hill south of the Brooks property as a day-use area called the Trestle Recreation Area. This area provides access to the site of the “S” Trestle, which collapsed in stages in the 1960s and 1970s. The Mexican Canyon Trestle to the north was listed in the National Register of Historic Places in 1979. In 2009 and 2010, the Mexican Canyon Trestle underwent a major restoration. Bridge A 249 is located between the Mexican Canyon Trestle and the site of the “S” Trestle, and provides continuity to one of the most challenging sections of the A&SM Railroad line.

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## 9. Major Bibliographical References

**Bibliography** (Cite the books, articles, and other sources used in preparing this form.)

Glover, Vernon J. *Logging Railroads of the Lincoln National Forest, New Mexico*. 1984.  
[<http://www.foresthistory.org/ASPNET/Publications/region/3/lincoln/cultres4/index.htm>]

Melton, Lynn. Mexican Canyon Restoration. 2009-2010. [<http://www.lincoln-nf-trails.org/mct.html>].

Myrick, David. F. *New Mexico's Railroads: A Historical Survey*. Albuquerque: University of New Mexico Press, 1990

Neal, Dorothy, Jenson. *The Cloud-Climbing Railroad*. El Paso: Texas Western Press, 1998.

“Right of Way Track Map,” Alamogordo and Sacramento Mountains Railroad. Cloudcroft, New Mexico: Sacramento Mountains Historical Society, before 1905.

“Right of Way Track Map, Alamogordo and Sacramento Mountains Railroad.” Alamogordo, New Mexico: Collection of Klad Zimmerle, 1948.

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### Previous documentation on file (NPS):

- preliminary determination of individual listing (36 CFR 67) has been requested
- previously listed in the National Register
- previously determined eligible by the National Register
- designated a National Historic Landmark
- recorded by Historic American Buildings Survey # \_\_\_\_\_
- recorded by Historic American Engineering Record # \_\_\_\_\_
- recorded by Historic American Landscape Survey # \_\_\_\_\_

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**Primary location of additional data:**

State Historic Preservation Office

Other State agency

Federal agency

Local government

University

Other

Name of repository: \_\_\_\_\_

**Historic Resources Survey Number (if assigned):** \_\_\_\_\_

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**10. Geographical Data**

**Acreeage of Property** Approximately one acre.

Use either the UTM system or latitude/longitude coordinates

**Latitude/Longitude Coordinates**

Datum if other than WGS84: \_\_\_\_\_

(enter coordinates to 6 decimal places)

1. Latitude: 32.959555                      Longitude: -105.748777

2. Latitude:                                      Longitude:

3. Latitude:                                      Longitude:

4. Latitude:                                      Longitude:

**Or**

**UTM References**

Datum (indicated on USGS map):

NAD 1927    or     NAD 1983

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- |          |          |           |
|----------|----------|-----------|
| 1. Zone: | Easting: | Northing: |
| 2. Zone: | Easting: | Northing: |
| 3. Zone: | Easting: | Northing: |

**Verbal Boundary Description** (Describe the boundaries of the property.)

The boundary appears as a heavy blue on a map, drawn to scale.

**Boundary Justification** (Explain why the boundaries were selected.)

The National Register boundary reflects the current legal boundary and includes the property historically associated with Bridge A 249 and 340 feet of intact rail bed south of the trestle.

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**11. Form Prepared By**

name/title: Lynn Melton, Project Coordinator  
organization: New Mexico Rails to Trails Association  
street & number: 1 Squirrel Run  
city or town: Cloudcroft state: NM zip code: 88317  
e-mail: lynnmelt@gmail.com  
telephone: 972-998-8567  
date: May 25, 2015

**State Historic Preservation Office Staff**

name/title: Steven Moffson / State and National Register Coordinator  
organization: New Mexico Historic Preservation Division  
street & number: 407 Galisteo Street, Suite 236  
city or town: Santa Fe state: NM zip code: 87501  
e-mail: steven.moffson@state.nm.us  
telephone: 505-476-044  
date: September 6, 2015

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### Additional Documentation

Submit the following items with the completed form:

- **Maps:** A **USGS map** or equivalent (7.5 or 15 minute series) indicating the property's location.
- **Sketch map** for historic districts and properties having large acreage or numerous resources. Key all photographs to this map.
- **Additional items:** (Check with the SHPO, TPO, or FPO for any additional items.)

### Photographs

Submit clear and descriptive photographs. The size of each image must be 1600x1200 pixels (minimum), 3000x2000 preferred, at 300 ppi (pixels per inch) or larger. Key all photographs to the sketch map. Each photograph must be numbered and that number must correspond to the photograph number on the photo log. For simplicity, the name of the photographer, photo date, etc. may be listed once on the photograph log and doesn't need to be labeled on every photograph.

### Photo Log

Name of Property: Bridge A 249—Cloudcroft, New Mexico

City or Vicinity: Cloudcroft

County: Otero

State: New Mexico

Photographer: Lynn Melton

Date Photographed: May 21, 2015

Description of Photograph(s) and number, include description of view indicating direction of camera:

1 of 17. Bridge A 249 and pedestrian bridge, photographer facing southwest.

2 of 17. Bridge A 249 and pedestrian bridge, photographer facing southwest.

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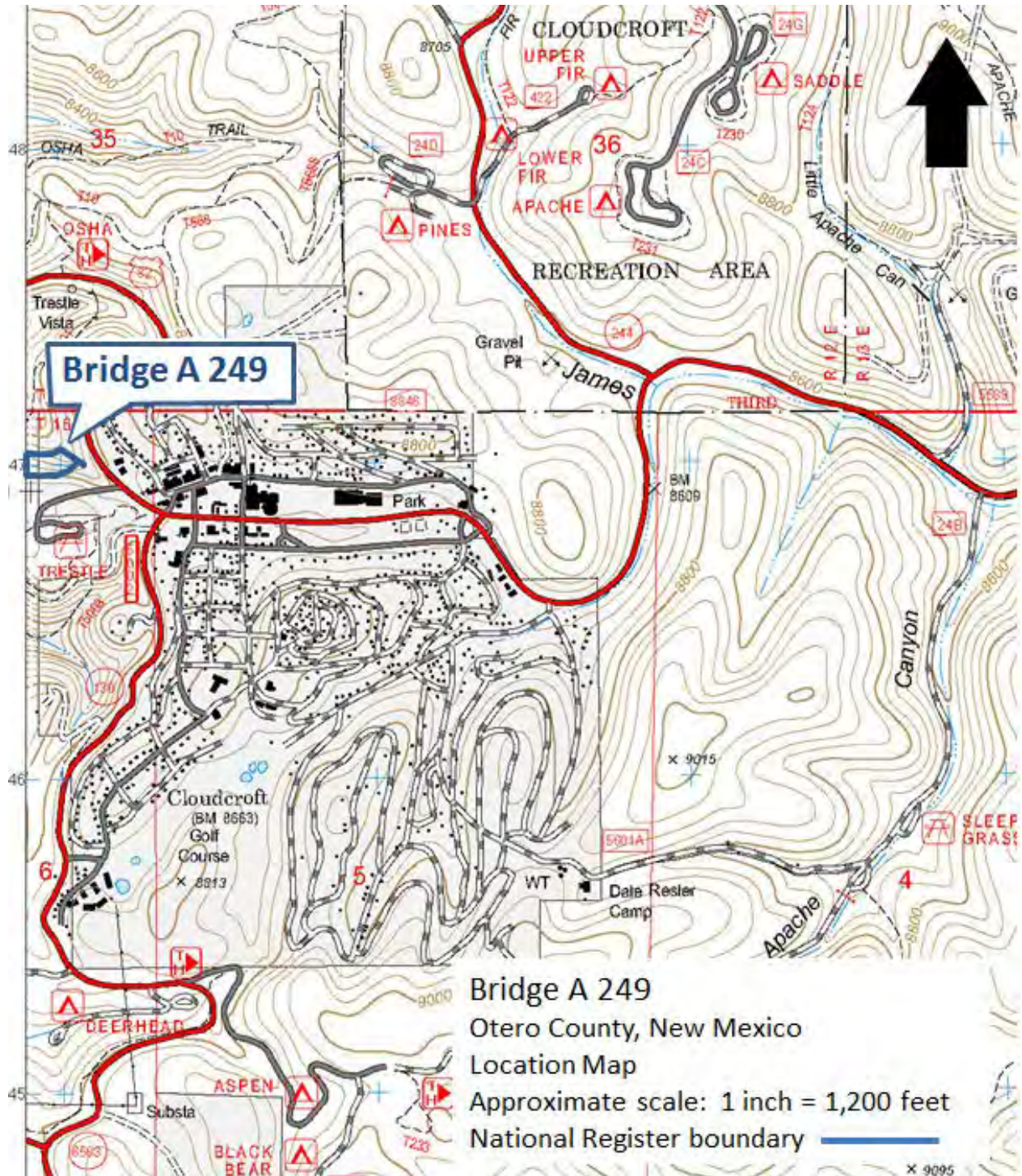
- 3 of 17. Bridge A 249 and pedestrian bridge, photographer facing northeast.
- 4 of 17. Bridge A 249 with bents 2 and 3, photographer facing east.
- 5 of 17. Bridge A 249 and down slope with remains of water control structure, photographer facing northeast.
- 6 of 17. Bridge A 249 before rehabilitation, c.2014, photographer facing northeast.
- 7 of 17. Bridge A 249 before rehabilitation with bents 3 and 4, photographer facing northwest.
- 8 of 17. Bridge A 249 before rehabilitation, with barrier fences, photographer facing southwest.
- 9 of 17. Railbed, photographer facing southwest.
- 10 of 17. Railbed, photographer facing west.
- 11 of 17. Railbed, photographer facing west.
- 12 of 17. Railbed, photographer facing west.
- 13 of 17. Railbed with coal on surface.
- 14 of 17. Bridge A 249, photographer facing southwest.
- 15 of 17. Bridge A 249, underneath with bent 3, photographer facing southwest.
- 16 of 17. Bridge A 249, underneath with bent 2, photographer facing south.
- 17 of 17. Bridge A 249, with remains of water control structure in foreground, photographer facing south.

**Paperwork Reduction Act Statement:** This information is being collected for applications to the National Register of Historic Places to nominate properties for listing or determine eligibility for listing, to list properties, and to amend existing listings. Response to this request is required to obtain a benefit in accordance with the National Historic Preservation Act, as amended (16 U.S.C.460 et seq.).

**Estimated Burden Statement:** Public reporting burden for this form is estimated to average 100 hours per response including time for reviewing instructions, gathering and maintaining data, and completing and reviewing the form. Direct comments regarding this burden estimate or any aspect of this form to the Office of Planning and Performance Management, U.S. Dept. of the Interior, 1849 C. Street, NW, Washington, DC.

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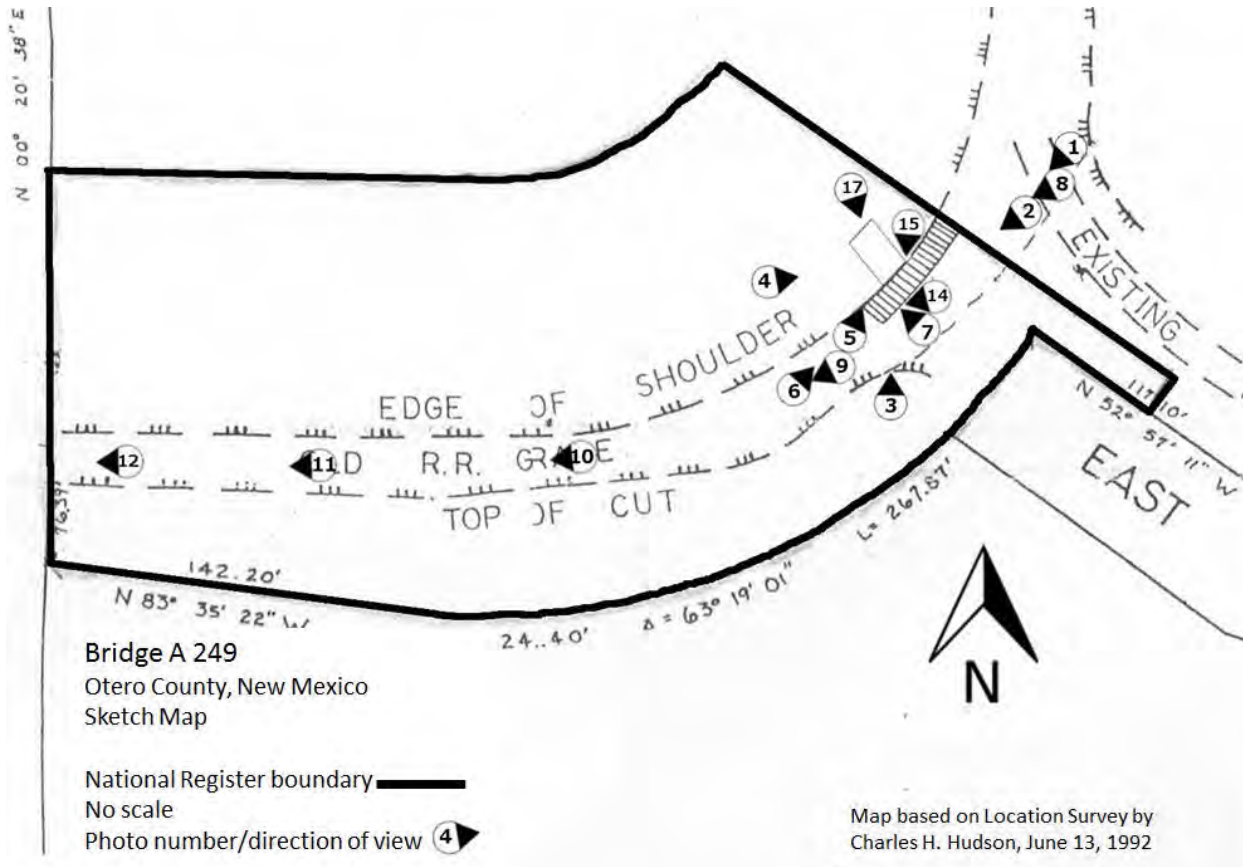
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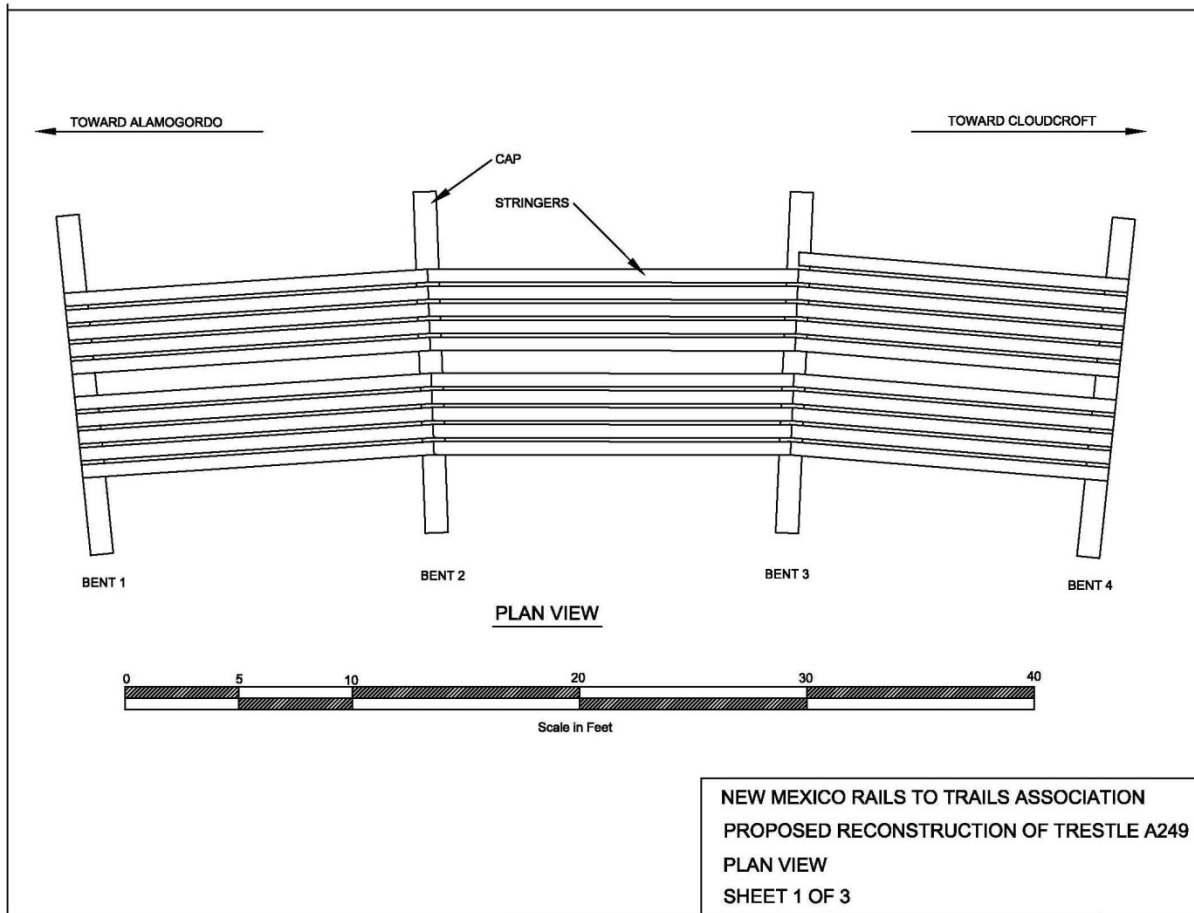


Figure 1. Proposed Reconstruction of Trestle A 249, Plan View.

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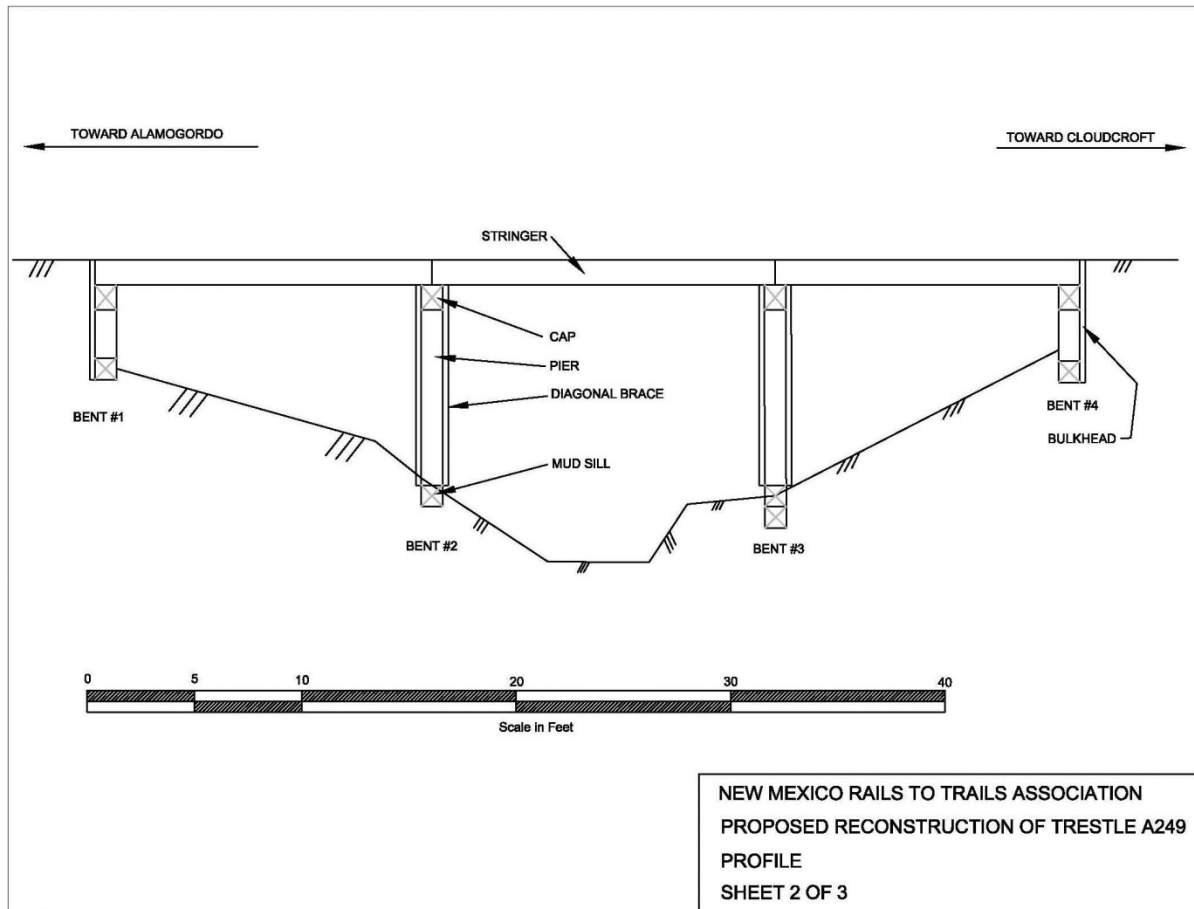


Figure 2. Proposed Reconstruction of Trestle A 249, Profile.

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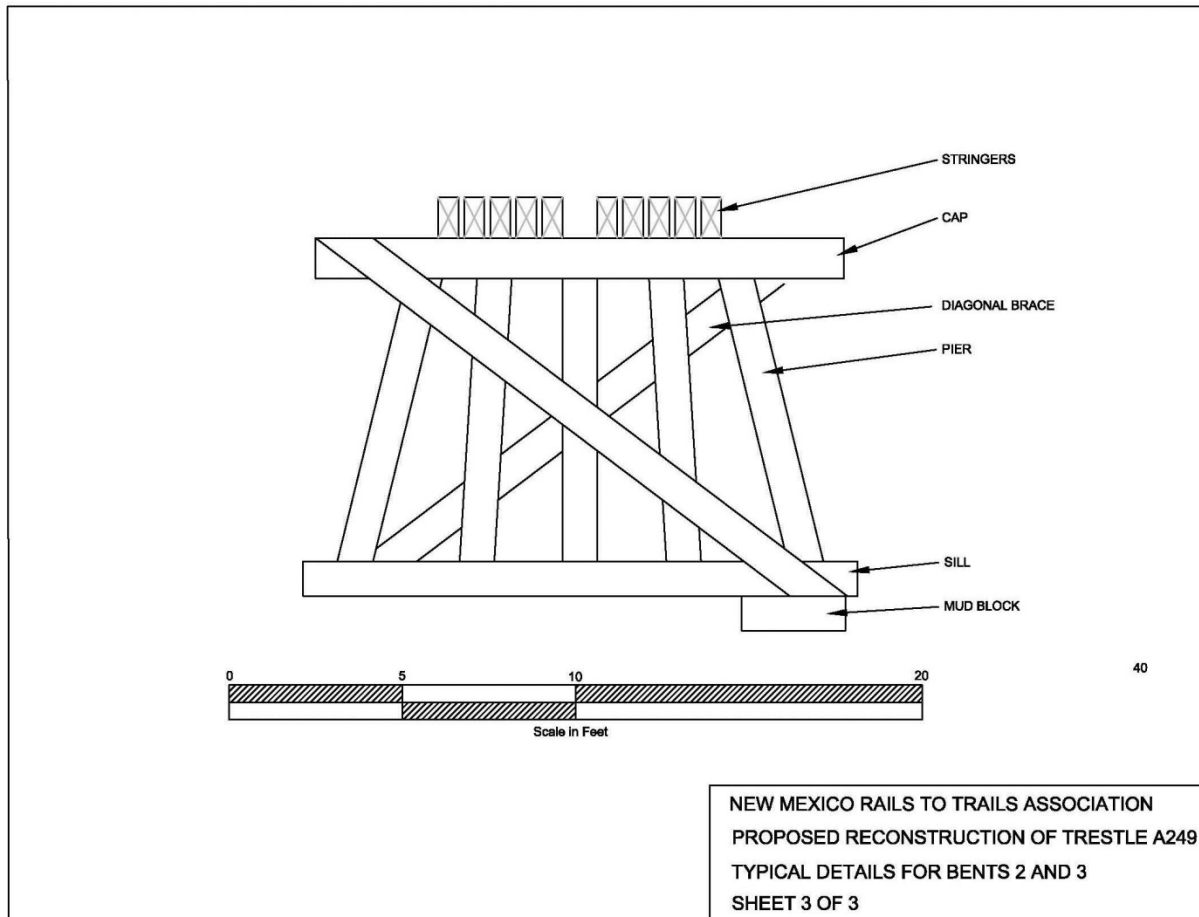


Figure 3. Proposed Reconstruction of Trestle A 249, Typical Details for Bents 2 and 3.

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Figure 4. Bridge A 249 in 1959, with exposed stringers.

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Figure 5. Bridge A 249 in 1959, with water control structure (lower left).

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1 of 17. Bridge A 249 and pedestrian bridge, photographer facing southwest.

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2 of 17. Bridge A 249 and pedestrian bridge, photographer facing southwest.



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3 of 17. Bridge A 249 and pedestrian bridge, photographer facing northeast.

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4 of 17. Bridge A 249 with bents 2 and 3, photographer facing east.

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5 of 17. Bridge A 249 and down slope with remains of water control structure, photographer facing northeast.

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6 of 17. Bridge A 249 before rehabilitation, c.2014, photographer facing northeast.

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7 of 17. Bridge A 249 before rehabilitation with bents 3 and 4, photographer facing northwest.

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8 of 17. Bridge A 249 before rehabilitation, with barrier fences, photographer facing southwest.

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9 of 17. Railbed, photographer facing southwest.

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10 of 17. Railbed, photographer facing west.



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11 of 17. Railbed, photographer facing west.

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12 of 17. Railbed, photographer facing west.

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13 of 17. Railbed with coal on surface.

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14 of 17. Bridge A 249, photographer facing southwest.

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15 of 17. Bridge A 249, underneath with bent 3, photographer facing southwest.

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16 of 17. Bridge A 249, underneath with bent 2, photographer facing south.

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17 of 17. Bridge A 249, with remains of water control structure in foreground, photographer facing south.









































UNITED STATES DEPARTMENT OF THE INTERIOR  
NATIONAL PARK SERVICE

NATIONAL REGISTER OF HISTORIC PLACES  
EVALUATION/RETURN SHEET

REQUESTED ACTION: NOMINATION

PROPERTY NAME: Bridge A 249--Cloudcroft, New Mexico

MULTIPLE NAME:

STATE & COUNTY: NEW MEXICO, Otero

DATE RECEIVED: 11/13/15      DATE OF PENDING LIST: 12/16/15  
DATE OF 16TH DAY: 12/31/15      DATE OF 45TH DAY: 12/29/15  
DATE OF WEEKLY LIST:

REFERENCE NUMBER: 15000948

REASONS FOR REVIEW:

APPEAL: N DATA PROBLEM: N LANDSCAPE: N LESS THAN 50 YEARS: N  
OTHER: N PDIL: N PERIOD: N PROGRAM UNAPPROVED: N  
REQUEST: N SAMPLE: N SLR DRAFT: N NATIONAL: N

COMMENT WAIVER: N

ACCEPT       RETURN       REJECT      12-29-15 DATE

ABSTRACT/SUMMARY COMMENTS:

Entered in  
The National Register  
of  
Historic Places

RECOM./CRITERIA \_\_\_\_\_

REVIEWER \_\_\_\_\_ DISCIPLINE \_\_\_\_\_

TELEPHONE \_\_\_\_\_ DATE \_\_\_\_\_

DOCUMENTATION see attached comments Y/N see attached SLR Y/N

If a nomination is returned to the nominating authority, the nomination is no longer under consideration by the NPS.



STATE OF NEW MEXICO  
**DEPARTMENT OF CULTURAL AFFAIRS**  
**HISTORIC PRESERVATION DIVISION**

BATAAN MEMORIAL BUILDING  
407 GALISTEO STREET, SUITE 236  
SANTA FE, NEW MEXICO 87501  
PHONE (505) 827-6320 FAX (505) 827-6338

Susana Martinez  
Governor

November 16, 2015

J. Paul Loether  
National Park Service  
National Register of Historic Places  
1201 "I" (Eye) Street, N.W. 8th floor  
Washington, D.C. 20005

Dear Mr. Loether:

The enclosed disk contains the true and correct copy of the nomination for Bridge A 249—Cloudcroft, New Mexico in Otero County, New Mexico to the National Register of Historic Places.

- Disk of National Register of Historic Places nomination form and maps as a pdf
- Disk with digital photo images
- Physical signature page
- Sketch map(s)/attachment(s)
- Correspondence
- Other:

COMMENTS:

- This property has been certified under 36 CFR 67
- The enclosed owner objection(s) do  do not  constitute a majority of property owners.
- Special considerations: Note that this property is nominated at the national level of significance.

Sincerely,

Steven Moffson  
State and National Register Coordinator

Enclosures