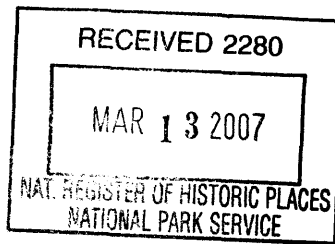


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**United States Department of the Interior
National Park Service**



**National Register of Historic Places
Registration Form**

This form is for use in nominating or requesting determinations for individual properties and districts. See instructions in *How to Complete the National Register of Historic Places Registration Form* (National Register Bulletin 16A). Complete each item by marking "x" in the appropriate box or by entering the information requested. If an item does not apply to the property being documented, enter "N/A" for "not applicable." For functions, architectural classification, materials and areas of significance, enter only categories and subcategories from the instructions. Place additional entries and narrative items on continuation sheets (NPS Form 10-900a). Use a typewriter, word processor, or computer, to complete all items.

1. Name of Property

historic name D. S. Chamberlain Building

other names/site number L. W. Taylor Motor Company and Payne Motor Company Building

2. Location

street & number 1312 Locust Street [N/A] not for publication

city or town Des Moines [N/A] vicinity

state Iowa code IA county Polk code 153 zip code 50309

3. State/Federal Agency Certification

As the designated authority under the National Historic Preservation Act, as amended, I hereby certify that this nomination [] request for determination of eligibility meets the documentation standards for registering properties in the National Register of Historic Places and meets the procedural and professional requirements set forth in 36 CFR Part 60. In my opinion, the property meets [] does not meet the National Register criteria. I recommend that this property be considered significant nationally statewide locally. ([] see continuation sheet for additional comments).

Howell J. Locke, Deputy SHPO, March 9, 2007
Signature of certifying official/Title Date

STATE HISTORICAL SOCIETY OF IOWA

State or Federal agency and bureau

In my opinion, the property [] meets [] does not meet the National Register criteria. ([] See continuation sheet for additional comments.)

Signature of certifying official/Title Date

State or Federal agency and bureau

4. National Park Service Certification

hereby certify that the property is:

- entered in the National Register.
- See continuation sheet.
- determined eligible for the National Register.
- See continuation sheet.
- determined not eligible for the National Register.
- removed from the National Register.
- other, (explain:)

Signature of the Keeper

Janice M. C. Williams

Date of Action

6/28/2007

D. S. Chamberlain Building
Name of Property

Polk County, Iowa
County and State

5. Classification

Ownership of Property

(Check as many boxes as apply)
 private
 public-local
 public-State
 public-Federal

Category of Property

(Check only one box)
 building(s)
 district
 site
 structure
 object

Number of Resources within Property

(Do not include previously listed resources in the count.)

Contributing Noncontributing
1 0 buildings

_____ sites

_____ structures

_____ objects
_____ Total
1 0

Name of related multiple property listing

(Enter "N/A" if property is not part of a multiple property listing.)

Number of contributing resources previously listed in the National Register

Architectural Legacy of Proudfoot & Bird in Iowa, 1882-1940 0

6. Function or Use

Historic Functions

(Enter categories from instructions)

Commerce/Trade: Specialty Store: _____

Current Functions

(Enter categories from instructions)

Commerce/Trade: Warehouse: _____

7. Description

Architectural Classification

(Enter categories from instructions)

Late 19th And Early 20th Century American Movements: _____

Commercial Style _____

Late 19th And Early 20th Century American Movements Chicago Style _____

Materials

(Enter categories from instructions)

foundation Concrete

walls Brick

roof Asphalt

other

Narrative Description

(Describe the historic and current condition of the property on one or more continuation sheets.)

8. Statement of Significance

Applicable National Register Criteria

(Mark "x" in one or more boxes for the criteria qualifying the property for National Register listing.)

A Property is associated with events that have made a significant contribution to the broad patterns of our history.

B Property is associated with the lives of persons significant in our past.

C Property embodies the distinctive characteristics of a type, period, or method of construction or represents the work of a master, or possesses high artistic values, or represents a significant and distinguishable entity whose components lack individual distinction.

D Property has yielded, or is likely to yield, information important in prehistory or history.

Criteria Considerations

(Mark "x" in all the boxes that apply.)

Property is:

A owned by a religious institution or used for religious purposes.

B removed from its original location.

C a birthplace or grave.

D a cemetery.

E a reconstructed building, object, or structure.

F a commemorative property.

G less than 50 years of age or achieved significance within the past 50 years.

Narrative Statement of Significance

(Explain the significance of the property on one or more continuation sheets.)

9. Major Bibliographical References

Bibliography

(Cite the books, articles, and other sources used in preparing this form on one or more continuation sheets.)

Previous documentation on file (NPS):

- preliminary determination of individual listing (36 CFR 67) has been requested
- previously listed in the National Register
- previously determined eligible by the National Register
- designated a National Historic Landmark
- recorded by Historic American Buildings Survey
- recorded by Historic American Engineering Record #

Areas of Significance

(Enter categories from instructions)

Commerce

Architecture

Period of Significance

1917

Significant Dates

1917

1936

Significant Person

(Complete if Criterion B is marked above)

N/A

Cultural Affiliation

Architect/Builder

Proudfoot, Bird and Rawson

Kucharo, W. F.

Primary location of additional data:

- State Historic Preservation Office
- Other State agency
- Federal agency
- Local government
- University
- Other

Name of repository: #

10. Geographical Data

Acreage of Property Less than one acre

UTM References

(Place additional UTM references on a continuation sheet.)

1 [1]5 [4]4[7]1[0]0 [4]6[0]3[8]3[1] 2 [][][][][][][][][][][][][][][][]
Zone Easting Northing Zone Easting Northing
3 [][][][][][][][][][][][][][][][] 4 [][][][][][][][][][][][][][][][]
 See continuation sheet

Verbal Boundary Description

(Describe the boundaries of the property on a continuation sheet.)

Boundary Justification

(Explain why the boundaries were selected on a continuation sheet.)

11. Form Prepared By

name/title James E. Jacobsen
organization History Pays! Historic Preservation Consulting Firm date March 6, 2007
street & number 4411 Ingersoll Avenue telephone 515-274-3625
city or town Des Moines state IA zip code 50312-2415

Additional Documentation

Submit the following items with the complete form:

Continuation Sheets

Maps

- A **USGS map** (7.5 or 15 minute series) indicating the property's location.
- A **Sketch map** for historic districts and properties having large acreage or numerous resources.

Photographs

Representative **black and white photographs** of the property.

Additional items

(Check with the SHPO or FPO for any additional items)

Property Owner

(Complete this item at the request of SHPO or FPO.)

name West End Renovations East LLC
street & number 2145 NW 134th Street telephone 515-270-1521
city or town Clive state Iowa zip code 50325-7510

Paperwork Reduction Act Statement: This information is being collected for applications to the National Register of Historic Places to nominate properties for listing or determine eligibility for listing, to list properties, and to amend existing listings. Response to this request is required to obtain a benefit in accordance with the National Historic Preservation Act, as amended (16 U.S.C. 470 et seq.).

Estimated Burden Statement: Public reporting burden for this form is estimated to average 18.1 hours per response including time for reviewing instructions, gathering and maintaining data, and completing and reviewing the form. Direct comments regarding this burden estimate or any aspect of this form to the Chief, Administrative Services Division, National Park Service, P.O. Box 37127, Washington, DC 20013-7127; and the Office of Management and Budget, Paperwork Reductions Projects (1024-0018), Washington, DC 20503.

**United States Department of the Interior
National Park Service****National Register of Historic Places
Continuation Sheet**Section number 7 Page 1**D. S. Chamberlain Building****Polk County, Iowa****7. Narrative Description:**

The D. S. Chamberlain Building is a two-story (with mezzanine, partial basement, three-story rear addition) brick building with a commercial plan. The building measures 100 feet in width and 167 feet in depth. The façade is on the north end and its ornamentation is continued across the northernmost bay of the west sidewall. A 1935-36 three-story rear addition (designed in 1919) extended the building to the alley to the south and the present-day building footprint occupies all of the double building lot. The original building was designed with mirror-image separate storefronts so that two auto dealerships could occupy it. The current interior plan reflects the most recent stud wall and plasterboard subdivision of the original space. These walls added a partial mezzanine in the front of the original display room, added smaller offices on the original mezzanine level and below it, and subdivided the back of the ground floor plan. These wall divisions are readily removable and to some extent have been partly removed. The original first floor layout featured a spacious high-ceilinged showroom in the front two bays. Sales offices and lounges separated the showrooms from the second-hand auto salesrooms that occupied the center of the plan. A centered mezzanine area housed the administrative and other offices and four elaborate stairways linked that level with the showrooms to the north. The back of the main floor was used for auto servicing and it had full-height ceilings. The original mezzanine level with its decorative ceilings survives but the twin stairs that led up to the mezzanine from each of the two storefronts have been removed. The dividing wall that separated the storefronts has been opened up to form a single storefront area. Originally the third floor was open for auto storage and a rear-centered freight elevator (the shaft remains) served the entire building. Four Hayes style skylights (openings extant but sealed) provided light for the upper level and a centered light well (also extant but sealed) did the same for the mezzanine. Because the west wall of the building fronted on an alleyway, that side of the design was fully fenestrated. In contrast, the east sidewall has virtually no fenestration apart from a recessed light well that is located north of center along that wall.

The building is located on the south side of Locust Street in the west end of the West Side of downtown Des Moines (to the west of the Des Moines River which cuts the city's downtown in half). The building fronts to the northwest and Locust Street is a principal one-way downtown street. The Gateway West project, which is establishing a downtown greenway, has removed the commercial buildings that were immediately opposite the building. An undeveloped lot (paved parking) with twin north-south running alleyways, is to the west of the building, while another paved lot, never built up, is to the east. An alleyway runs behind the building.

The façade design overlays a variation of the Chicago-style window on a Commercial Style base design. The architects were Proudfoot, Bird and Rawson, a Des Moines architectural partnership that is rated by architectural historian Wesley Shank as Iowa's "preeminent early-twentieth-century architectural firm." The façade design is symmetrical and reflects the original division of the storefront into two identical stores. The design is notable in that a high proportion of the façade was glass. The high ceiling of the auto salesrooms demanded extensive display windows as well as a very substantial transom. Figure 1 depicts these large glassed areas, virtually devoid of subdivisions. The second floor front was similarly almost all glass-fronted, with a nearly solid band of Chicago-style windows comprising most of its frontage. A high castellated parapet front tops the façade. Typical of the comparable designs by this firm, ornamental stone, terra cotta and tile inserts are employed to elaborate the common bond with Flemish headers brickwork of the parapet front. On the second floor front, framed brick decorative panels define the corners and center of that level. Two broader panels flank a centered single double-hung window opening on each storefront and the panels and central window are aligned with the entry below it. These panels feature woven brick and decorative inserts. The upper corners of the façade feature elaborate terra-cotta inserts and the parapet coping is composed of broad stonework with a castellated effect. Twin slightly rounded pediments are centered above each storefront entrance. The original design envisioned twin flagpoles that further distinguished the two storefronts (Shank, p. 127).

It is the Chicago-style windows that are the outstanding façade feature of the Chamberlain building. The windows are double-hung sash with the lower sash being twice the length of the upper ones. The striking design element however, is the use of very thin and elongated lights in the lower sash. The real miracle is that these survive today. The original rear wall (Figure

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National Park Service**

**National Register of Historic Places
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D. S. Chamberlain Building

Polk County, Iowa

3) featured narrow elongated windows (one of which survives, Figure 8), evidence of a design aesthetic that transcended simple façade ornamentation.

The façade materials include a variegated rug brick, Bedford, Indiana, limestone (on the base plate) and the aforementioned decorative insert materials. The sidewalls are laid up with a vitrified brown shale brick. Sidewall parapet copings are a vitrified salt-glazed ball joint tile coping. The original entry vestibules featured a marble base, as did the showrooms (black Belgian marble border and white Italian marble base). The salesroom base marble was Sylvan Green in the east half and Grey Tennessee in the west half. The floor tile designs (intact, Figure 6) also featured a green color scheme (east) and gray scheme (west). All window and storefront framing was built of Idaho soft white pine lumber.

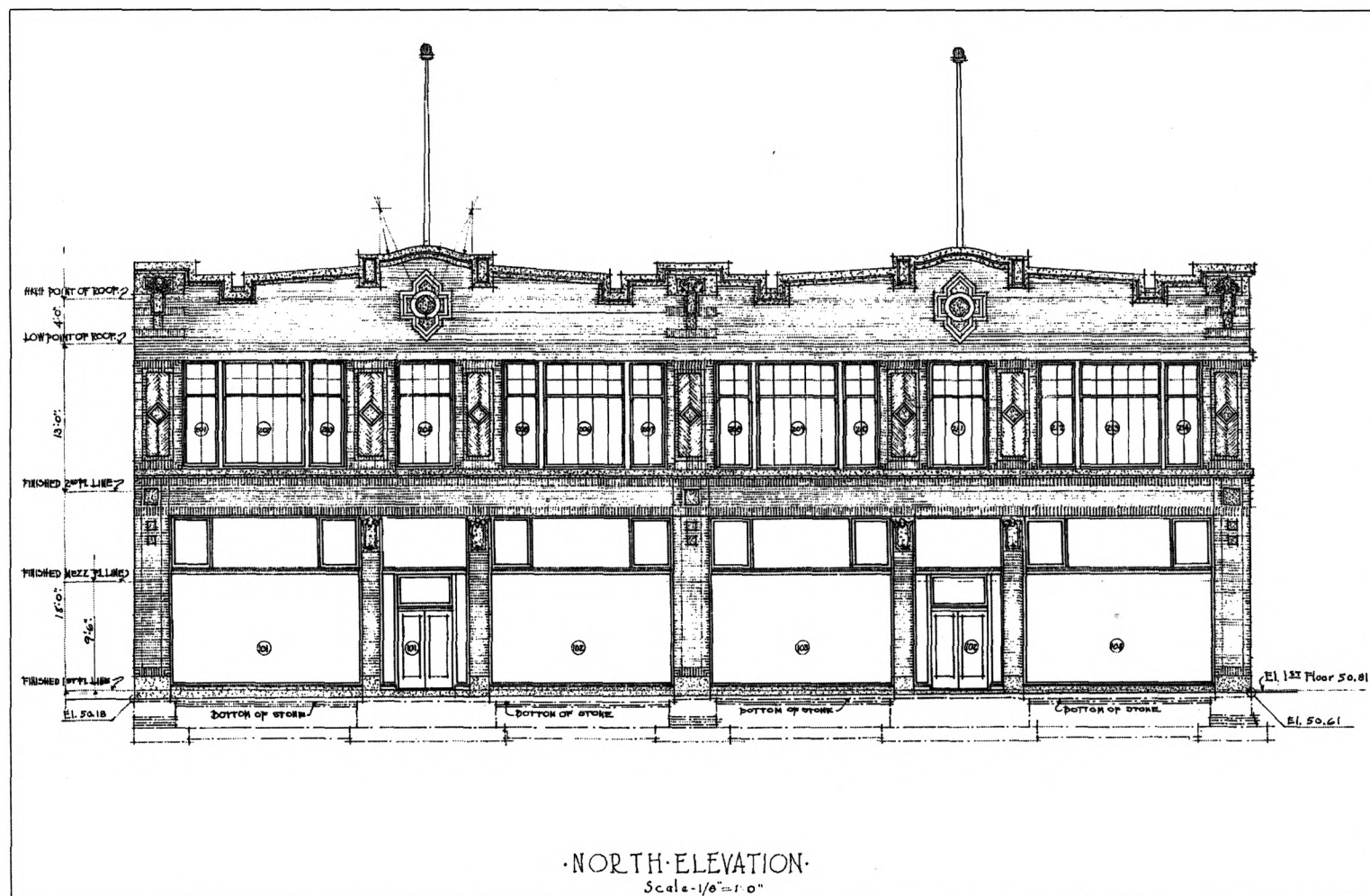


Figure 1: Main Façade Drawing
(Original architects plans, Proudfoot, Bird and Rawson, Des Moines, 1916)

The structural system consists of spread concrete footings with brick walls (see Figures 1-4 which depict excavation levels, and Figure 10). The width of the spread footings varies from 23 inches to 5 feet 11 inches and all footings are excavated down to solid stratum save for the east wall that rests on gravel and sand. The basic support system consists of perimeter walls and a central dividing wall, with footings running continuously beneath them. Each half-plan also has two rows of five

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D. S. Chamberlain Building

Polk County, Iowa

columns, each of which has its own floating concrete spread footing. The spacing of the columns east to west across the width of each half-plan are 11 feet two inches, 26 feet, and 11 feet two inches. The spacing between the four columns is a constant 18 feet 2 inches save for the southernmost or rear bay that is 20 feet long (this arrangement applies to the original building exclusive of the 1936 rear addition which added two bays beyond the former exterior rear wall). The clear span of the showroom is achieved by bearing massive steel beams on two centered pilasters in the front of the plan. The first and main pilaster is supported on two layers of concrete spread footings (6 feet by eight feet) and the pilaster itself is 2.5 feet square. The second pilaster, set 18 feet 2 inches south of the first one, also has two layers of spread footings and the brick pilaster measures 13 inches square. The first interior column is 37.5 feet from the front wall. Structural steel columns support the roof and presumably the mezzanine above the showroom and the northernmost of these is notable for its scale and composition. The beam is built-up much like a pony truss in a railroad bridge, with riveted square panels that are framed with base and cap "L" rails. Columns are simple I-beams and simple "stops" are riveted into the top of each beam to brace the beams. Figure 9 illustrates how a truss system is used to support the roof in the front of the plan. A similar truss is used to create the clearspan showroom space. The roof support system uses lighter beams along the perimeter brick support walls. Floor joists are 2x8 inch spaced 14 inches on center, while roof joints are 2x10 inch, spaced 16 inches on center.

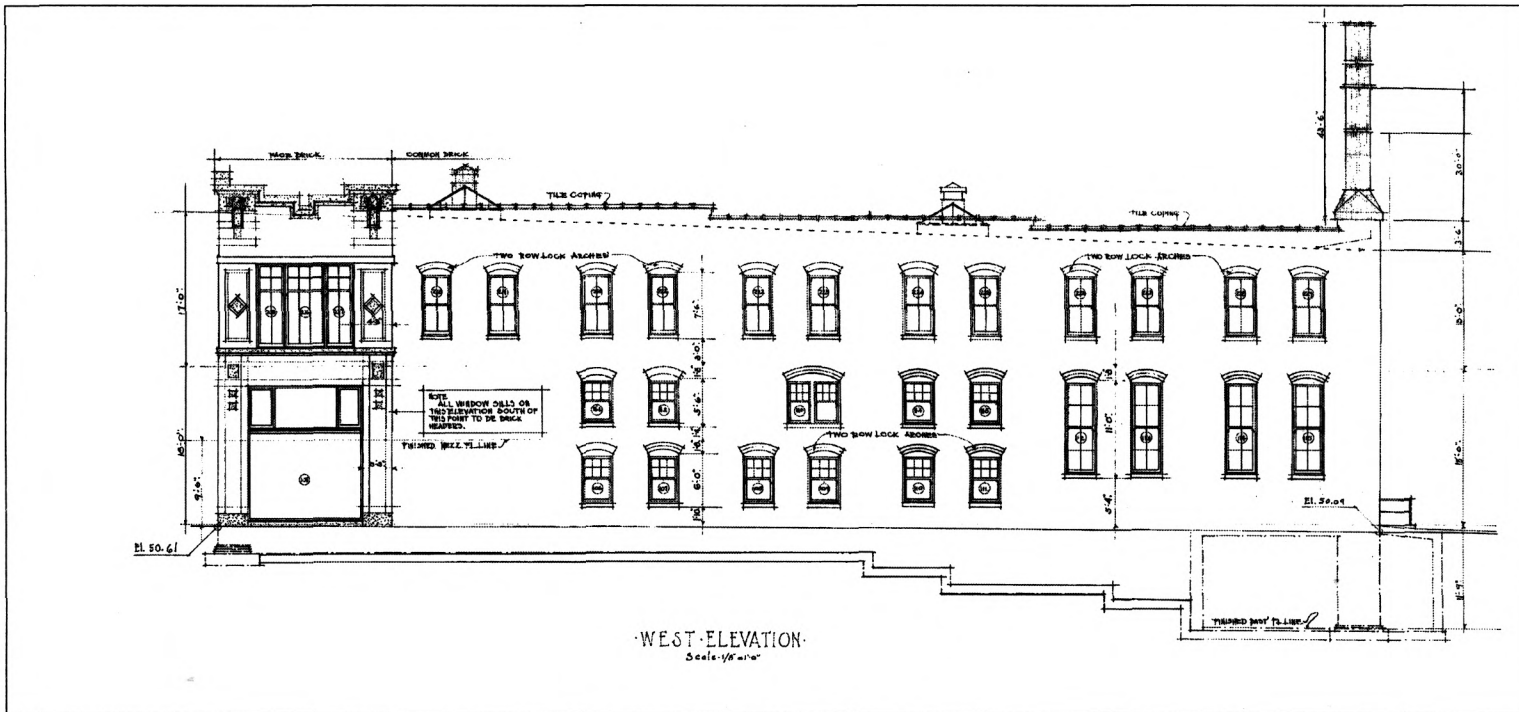


Figure 2: West Elevation Drawing
(Original architects plans, Proudfoot, Bird and Rawson, Des Moines, 1916)

There is no usable basement apart from the boiler room and coal rooms. The boiler and coal rooms (Figures 2, 12) are excavated in the southwest corner of the original building and the entrance to these areas is largely sealed off by the rear addition. These areas have reinforced concrete roofs. The elevator pit is in the center of the plan and set to the east of the main boiler room while the coal room and chimney are at the west end, the latter being incorporated into the southwest corner of the west wall (Figure 3). A metal chimney (only the base of which survives) originally towered above the building (30 inches in diameter, 43.5 feet high above the chimney base). The coal room and its manhole extend beyond the rear wall (Figure 2).

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D. S. Chamberlain Building

Polk County, Iowa

When the rear addition was designed, the third floor brickwork was corbelled above the metal base of the chimney in lieu of simply extending the brick chimney up one story.

The original interior plan allowed for open garage areas in the southeast and southwest ground floor corners, each of these having full-height ceilings (Figure 13). The elevator was used to haul automobiles only to the second floor, as each garage originally had its own rear centered garage door. The mezzanine extends as a bridge across the center of the plan and the elevator was used to deliver parts and supplies to storage areas on the mezzanine level. Twin concrete wash racks originally flanked the hall and stairwells and toilets flanked the elevator on each side of the plan (these elements are non-extant).

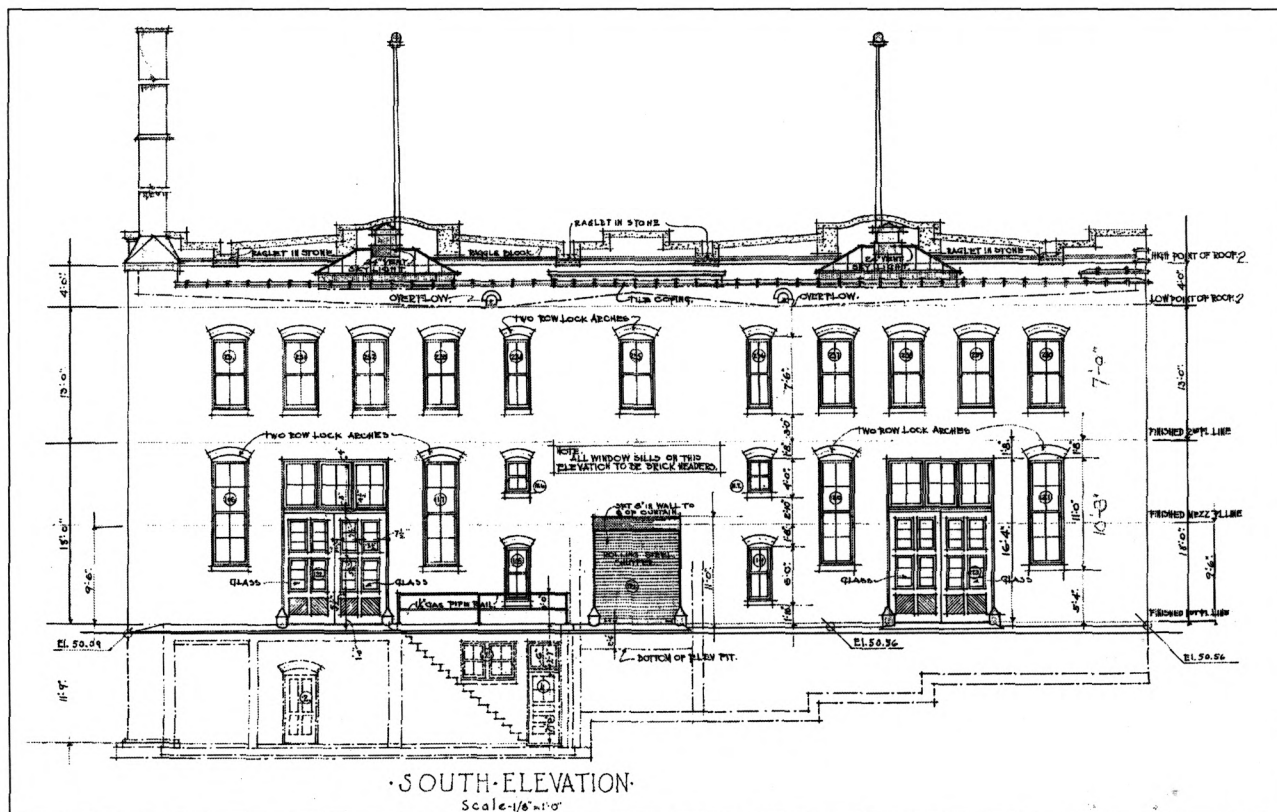


Figure 3: South/Rear Elevation Drawing
(Original architects plans, Proudfoot, Bird and Rawson, Des Moines, 1916)

The 1935-36 rear three-story addition was actually planned by the same architects in September 1919 but construction was delayed for 15 years (Figures 4, 5, 14-16). While any number of post-war related economic factors likely delayed this planned enlargement, the point of interest is that a successor tenant firm simply followed the old plans to the letter. Thus, the rear windows have the same arched brick lintels as do the sidewall windows.

The west wall fenestration of the Chamberlain building consists of elongated double-hung sash of even length, and the openings and distribution naturally reflect the interior layout of plan (Figure 2). Only the second floor is regularly and fully fenestrated, reflecting its open and continuous plan. The north bay is a continuation of the façade design with a full-sized display window. The next bay to the south is devoid of openings below the uppermost floor due to the presence of the

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D. S. Chamberlain Building

Polk County, Iowa

showroom. The central three bays have paired half-sized openings due to the presence of the mezzanine and the rearmost two bays, with elongated lower level window openings, marked the location of the rear corner garage area.

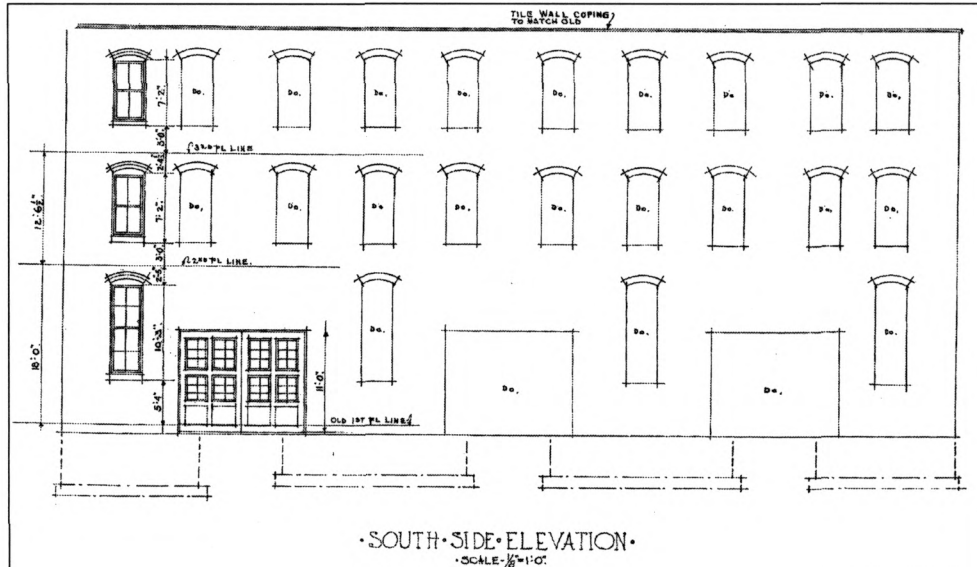


Figure 4: Addition Drawing, South/Rear Elevation
(Original architects plans, Proudfoot, Bird and Rawson, Des Moines, 1919)

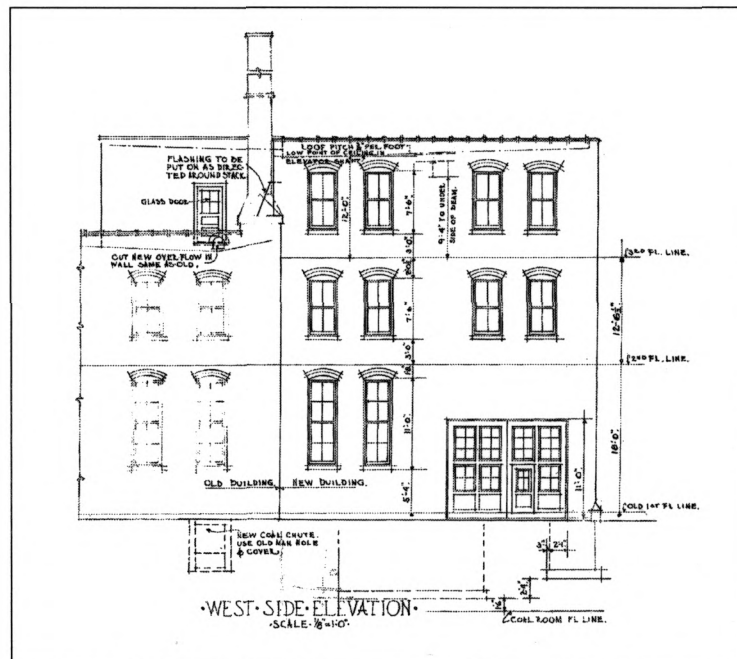


Figure 5: Addition Drawing, West Wall Elevation
(Original architects plans, Proudfoot, Bird and Rawson, Des Moines, 1919)

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D. S. Chamberlain Building

Polk County, Iowa



Figure 6: Surviving framed plaster wall panels, east sidewall, surviving tile floor, view southeast
(Photo by J. Jacobsen, August 2006)



Figure 7: Decorative brackets at showroom juncture of former partition, view southwest
(Photo by J. Jacobsen, August 2006)

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D. S. Chamberlain Building

Polk County, Iowa



Figure 8: Surviving elongated 2/2 window on former or original rear wall, view northeast
(Photo by J. Jacobsen, August 2006)



Figure 9: Structural truss above showroom area on second floor, view southeast
(Photo by J. Jacobsen, August 2006)

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D. S. Chamberlain Building

Polk County, Iowa

Alterations Since Construction:

Just three substantial building permits have been located for the Chamberlain building. Payne Motor Company received a \$7,000 permit January 11, 1927 for unspecified work. The Iowa Auto Market received a \$1,000 permit on April 7, 1933, also for undetermined modifications. Dr. L. H. Chamberlain obtained a \$5,000 permit for the rear addition on November 29, 1935 (Des Moines Building Permits).

Overall the building exterior and interior are remarkably well preserved. The east side façade display windows survive at least in their original scale, while those on the west side have been reduced. But all of the display window infill is of recent vintage (c.1980s). The west front side display window has lost all of the original base plate and all window material have been lost. It is very probable that all or most of the transom area in this same area survives intact but this area remains covered. The west side of the store front has also lost its base plate. The entire upper façade survives intact and minimal damage was suffered when a glass paneled slip screen was installed in the 1980s. That same area was coated with a black spray paint above the transom level, as a part of the slip screen application. This paint can be readily removed. Large areas of original façade brick on the ground level retain their original polychrome coloration and much of the stone base plate and entry surrounds survive. Remarkably nearly all of the façade upper level window sash survive intact.

On the west sidewall all of the windows have been sealed with panels and/or replaced with metal frame windows. A garage door was cut through the base of the fourth bay (c.1936) and the 1936 garage door at the southwest corner was bricked in and replaced with a band of four square lights (c.1980). The rear/south wall similarly has had its window sash replaced and the two outermost of four first floor windows have been bricked-in (c.1980). The two easternmost garage doors have also been bricked in, but the brickwork is recessed relative to the main wall plane. The surviving garage door opening has replacement doors. The east sidewall was not initially fenestrated given the assumption that a building would be constructed there. Thus the façade was not wrapped around the front bay and indeed the architects didn't even bother to draw that elevation, apart from the light well. The east sidewall gained a rear garage door (c.1980), a single horizontal mezzanine level window (c.1980), and a side storefront entrance on the northeast corner (c.1980).

Inside, the building is surprisingly well preserved. The showroom floor tile, some marble base plate and much of the wood framed plaster panel wainscot survive along both sidewalls of the showroom area. On the west sidewall, green marble baseboard survives as well. In recent years, the mezzanine level was extended to the front wall, but this work has been removed. The structural mezzanine survives. The twin-office/lounge mezzanine areas that once flanked the front stairways have been lost. A pressed tin ceiling remains beneath the mezzanine and plaster ceilings survive on the mezzanine. Original wood floors survive throughout the upper floor levels. Sometime after 1950 (see Sanborn Map, Figure 24) the elevator was removed and an angled auto ramp was built into it, allowing the moving of cars to the third floor. The front portion of this ramp, outside of the elevator shaft, has also been removed. The stairs adjacent to the elevator have been rebuilt. Within the 1936 addition, the third floor wooden floor shows signs of excessive wear. The original rear exterior wall remains in place, with most of its window openings also surviving. Garage doors were enlarged on either side and the west door shows evidence of successive enlargements with increasingly massive lintels being placed below the older on.

Integrity Analysis:

The exterior of the Chamberlain Building maintains a high degree of all seven aspects of integrity: location, design, setting, materials, workmanship, feeling, and association. The integrity of the immediate setting of the building, illustrated by its commercial setting and streetscape, has been somewhat compromised by the substantial loss of other similar buildings along Locust Street and by the establishment of an open park immediately north. Still, two apartment buildings located to the northeast and opposite the Chamberlain building, preserve much of the contextual feel of the area, and several other auto-related buildings survive immediately to the west, along the same side of the street as the Chamberlain building. The building retains

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**National Register of Historic Places
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D. S. Chamberlain Building

Polk County, Iowa

its ability to maintain integrity of feeling and association. Its original owner and designers would readily recognize it and the survival of original materials both inside and out, outpaces most comparable commercial buildings of its era. The retention of the human scale components of the building underscore the requisite integrity for the building's Criterion A association. Because the Chamberlain Building is being nominated under Criterion C, design, materials, and setting are especially important. Workmanship, reflected in the well-preserved facade, is also of particular importance. The overall form of the building has remained unchanged once it assumed its intended scale in 1936. No other substantial additions or alterations have been built. The majority of the original architectural materials and detailing, including cornice, window openings, and ornament remain as built. The most notable loss has been the expected replacement of the original storefront material, but even there, substantial portions of the original base plate remain and the original scale of the display windows is retained. The interior retains substantial components of its original ornamentation and layout and temporary stud walls of recent date can be readily removed. On the upper level and within the rear addition, the only substantial changes entailed the building of a ramp within the original elevator shaft and the reconstruction of the stairways and bathrooms that flanked that elevator.

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D. S. Chamberlain Building

Polk County, Iowa

Historical Floor Plans:

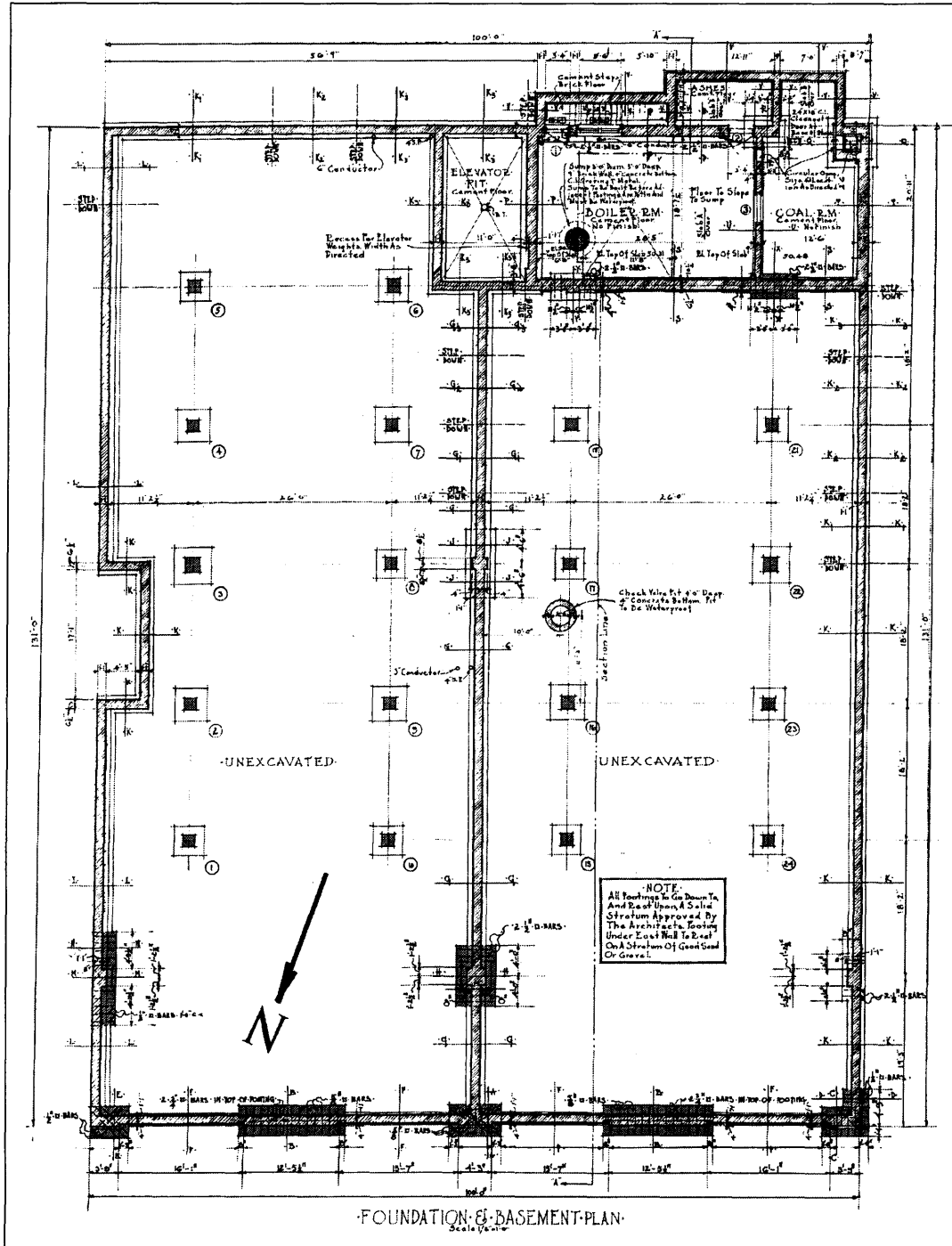


Figure 10: Historic foundation floor plan (Proudfoot, Bird & Rawson, architects, 1916)
(structural walls and columns shaded gray)

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D. S. Chamberlain Building

Polk County, Iowa

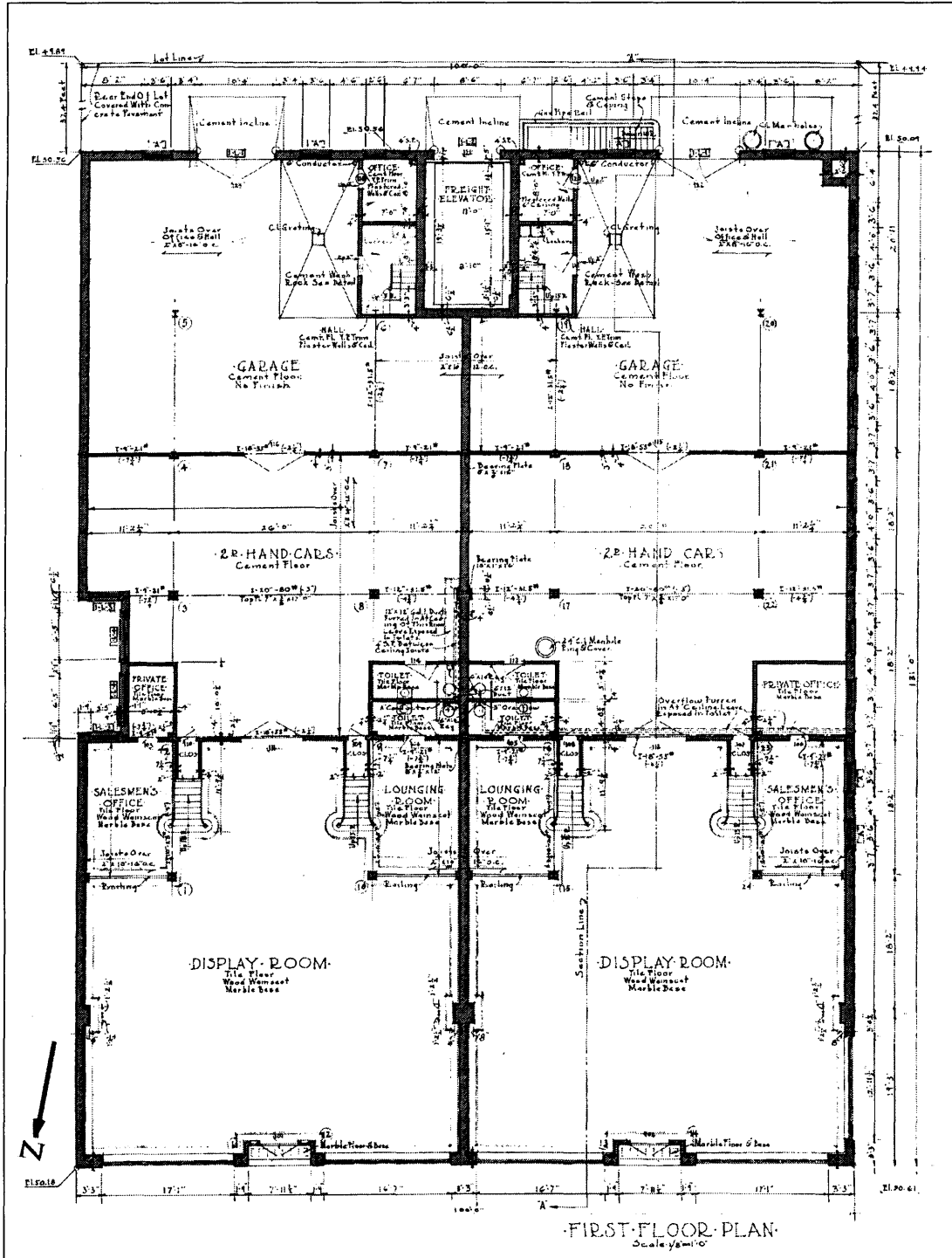


Figure 11: Historic first floor plan (Proudfoot, Bird & Rawson, architects, 1916)
(structural walls and columns shaded gray)

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D. S. Chamberlain Building

Polk County, Iowa

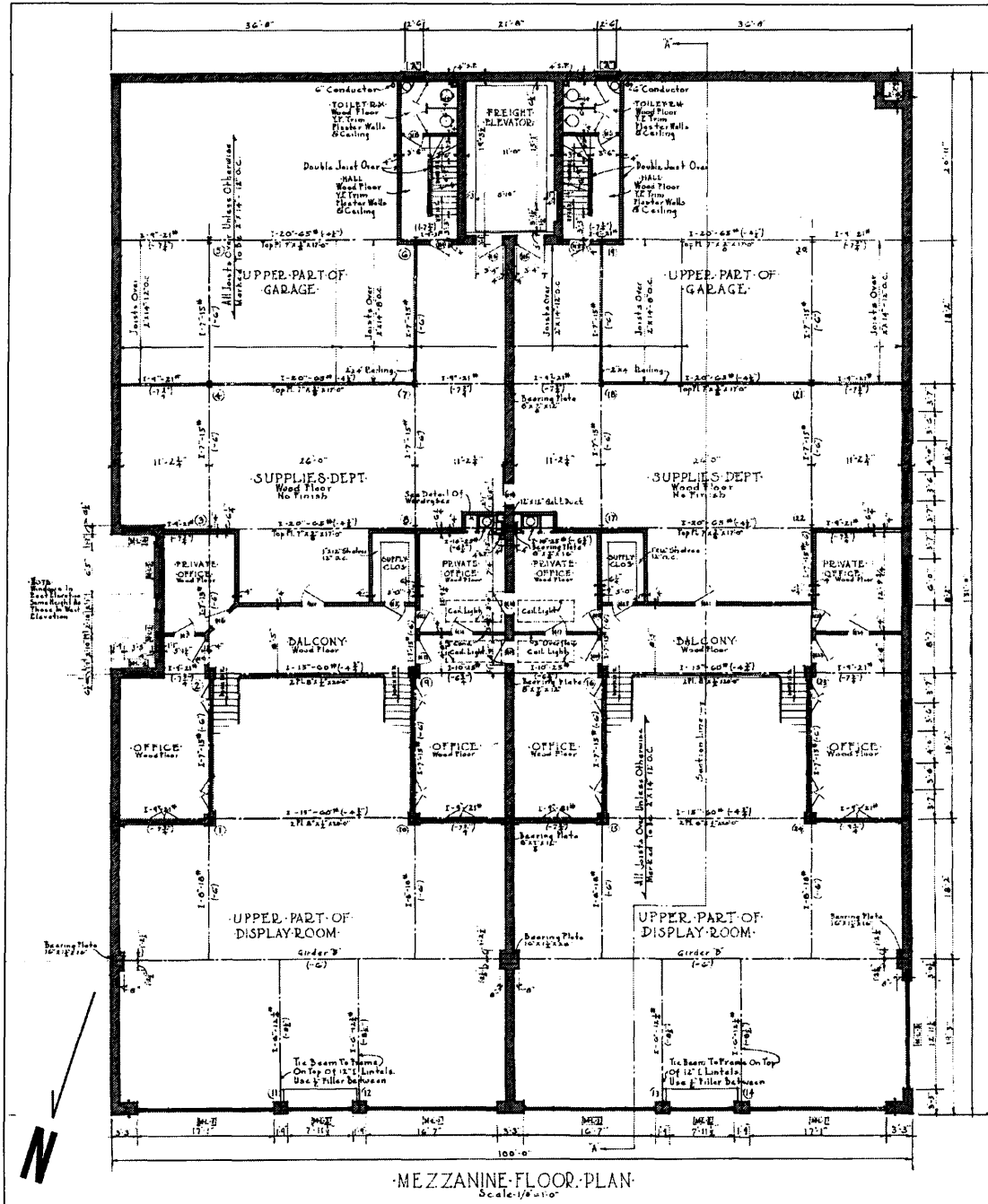


Figure 12: Historic mezzanine floor plan (Proudfoot, Bird & Rawson, architects, 1916)
(structural walls and columns shaded gray)

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D. S. Chamberlain Building

Polk County, Iowa

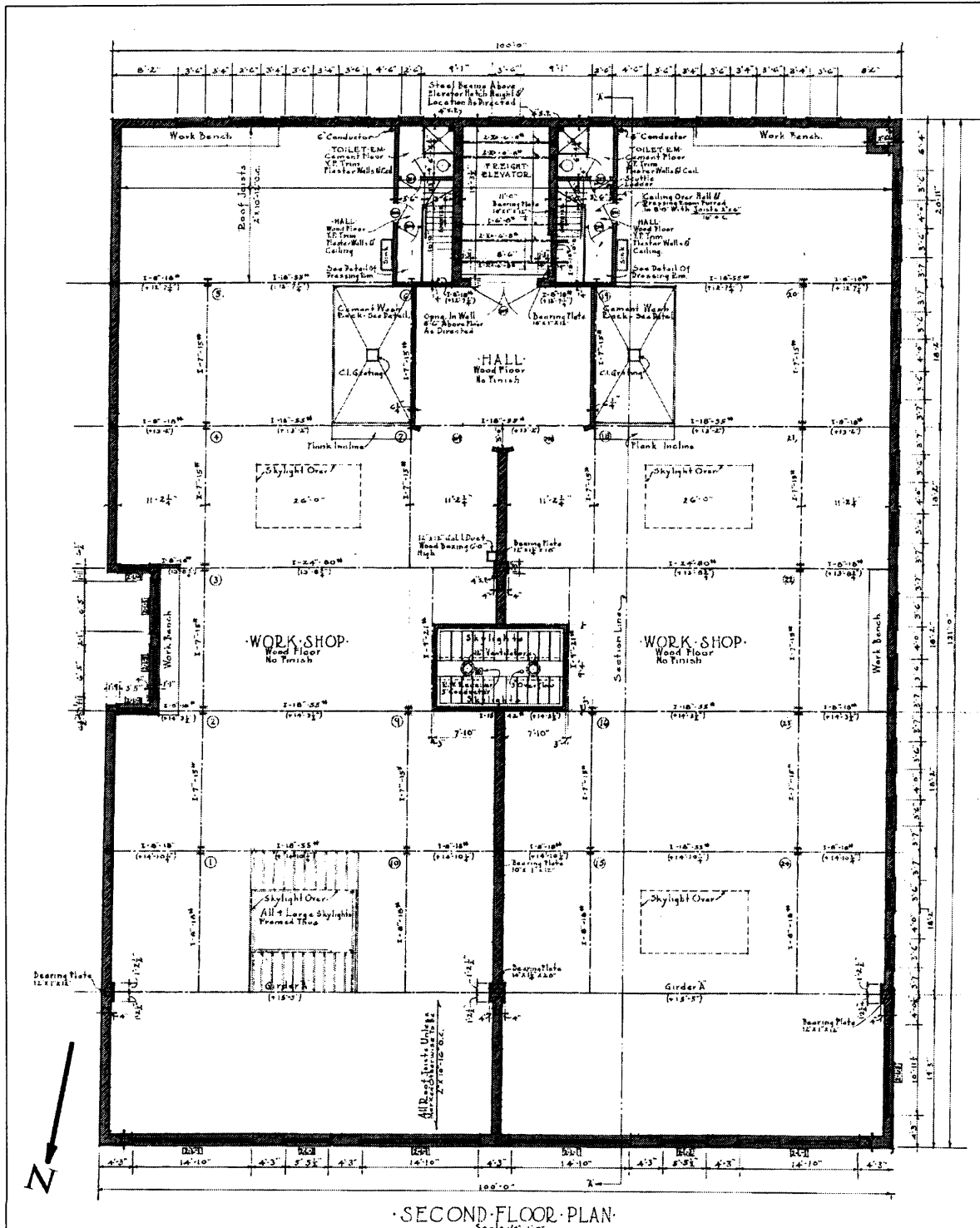


Figure 13: Historic second floor plan (Proudfoot, Bird & Rawson, architects, 1916)
(structural walls and columns shaded gray)

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D. S. Chamberlain Building

Polk County, Iowa

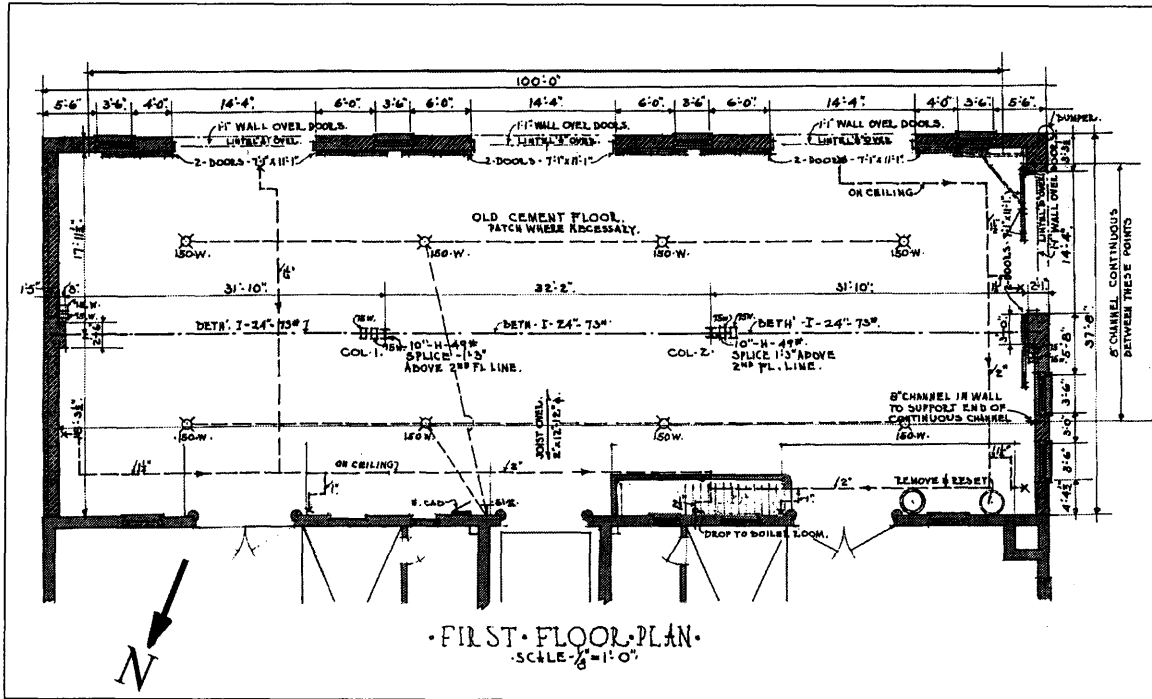


Figure 14: Historic addition first floor plan (Proudfoot, Bird & Rawson, architects, 1919)
(structural walls and columns shaded gray)

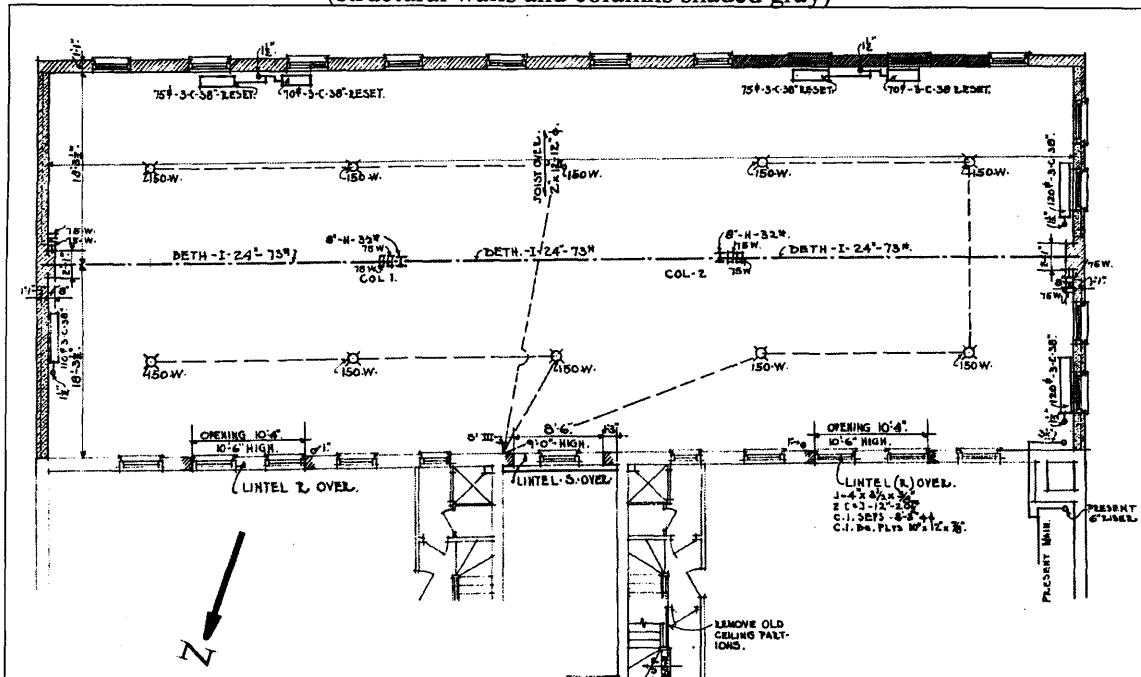


Figure 15: Historic addition second floor plan (Proudfoot, Bird & Rawson, architects, 1919)
(structural walls and columns shaded gray)

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National Park Service

National Register of Historic Places
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D. S. Chamberlain Building

Polk County, Iowa

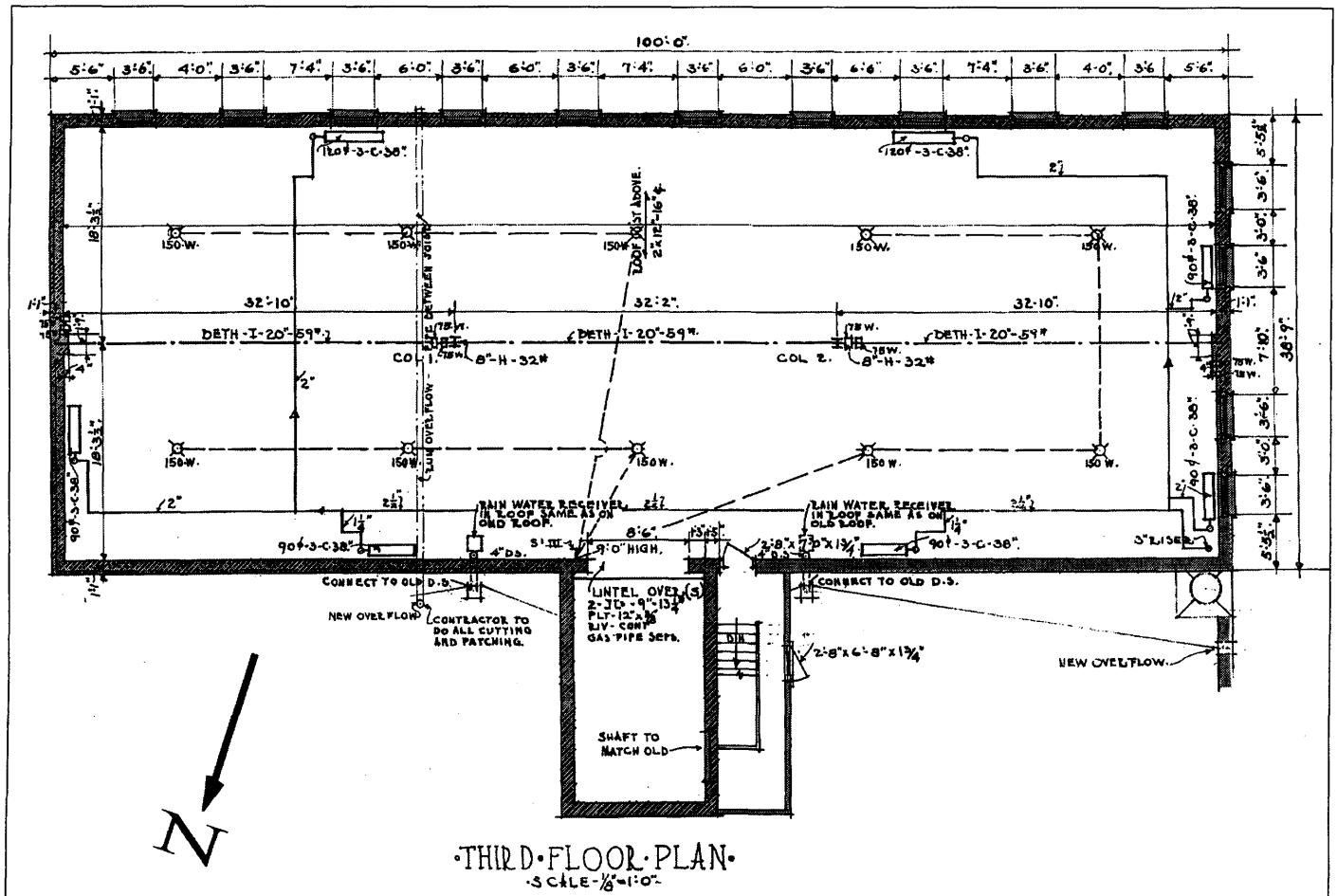


Figure 16: Historic addition third floor plan (Proudfoot, Bird & Rawson, architects, 1919)
(structural walls and columns shaded gray)

The stairway to the west of the former elevator housing leads to the roof and the elevator penthouse. The uppermost level of the stairwell is a frame structure.

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D. S. Chamberlain Building

Polk County, Iowa

Current Floor Plans:

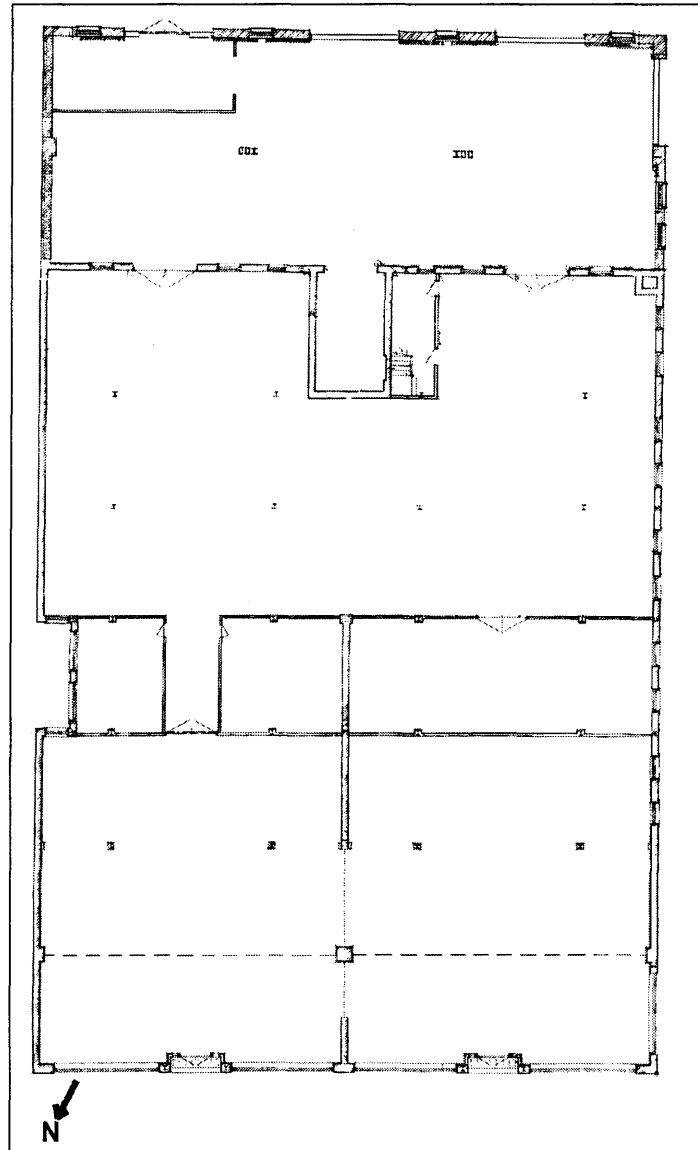


Figure 17: Current First Floor Plan

The original rear wall remains in place. A second elevator of recent vintage occupies the southeast corner of the rear addition. The east stairwell alongside the original elevator has been removed. Metal stud walls have divided up the area beneath the mezzanine. Mezzanine enlargement remnants fill the front bays on this level and there is a stairway to the east of the west front entrance. These were being removed in 2006 and it is planned that they be fully removed as part of the planned redevelopment of the property. That plan also plans to restore the stair approaches to the mezzanine and to utilize the original mezzanine area (temporary mezzanine area not shown on the plan, it occupies the front of the west storefront area).

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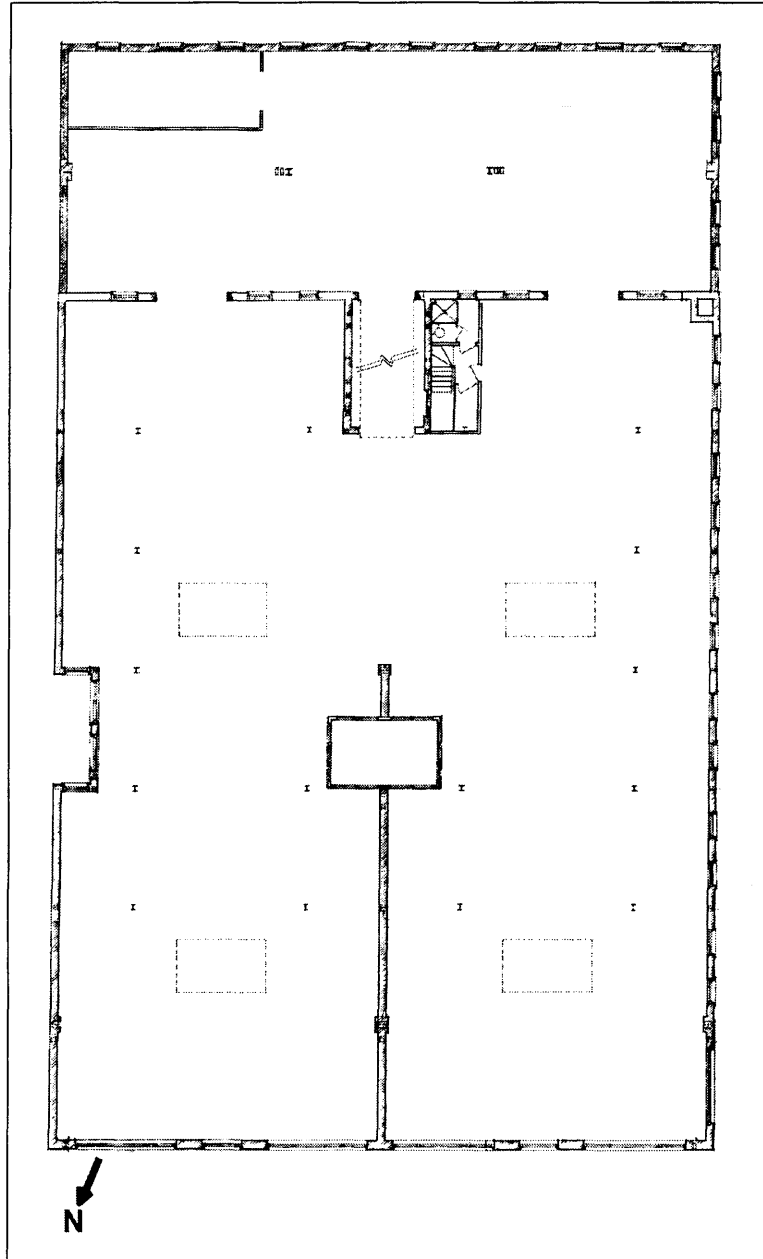


Figure 18: Current Second Floor Plan

This level remains open. Figure 18 depicts the locations of the four skylights, two of which are centered on each half of the plan. It also shows the central light well that provided natural light to the offices on the mezzanine area. All of these roof openings remain structurally in place and have only been sealed off on their undersides with drywall. Note the absence of columns in the rear center of the plan. The roof is carried on a massive central north/south running beam. The elevator shaft is filled on the second floor with the remnants of an auto ramp that allowed cars to be transferred from the third floor rear to this level.

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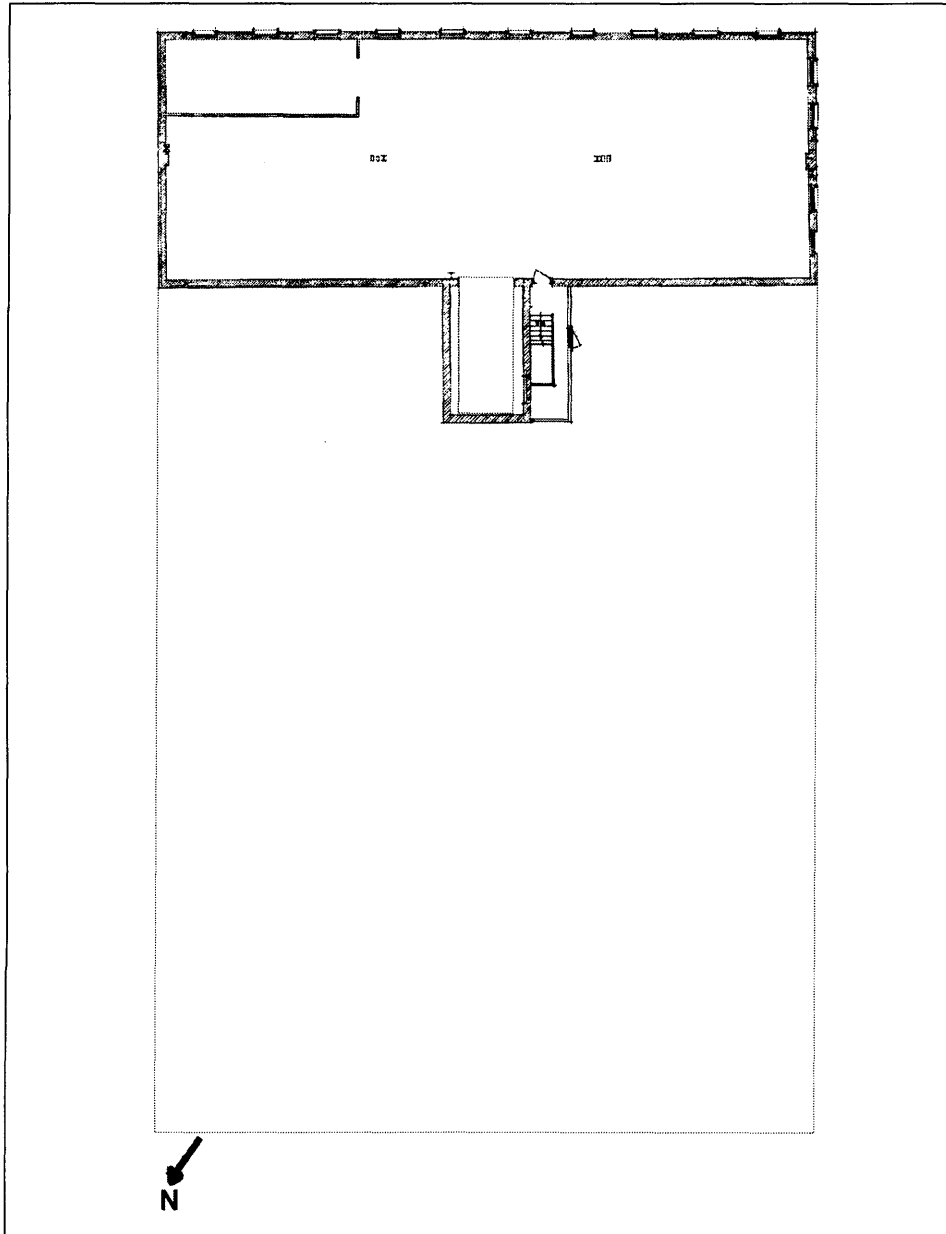


Figure 19: Current Third Floor Plan

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8. Significance Statement:

Significance Summary:

The D. S. Chamberlain Building (1916-17) is locally significant on the basis of its architecture (Criterion C) and for its historical association with the theme of commerce (Criterion A). Its architectural significance is due to its being a well-preserved example of an increasingly rare automotive garage example of a once much more numerous array of similar buildings that once comprised Des Moines' Auto or Motor Row. It is also individually significant as a well preserved and important design of the noted Iowa design firm of Proudfoot and Rawson. This design is the earliest surviving automobile garage example that this architecture firm produced. The firm is generally accepted as being Iowa's preeminent designers during the early 20th century. While a smaller scale example of their work, the quality of the building design is substantiated by the use of an elongated Chicago-style window form on the upper façade along with a rich mix of decorative inserts, combined with a polychrome brick facade. This design example is of particular interest because the architects designed a rear addition that was finally built in 1936, 19 years after the original construction.

The claim to historical significance for the Chamberlain building's association with commerce, particularly the early automobile trade sector is based upon the dominant role that Des Moines played in the emergence of automotive sales. The *Des Moines* magazine of May 1916 observed that "Practically every car sold [in Iowa] is handled in Des Moines...Accessory houses cover not only the state of Iowa, but adjoining states, and their ramifications are great indeed. Missouri, Nebraska, the Dakotas and even further territory, are reached by supply houses in Des Moines." A substantial "Auto Row" commercial district developed along the western edge of downtown Des Moines during the pre-World War I years and the Chamberlain building, designed to house dual dealerships, reflects the emerging form of the ideal auto showroom. The automobile sales and service district, once considered to be National Register-eligible, is no more, and this is one of a small handful of surviving showrooms. Its integrity, both inside and out, is remarkable. The building enjoyed a lengthy and sustained direct association with automotive sales throughout the majority of its history.

The associated multiple property document, titled "The Architectural Legacy of Proudfoot and Bird in Iowa, 1882-1940" traces the history and significance of Proudfoot and Bird architects, and evaluates them as being the "foremost architectural firm" practicing in Iowa between 1910 and 1925. The document defines automotive related buildings as a subtype of the commercial designs that this firm completed. Seventeen examples were credited to this subtype, although for whatever reason, the Chamberlain building drawings were missed when this study was underway. Automotive-related designs of this firm are described as follows:

The firm's one and two-story commercial buildings, especially automobile-related ones, are typically brick, have fairly simple geometric detail (including the diamond shape [inserted ornamentation on the façade]), and contrasting stone or concrete trim. However, a number of these smaller buildings are (or were) faced with smooth stone and are executed in the Art Moderne or International styles.

The Gothic Revival style is also attributed to many of these designs and the Chamberlain building could be said to reflect what is termed a "Collegiate Gothic" style or elements of that style (Long, pp. E.2-E.14).

The survey determined that "most of the known [automobile-related] examples in Des Moines have been altered." Had this building been considered at that time, it too would have been rated as being too altered for eligibility consideration.

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However, given the recent removal of the slip screen, this building would now be rated as a well-preserved example of this building subtype (Long, p. E.14).

The multiple property document directs that Criterion C-Architectural significance applies when a building qualifies as representing the work of this master architectural firm. Registration requirements require attribution to the architectural firm, a test that is satisfied by original dated plans. It further states that the building must be of "sufficient design quality to embody distinctive and successful characteristics of the firm's design work. While the class of smaller commercial buildings (commercial garages, auto showrooms and filling stations) was for the most part a new design niche for a firm that was otherwise focused on much more substantial institutional, commercial or residential buildings, design opportunities like the Chamberlain building frequently involved important clients and provided an opportunity for some innovation. The special design merits of the Chamberlain building include its double auto showroom arrangement, its use of the Chicago-style windows, the use of trusses in its support system, the combination of specialized decorative inserts and terra cotta with a polychrome brick façade, the opportunity to fenestrate the sidewall of an automobile design (as well as the related opportunity to extend the façade along the front of the west sidewall) and the provision for a future expansion (Long, pp. F.3-F.4).

The building has to illustrate the architectural heritage of Iowa during the years 1882-1940. The Chamberlain building meets this test for the same reasons as already stated previously, but it also represents the architectural evolution of the automotive salesroom within the context of Des Moines, the center of early automobile sales and service in the state. Finally, the multiple property document establishes an integrity test, stating "some ground floor alterations may be acceptable..." Taller buildings in particular are less effected by such changes, but in this case, the expected replacement of display window openings and materials is counterbalanced by the retention of interior storeroom floor, ceiling and wall surfaces, as well as the excellent state of façade materials, massing, ornamentation and design (Long, p. F.4).

Architects Context:

The Des Moines architectural firm that marshaled the design talents of William Thomas Proudfoot (1860-1928); Harry Dustan Rawson (1873-1934), George Washington Bird (1854-1953), Henry Clark Souers (1888-1970), John Woolson Brooks (1897-1982) and Elmer Herman Borg (1893-1970) was considered by architectural historian Wesley Shank to have been "Iowa's preeminent early-twentieth-century architectural firm." Proudfoot & Bird first worked in Wichita, Kansas, and then Salt Lake City, Utah, before finally locating in Des Moines, in 1896. Within two years the firm gained statewide prominence with its awarded commissions for several state university buildings. The partners designed the Polk County Courthouse in the fall of 1901. Partner George Bird retired in 1913 and was not involved in the design of the Chamberlain building. The expanded firm of Proudfoot, and Rawson consequently designed the Chamberlain building in the fall of 1916. The firm was the subject of a 1988 historical study and multiple property document, authored by Barbara Beving Long and titled "The Architectural Legacy of Proudfoot & Bird In Iowa, 1882-1940." That study failed to identify the Chamberlain building as a product of this firm and the link was not discovered until a search of their archives inventory was conducted as a part of the research for this nomination (Shank, pp. 25-28, 30-32, 127-32, 138-39, 153-54).

The firm increasingly specializing in large public and commercial design projects by 1916 and designed very few residences. Twenty-two building designs by this firm, dating from 1911 through 1937, were located in the western portion of the West Side downtown. Only 11 of these survive and these are primarily major buildings (Hotel Fort Des Moines, 208-10 West 10th; Herring Motor Car Company, 912-22 West 10th and 106-10 West 10th; Successful Farming, 1714-20 Locust Street; Masonic Temple, 1007 Locust Street; Arlington Apartments, 1301 Locust Street; Homestead Publishing, 1921 Grand) or public buildings (Central Fire Station, 900 Mulberry Street; Polk County Courthouse, 6th and Court Avenue). Eleven designs involved

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the automobile retail or wholesale trade and two of these were multi-story automotive “department stores.” Nine remaining designs ranged from single to three-story buildings and the three surviving examples all stand on this same half block. The largest of these three is the Studebaker Corporation Branch House, 1436-44 Locust Street, a three-story massive block that dates to 1917. The Chamberlain and Apperson (1420-22 Locust, 1920) buildings are the others and the Chamberlain building is the earliest surviving example of a garage design by this design firm.

Automotive Sales Context:

Des Moines, the state capital, emerged as a state and regional wholesale and manufacturing center beginning in the late 1880s. This new-found surge of development reflected the advantages of geographic centrality, urban centralization, and the development of an excellent railroad shipping advantage. The automobile sales sector grew rapidly after 1910, and Des Moines entrepreneurs dominated every automobile make in terms of securing statewide or larger market distributions. Iowa was the ideal location for the auto entrepreneur given its national leadership in per capita auto ownership. By mid-1917, one in eleven Iowans had a machine, the highest ratio of any state, with California in close second place, with a ratio of a dozen souls to one automobile (*Des Moines* magazine, August 1917, p. 36).

An automobile sales architecture soon emerged with the largest firms building multi-story automobile “department stores” in the west end of the West Side downtown. Lesser firms first occupied single-story storefronts or former liverys, but by 1913 a single or two-story auto garage form was being constructed and increasingly, an “Auto Row” of these storefronts developed along Locust Street, just beyond the established downtown proper. The hubris of these early years of high profit and ever-expanding sales was directly reflected in the automobile sales architecture. While most Des Moines building clients were too conservative to actually combine automotive symbols in the building ornamentation, the best showroom-garages featured elaborately ornamented facades and immense and highly appointed showroom spaces. Offices were clustered in overlooking mezzanines and the “dirty work” of auto repair or maintenance was relegated to the rear portions of the building. Early garage designs provided front-end egress for automobiles but later designs such as this one, employed a traditional retail front and human-scale double doors, while the “machines” were brought in from alleyway doors.

By 1916, the motor trades industry in Des Moines was valued at \$12 million in annual trade. There were 111 motor related firms in the city (43 car dealers, 37 accessories dealers, 21 garage and repair, 11 oil six, and manufacturing) with 1,000 employees, an annual payroll of \$1 million, and capital investment worth \$2 million. These numbers excluded nine larger firms and 29 smaller ones which failed to provide figures. The *Des Moines* magazine issued a special motor trades issue in May 1916. It boasted:

Practically every car sold [in Iowa] is handled in Des Moines...Accessory houses cover not only the state of Iowa, but adjoining states, and their ramifications are great indeed. Missouri, Nebraska, the Dakotas and even further territory, are reached by supply houses in Des Moines.

The garage and repair subdivision of the Motor Trades Industry in Des Moines is of immense pretensions. It includes many very large and exceedingly well-equipped establishments.

In manufacturing, several establishments make specialty of welding in all its difficult methods; in construction of bodies, tops, truck work, and the like; and a tire factory is now in process of preliminary organization.

Among the most alert businessmen, some veterans, many of the younger type, are engaged in the Motor Trades Industry of Des Moines. Their investigation into Des Moines territory, their pushing out beyond the ordinary lines of the confines of Des Moines trade territory, their general relationship with civic and business affairs, leads to contemplation of them in the light of pioneers in an entirely new business era and business type.

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The industry has no fixed district in Des Moines, yet on West Locust and West Walnut, and on adjoining cross streets, there is building up a tremendously promising and interesting industry which it will pay all Des Moines to observe (*Des Moines* magazine, May 1916, p. 2).

The growing dominance of the lucrative motor sector was further reflected in the fact that the Motor Trades Bureau was the Chamber of Commerce's second largest with 71 member firms and 114 individual members just six months after it was formed in early December 1916 (*Register*, December 8, 1916, May 18, 1917).

"Motor Row" or "Auto Row" began its development along Locust Street, to the west of the downtown, by 1910 and by 1916, when the Chamberlain building was erected, the linear motor district had assumed substantial form:

The motor district is rapidly increasing in ramifications. Many new buildings are being erected, and nearly every day there is a new move made. The district will, in a year or so, become one of the largest and most attractive in any American city (*Des Moines Magazine*, July 1917, p. 30).

The district enjoyed its zenith during the middle-1920s before the automobile industry evolved into a phase of mergers and consolidations. The surviving auto vendors and their increasingly smaller number of car lines suffered the reduction of their once massive franchise areas. After World War II, East Des Moines largely supplanted the original automobile sales district. In the year 2000, a substantial portion of the original West Side automobile district was demolished as part of a public redevelopment effort. The only surviving salesroom and garage buildings are located along Locust and Walnut streets, west of 13th Street. The majority of these are doomed as the development of the Gateway West park project spurs redevelopment around its periphery.

Building and Developer History:

In Des Moines it was commonplace to have new commercial buildings built by speculative investors and it was also common to build on leased ground. The Chamberlain family established a drug manufacturing company in the city in 1881 and the firm was re-titled the Chamberlain Medicine Company in 1892, and after 1930, Chamberlain Laboratories which produced only cosmetics. The company was one of a number of nationally known cosmetics manufacturers which collectively made Des Moines a major center for the production of that profitable range of products. David Stanley Chamberlain (1848-1933), his brother Lowell H. Chamberlain (1851-1922), and sister Izanna Chamberlain (1854-1940) were the founders of the firm and the Chamberlain building was one of a relatively small number of speculative ventures that Davis S. Chamberlain undertook beginning in 1903, all of which were designed by Proudfoot, Bird and Rawson, architects. The list included the Chamberlain Hotel (1903, non-extant), a single-story auto garage/showroom (Locust near 11th, 1913, non-extant), a personal residence (1916), the Chamberlain building (1916), and a garage/showroom for Chamberlain-Mitchell (building not identified, 1931) (Baldwin, p. 53; *Register*, March 15, 1933).

Chamberlain secured the requisite two full lots for the Chamberlain building in transactions dated May 5 and October 6, 1916. Chamberlain's and similar acquisitions along "upper" Locust Street drew the attention of realtors and businessmen to that fast-growing part of the new downtown. The same week witnessed half a dozen similar property transfers and a continued "phenomenal rise in the prices paid." The *Register* predicted that land on "Auto Row" would sell for \$1,000 a frontage foot within ten years (a prediction that was correct). The same source observed: "The demand for locations in this district, brought by the great increasing automobile trade, fills any building as soon as constructed." The architects of the Chamberlain building finished their plans in November 1916, and advertised for bids with a submittal deadline of November 20, 1916. The contracts

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were awarded on November 21, 1916, with W. F. Kucharo securing the contract as general contractor. The Electrical Equipment Company of Iowa and Walker Plumbing and Heating also received sub-contracts. Chamberlain was issued a \$30,000 building permit on December 1, 1916, to build the Chamberlain building (*Register*, October 15, December 3, 1916).

Actual construction began immediately, given that when the building was finished, it was credited with having been built “[during] the past winter.” The construction coincided with the organization of a separate Chamber of Commerce bureau for the “Motormen” in early December (*Register*, December 8, 1916).

Newspaper coverage of the building’s construction was apparently lost in the public focus on the entry of the United States into the Great War. The only references to the new building came on May 4, 1917 when it was announced that Payne Motor Company and the L. W. Taylor Motor Company, dealers respectively in the Paige and Saxon automobile lines, had occupied the finished building. Some delay in the completion likely resulted from a four-week-long building trades strike that had started on April 2 and was settled only at the end of that month (*Register*, April 29, May 4, 1917).

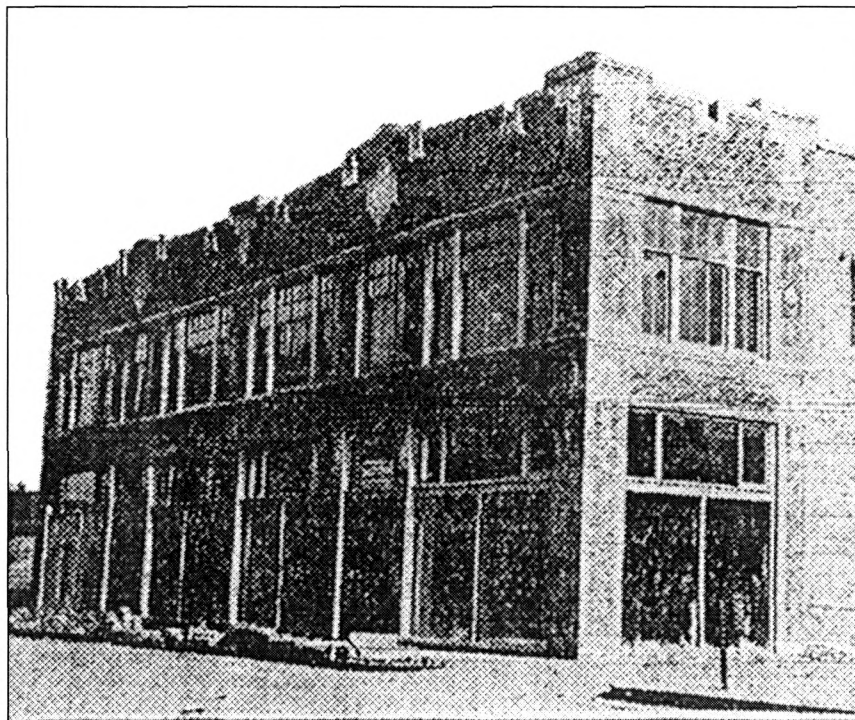


Figure 20: Newly completed building, view southeast (*Capital*, May 18, 1917)

The Chamberlain building construction coincided with that of the Des Moines Buick Company Garage, located two lots immediately east at 1208-10 Locust. The dual construction marked the building up of what had been a residential half block, starting from its east end. Both buildings were “presented” in early May in the *Register* (*Register*, May 18, 1917).

The Chamberlain building was built to house the Taylor Motor Company (first located at 1312-14 Locust) and the Payne Motor Company (1316-18 Locust). Clearly the building was designed to house two separate firms at a time when auto dealers could still afford and utilize a traditional scale storefront. L. W. Taylor organized Taylor Motor Company in mid-

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August 1915 and Taylor had the central Iowa distribution for the Saxon automobile. The business was originally and previously located at 1016 Locust Street. Its 1916 annual banquet at the Chamberlain Hotel drew 125 retail auto dealers to the city. By the time the firm relocated to this address, Clyde W. Payne, of Payne Motors, was the company president and T. F. Shaw was the business manager. The company disappears from the record by 1919, having been apparently absorbed by Payne Motor Company (*Horseless Age*, August 15, 1915, p. 209; *Register*, February 20, 1916).

Dr. Clyde Payne was born in 1872 in Henry County, Iowa. By 1910 he resided in Boone, Boone County, and was an eye and ear doctor. Ten years later, he was residing in Minneapolis, where he was recorded as an auto dealer. In early September 1913 he organized the Payne Motor Company and had the agency for the Paige and Lozier automobiles in Central Iowa. The first location for the new firm was at 1012-14 Locust Street. Roy O. Gresham (1888-?) was the actual business manager through 1927. After 1919, Dr. Payne was a Minnesota resident. In September 1919 the original architects were commissioned to design a three-story rear addition to the original building. It is probable that the high postwar labor and materials costs prevented the planned construction and it took 15 years and a different tenant firm to do the building. Algernon B. Rockwell (1887-?) became company president in 1927 and the firm was re-named Rockwell Motor Company. The company was last listed in city directories in 1930 and the building was vacant as of 1931 (*Horseless Age*, September 3, 1913, p. 391; Federal Census Records; Payne-1880, T9, R-343, p.1881; 1900-T623, R-418, p. 100; 1910-T624, R-392, p. 115; 1920-T625, R-836, p. 226; Gresham-1920, T625, R-507, p. 223; Baldwin, p. 223).

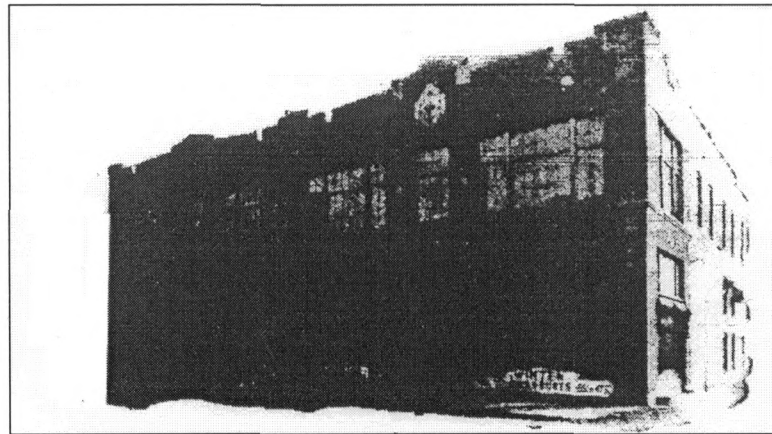


Figure 21: Payne Motor Car Company (*Register*, April 4, 1926)

Interim tenant auto firms were the Ray Dodge Chevrolet Company (1932-33) and the Iowa Auto Market (1934-35). Manbeck Motor Sales Company, organized in 1916, had the Iowa franchise for Plymouth and Chrysler automobiles. Earl N. Manbeck (1887-1967) was the company president, and his brother Roland Manbeck (1880-1974) was vice president. The company was sufficiently successful by 1935 that it leased the Chamberlain building and built the 1919 rear three-story addition that had been envisioned by Payne's firm, but had not been built. The property owner and trustee, Lowell Holbrook Chamberlain (1876-1946), the son of D. S. Chamberlain, was issued a \$5,000 building permit on November 29, 1935, to build the three-story brick rear addition to the building. The new tenant's grand opening took place on January 31, 1936. The company expanded its site usage to include 1304-06 Locust and the vacant lot at 1308 Locust by 1942 but it progressively reduced its turf to just the Chamberlain building by 1952. It was renamed Friedman Motors and was relocated to 1304 Locust by 1957 (Des Moines building permits; Baldwin, pp. 171-72; *Tribune*, July 19, 1974, *Register*, January 8, 1967).

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The Manbeck Motor Sales Company reincorporated for 20 years on July 9, 1938 with a capital stock of \$110,000. Eleven years later, the company assumed a purchase contract for the land and received title on May 12, 1949. The property was leased to Friedman Motors Inc. in December 1956 and the lease was assigned to Arthur K. Jones in 1957. More recent building tenants were Ben Handler Motors, dealers in Plymouth and Dodge automobiles (1957), Forman Ford Glass Company (1960-2000) and Arthur K. Jones (2003) (Property abstract).

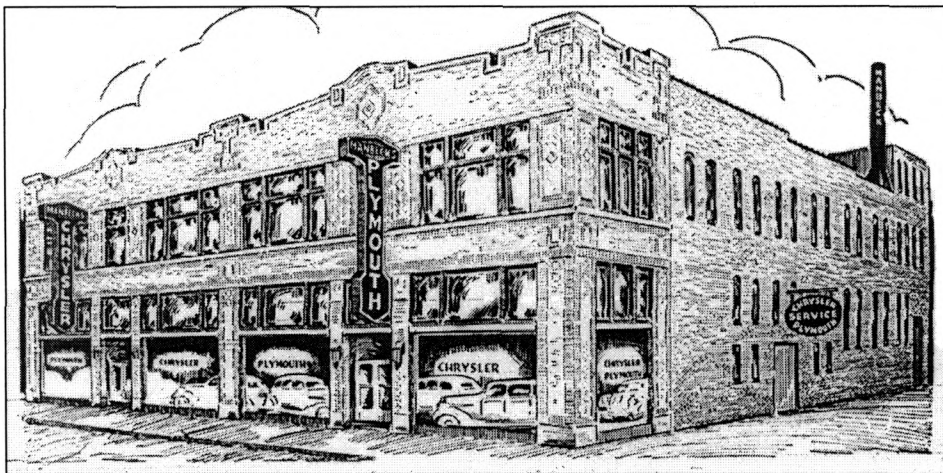


Figure 22: Enlarged Manbeck Motor Sales Company (*Tribune*, January 29, 1936).

Building Parcel Ownership History and Development:

Prior to the construction of the Chamberlain building, this property was occupied by two frame residences. Davis S. Chamberlain owned the property effective October 6, 1916 (Property Abstract).

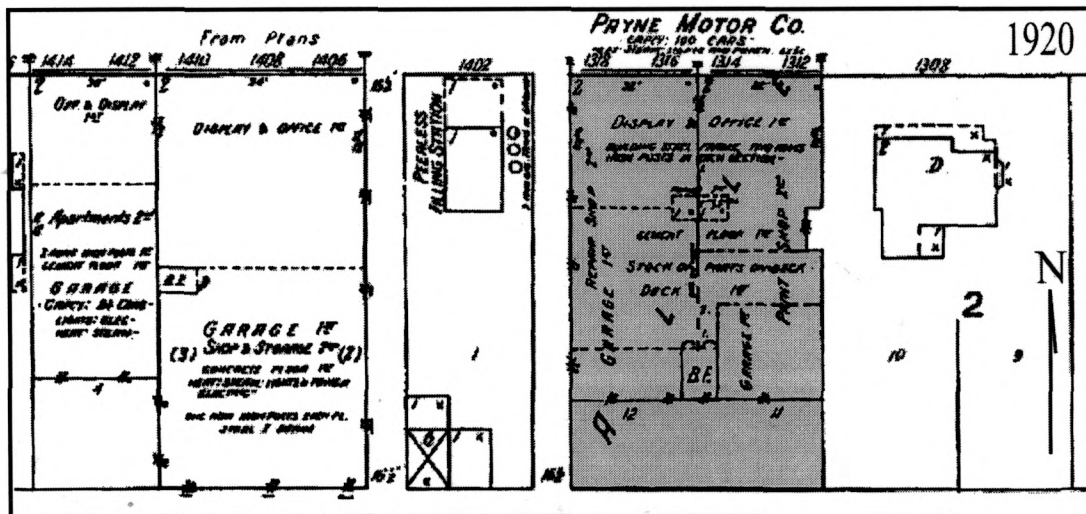


Figure 23: Detail, 1920 Sanborn Fire Insurance Map (building shaded)

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Davis Chamberlain died in 1933 and his son Lowell H. Chamberlain received the property through the estate. Manbeck Motor Company acquired Lot 10 from the Hubbell Trustees in 1949 and sold that lot to Arthur K. Jones in 1959. Jones (?-1986) and his estate owned Lots 11-12 as of 1958. Jones' Estate sold the whole to the West End Renovation East, LLC, the current owner, in 2005. Figure 23 depicts the new Chamberlain building as of 1920. Note the alley (established in 1909) to the west of the Chamberlain building and the service station. This alley allowed for the fenestration of the west side of the Chamberlain building.

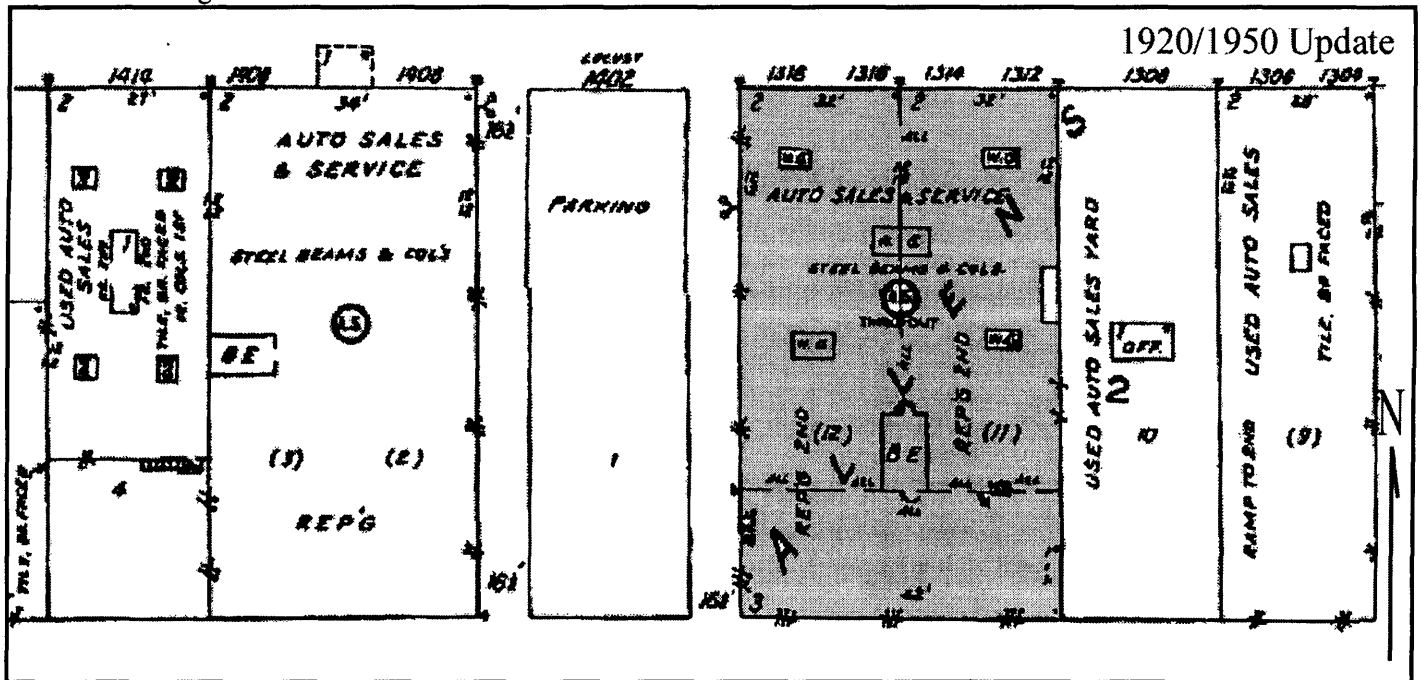


Figure 24: Detail, 1920 Sanborn Fire Insurance Map (updated to 1950) (building shaded)

Figure 24 again depicts the Chamberlain building. The lots on both sides of the building remained and still remain undeveloped, apart from being paved for parking or similar purposes.

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Des Moines City Directories, 1910-2003

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Des Moines Tribune

Des Moines Capital

The Midwestern [Magazine], 1901-15

The Horseless Age [Magazine], 1913-15

Architects' Plans, Proudfoot, Bird & Rawson: (courtesy of Brooks, Borg & Skiles Architecture Engineering LLP)
Building Number 1248: A Garage For Mr. D. S. Chamberlain, 1312'18 Locust St., Des Moines (seven sheets, dated November 1916)

Building Number 1361: An Addition to A Garage For Mr. D. S. Chamberlain, 1312-18 Locust St., Des Moines (two sheets, dated September 1919)

Architect's Specifications:

#1248-Electric Wiring For D. S. Chamberlain Garage-Des Moines, Iowa

#1248-Heating and Plumbing For D. S. Chamberlain Garage-Des Moines, Iowa

#1248-Garage Building For D. S. Chamberlain-Des Moines, Iowa

#1361-Electric Wiring For Chamberlain Garage-Des Moines, Iowa

#1361-Heating and Plumbing For Chamberlain Garage-Des Moines, Iowa

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10. Geographical Data:

Boundary Description:

Lots 11-12, Block 2, B. F. Allen's Addition, City of Des Moines.

Boundary Justification:

While Lots 10-12 comprise the current land parcel, the subject building occupies Lots 11-12 completely and these two lots have been directly associated with the original and the enlarged building since its construction.

Photographs:

Photographer: James E. Jacobsen
Date of Photographs: July 31, 2006
Repository for Original Negatives: Property owner

Photo: View: Description:

1	southeast	façade and north end of west sidewall
2	southeast	northwest corner of façade, wrap-around façade on west sidewall
3	southeast	upper façade detail, west portion
4	southeast	upper façade detail, east and central portions
5	southeast	west sidewall
6	northeast	south/rear wall and alleyway
7	southwest	east sidewall

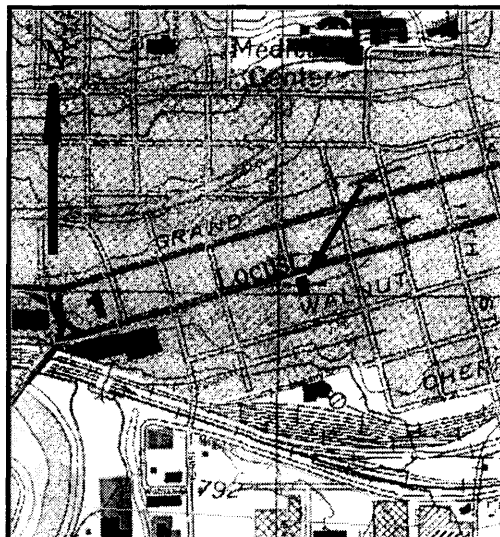


Figure 25: Locational Map (base map courtesy of <http://cairo.gis.iastate.edu>)