

United States Department of the Interior
National Park Service

National Register of Historic Places Registration Form



This form is for use in nominating or requesting determinations for individual properties and districts. See instructions in National Register Bulletin, *How to Complete the National Register of Historic Places Registration Form*. If any item does not apply to the property being documented, enter "N/A" for "not applicable." For functions, architectural classification, materials, and areas of significance, enter only categories and subcategories from the instructions. Place additional certification comments, entries, and narrative items on continuation sheets if needed (NPS Form 10-900a).

1. Name of Property

historic name Buffalo Creek Bridge

other names/site number Long Grove Bridge

Name of Multiple Property Listing _____

(Enter "N/A" if property is not part of a multiple property listing)

2. Location

street & number Robert Parker Coffin Road over Buffalo Creek not for publication

city or town Long Grove vicinity

state Illinois county Lake zip code 60047

3. State/Federal Agency Certification

As the designated authority under the National Historic Preservation Act, as amended,

I hereby certify that this nomination request for determination of eligibility meets the documentation standards for registering properties in the National Register of Historic Places and meets the procedural and professional requirements set forth in 36 CFR Part 60.

In my opinion, the property meets does not meet the National Register Criteria. I recommend that this property be considered significant at the following level(s) of significance: national statewide local

Applicable National Register Criteria: A B C D

[Signature] 4/19/18
Signature of certifying official/Title: Deputy State Historic Preservation Officer Date

Illinois Department of Natural Resources - SHPO
State or Federal agency/bureau or Tribal Government

In my opinion, the property meets does not meet the National Register criteria.

Signature of commenting official Date

Title State or Federal agency/bureau or Tribal Government

4. National Park Service Certification

I hereby certify that this property is:

- entered in the National Register determined eligible for the National Register
- determined not eligible for the National Register removed from the National Register

other (explain): _____

[Signature] 6-11-18
Signature of the Keeper Date of Action

Buffalo Creek Bridge
Name of Property

Lake, Illinois
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5. Classification

Ownership of Property
(Check as many boxes as apply.)

Category of Property
(Check only **one** box.)

Number of Resources within Property
(Do not include previously listed resources in the count.)

- private
- public - Local
- public - State
- public - Federal

- building(s)
- district
- site
- structure
- object

<u>Contributing</u>	<u>Noncontributing</u>	
		buildings
		site
1		structure
		object
1		Total

Number of contributing resources previously listed in the National Register

N/A

6. Function or Use

Historic Functions

(Enter categories from instructions.)

Transportation - Road Related (vehicular)

Transportation - Pedestrian Related

Current Functions

(Enter categories from instructions.)

Transportation - Road Related (vehicular)

Transportation - Pedestrian Related

7. Description

Architectural Classification

(Enter categories from instructions.)

Other: Half-Hip Pin-Connected Pony Pratt
Truss Bridge

Materials

(Enter categories from instructions.)

foundation: Stone

walls: Steel

roof: Wood

other: _____

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Narrative Description

(Describe the historic and current physical appearance and condition of the property. Describe contributing and noncontributing resources if applicable. Begin with a **summary paragraph** that briefly describes the general characteristics of the property, such as its location, type, style, method of construction, setting, size, and significant features. Indicate whether the property has historic integrity).

Summary Paragraph

Buffalo Creek Bridge carries a paved single lane road and a single pedestrian walkway across Buffalo Creek in Long Grove, Illinois. It sits between the businesses and residences in the Long Grove Business District and the major church in the area, the Long Grove Community Church. The 41 foot structure consists of one steel Truss Bridge constructed in 1906, and a wood covering from 1972. It is in an area of suburban Chicago where local zoning has maintained low population density and a pastoral environment.

Narrative Description

The bridge was constructed in 1906 by the Joliet Bridge and Iron Company of Joliet, Illinois, and is a rare surviving example of a pin-connected truss bridge built for an urban setting. A low height nostalgic covering was added in 1972 as a life extension measure. The function of the covering was to limit truck traffic and heavy vehicles and shield the steel from the elements.

The covering is composed of common dimensional lumber. The roof portion is supported by wooden beams that are attached with a steel bracket to the bridge's vertical steel beams. The roof on the covering is wood shake. On the east side (opposite side of the walkway), a short panel constructed of wooden slats shields the steel infrastructure from the weather.

The bridge supports load via a series of steel elements connected into triangular units - a Truss bridge. The bridge features short vertical beams, which are able to sustain load by depending on diagonal beams for support. This design is referred to as a Pratt Truss Bridge. The truss elements are connected via pins rather than rivets, so a further classification can be added to the bridge type - Pin-Connected. The steel elements support the load from above, and there is no top connection between the left and right sides of the bridge - a design style referred to as a "Pony". The end posts of the bridge are angled such that they extend approximately a half-panel in width, referred to as a "half-hip" design. Thus the full typing of the Buffalo Creek Bridge can be expressed as a "Half-Hip Pin-Connected Pony Pratt Truss Bridge".

The major components of the sub-structure are original, and are easily viewed from the shore adjacent to the abutments. The abutments are the original limestone block. The floor beams are the original twin steel I-beams. The bottom 6 laterals are the steel originals. The 13 stringers are also all original.

The major components of the top structure are also original, and are easily viewed from the pedestrian portion of the bridge. The 4 steel intermediate posts are original. The 8 steel counters and diagonals are original. The 2 steel top cords and 4 end posts are original. The steel pins connecting the chords and posts are original. The pedestrian walkway steel elements are also original.

Over the years there have been a few repairs and supplementation to the original materials. The abutment ledge on one side was reinforced with concrete. Many of the original rivets in the steel have been replaced with bolts. The wooden bridge decking has

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been replaced several times. Angle iron has been added to function as supplementary floor beams. The wooden covering has been replaced completely on at least one occasion.

Historic Integrity

The original foundation and steel structure of the bridge maintains its original function and appearance. While the 1972 cover is not yet historic in its own right, it does not unduly obscure or alter the bridge's historic elements or their function. Its impact is primarily visual from a distance. The bridge's slender profile would likely not be noticeable were it not be for the covering.

The significant architectural elements of the Pratt Pony Truss design can be enumerated as follows:

- 1) End Posts in tension
- 2) Top Chord in compression
- 3) Bottom Chord in tension
- 4) verticals in compression
- 5) Center Sloping Diagonals in tension
- 6) Decking
- 7) Floor Beams
- 8) Stringers

These elements are shown in Figure 13. The covering partially obscures the connection of the end posts to the bottom chord, but otherwise has no material impact on the visibility of the other elements. Additionally, the pedestrian walkway is an uncommon element whose visibility is not affected by the covering.

As shown in Figure 10, there is only one other bridge of this type in the greater Chicago area, and it has greater integrity issues. Looking beyond the area of local significance, bridges of this type appear to be very rare and typically have integrity issues.

Greater Chicago Area (Illinois DOT District 1) Comparable Bridges

(Subject Bridge) Buffalo Creek Bridge, Lake County, IL (IL# 049-7150)

- located 28.5 miles northwest of Chicago's loop
- covering added in 1972

Grant Creek Bridge, Will County, IL (IL# 099-9940)

- located 46.6 miles southwest of Chicago's loop
- has been relocated and repurposed due to changing land use in the area
- original foundation lost
- age, builder, and modification history unknown/un-researched

Eastern/North-Central Illinois Area (Illinois DOT District 3) Comparable Bridges

As some of this region lies in North Eastern Illinois, Pratt Pony Truss Eyebar Bridges located within it were also analyzed, though none were found in the North-Eastern Illinois portion. For completeness, the three similar bridges in this region are enumerated below.

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Iroquois 1300 Spring Creek Bridge, Iroquois County (IL# 038-5332)

- located 85.2 miles south of Chicago's loop, in Central Illinois
- built in 1901
- no longer carries traffic
- original function changed due to nearby highway which changed traffic flow and obsoleted the road
- additional integrity issues are unknown/un-researched

Iroquois 1250 Bridge Spring Creek Bridge, Iroquois County (IL# 038-5331)

- located 85.4 miles south of Chicago's loop, in Central Illinois
- built circa 1904
- no longer carries traffic
- original function and setting was changed by construction of highway 1000 feet east of the bridge circa 1970
- unique, atypical Bedstead subclass of Pratt bridges
- additional integrity issues are unknown/un-researched

Whiskey Creek Bridge, Iroquois County (IL# 038-4141)

- located 93.2 miles south of Chicago's loop, in Central Illinois
- outside of what is considered Chicago Metro
- constructed in 1896
- foundation is not original
- decking design has changed
- additional integrity issues are unknown/un-researched

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8. Statement of Significance

Applicable National Register Criteria

(Mark "x" in one or more boxes for the criteria qualifying the property for National Register listing.)

- A Property is associated with events that have made a significant contribution to the broad patterns of our history.
- B Property is associated with the lives of persons significant in our past.
- C Property embodies the distinctive characteristics of a type, period, or method of construction or represents the work of a master, or possesses high artistic values, or represents a significant and distinguishable entity whose components lack individual distinction.
- D Property has yielded, or is likely to yield, information important in prehistory or history.

Criteria Considerations

(Mark "x" in all the boxes that apply.)

Property is:

- A Owned by a religious institution or used for religious purposes.
- B removed from its original location.
- C a birthplace or grave.
- D a cemetery.
- E a reconstructed building, object, or structure.
- F a commemorative property.
- G less than 50 years old or achieving significance within the past 50 years.

Areas of Significance

(Enter categories from instructions.)

Engineering

Period of Significance

1906

Significant Dates

1906

Significant Person

(Complete only if Criterion B is marked above.)

n/a

Cultural Affiliation (if applicable)

n/a

Architect/Builder

Joliet Bridge & Iron Company, Joliet, Illinois

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Statement of Significance Summary Paragraph (Provide a summary paragraph that includes level of significance, applicable criteria, justification for the period of significance, and any applicable criteria considerations).

Buffalo Creek Bridge is being nominated to the National Register of Historic Places for local significance under Criterion C (ARCHITECTURE) as a good example of a Pony Truss bridge. The bridge maintains good integrity despite having a covering placed over it in 1972. The bridge was identified by the Illinois Department of Transportation as being one of only 2 remaining Steel Pratt Pony Truss Eyebar bridges in the Chicago Metro area (IDOT District 1), and one of only 33 remaining in Illinois. The majority of these remaining bridges are no longer in service, and few, if any, have an original pedestrian walkway feature. The period of significance is 1906, the year the bridge was constructed.

Narrative Statement of Significance (Provide at least **one** paragraph for each area of significance.)

Brief History of Long Grove

The Buffalo Creek Bridge carries Robert Parker Coffin road over Buffalo Creek. This road was originally a trail used by local Native Americans to traverse between villages located near where Barrington and Lincolnshire are today. Another trail, now known as Old McHenry Road connected villages near present day Wauconda and Des Plaines. The intersection of these two trails comprises the crossroads that anchor the Long Grove Business District.

The first bridge over Buffalo Creek in downtown Long Grove was said to be a simple wooden affair, created in the late 1840's as a "less messy" way for area people to get to the church and cemetery - riding your horse through Buffalo Creek would have been the earlier "messy" way. It was likely a simple structure, but adequate for the occasional horse and wagon. As the population in the Long Grove area increased, the crossroads became a strategic central location for goods and services - by 1900 it had become a true commerce center for area farmers, featuring two corner stores, a creamery, a hotel, a tavern, and even a town hall.

In 1902, the automobile was just starting to take hold - one automobile for every 1000 people in the US. Where does one drive their car in a horse and wagon world? Not very far on dirt roads and certainly not alone - AAA was formed in Chicago in 1902 to advocate for road improvements. Clubs were formed for people to take excursions together. Long Grove was a popular early destination for these early adopter auto enthusiasts. Perhaps in a related response, the old wooden bridge was replaced in 1906 with a "modern" steel truss bridge.

Ironically, this burgeoning mobility would be the downfall of early downtown Long Grove, and later its revival. With the advent of the automobile and tractor, Long Grove lost its strategic location advantage - area residents no longer needed local horse and buggy shops, and were able to drive farther to larger towns to do their weekly marketing and shopping. Some Long Grove shops closed up and sat empty, while others tried to adapt - the Smithy put in an auto garage and gas pump. The hotel was converted to a private residence. A chance for commercial redemption came and went in 1930, when the State routed Highways 53 and 83 around Long Grove, bypassing the central crossroads. The rationing and economics of World War II pretty much finished off what was left. By 1945, Long Grove had become a ghost town - two dirt roads crossing with shuttered buildings all around.

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When the economy improved after the war, so did the roads - Old McHenry Road was blacktopped and the corner stores were acquired by the wives of two local farmers who began selling antiques, handmade dresses, ice cream, and homemade pies. Long Grove again became an interesting and fun place to drive on a nice afternoon. Its growing popularity attracted the attention of a few other antique dealers and adjacent downtown residences were acquired and repurposed as additional shops. Recognizing that expansion could hurt the charm of the quaint crossroads, action was taken in 1962 - the Long Grove Village Board passed the "Historic Landmark Ordinance", one of the first of its kind in the country. The ordinance required new construction and remodeling to conform to the historic style already present near the crossroads.

The crossroads that anchor the Long Grove Business district (Robert Parker Coffin Road and Old McHenry Road) have contained businesses since the 1840's and a mixture of homes and businesses since at least the 1860's. That mixture continues to exist today. The business district is the most immediate feature served by one side of the bridge. On the opposite side, the Long Grove Community church, which dates to 1847 is the most immediate feature served by the bridge. The flow of pedestrians and wheeled traffic between these two features exists today, much the same as it existed when the bridge was created in 1906. The local oral history is that there was a wooden structure that served as a bridge over Buffalo Creek prior to 1906, but no records or photographs are known to exist that document this.

Dating Buffalo Creek Bridge

The 1906 construction date of the bridge has been established using a July 1906 newspaper account of the bridge contract being awarded. The same newspaper account is used to establish the builder as Joliet Bridge and Iron Company. Additionally, a photo postcard of the bridge which is postmarked 1908 establishes that the bridge must have been constructed prior to 1908. Circumstantially, a Steel Half-Hip Pratt Pony Truss Bridge would be most commonly found to have been constructed between 1890 and 1910.

Joliet Bridge and Iron Company Background

The Joliet Bridge and Iron Company was formed in 1896. Robert C. Morrison served as President of the company. The plant was located adjacent to the Elgin, Joliet and Eastern Railroad (EJ & E) and employed about 75 workers to design and fabricate "steel bridges of every description". This rail line (the EJ & E railroad) runs through the village of Long Grove about four miles north of the Buffalo Creek Bridge, and about 50 miles distant from Joliet. Morrison died in 1913 and the company was shortly thereafter reorganized and renamed The Joliet Bridge and Construction Company.

Buffalo Creek Bridge Design

The Pratt bridge design was patented in 1844 by Thomas and Caleb Pratt, and featured vertical members in compression and diagonal members in tension. Favored for its simplicity, by 1870 it had become the standard for steel Truss design and remained so until around 1910. The Pratt Pony Truss is a variety in which the roadway passes through or between the trusses, but the Pony Trusses are small enough not to require overhead bracing to resist the force of winds. Pratt Half-Hip Trusses are a variety characterized by inclined end posts that do not extend the length of a full panel. The Pratt Half-Hip variation was popular from the 1890's onward. The integration of a pedestrian walkway is a unique feature of the Buffalo Creek Bridge design, and underscores the bridges dual function for both vehicular and pedestrian traffic.

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Illinois Context of Buffalo Creek Bridge

Illinois Department of Transportation records classify the Buffalo Creek Bridge as a ``Steel Pratt Pony Truss Eyebar`` bridge, and establish that it is one of two remaining in the Chicago area, and one of only 33 remaining in the state. See Figure 10 for a graphic illustrating Illinois Department of Transportation records.

The National Bridge Inventory (NBI) is a different database which compiles information on all bridges and tunnels which have roads passing above or below. Any bridge longer than 20 feet is included in this database. The NBI data provides a few additional bridge characteristics which offer an alternative objective base to assess the uniqueness of the Buffalo Creek Bridge.

Unique aspects of the Buffalo Creek Bridge that can be discerned using the NBI data are as follows:

- features a steel truss thru design
- over a century old (built before 1917 - the Buffalo Creek Bridge was built in 1906)
- carries a single lane of traffic
- carries a pedestrian walkway

The most characteristic geographic area relevant to the Buffalo Creek Bridge is the Chicago Metropolitan Area. This US Census Bureau defines this area as being comprised of the Illinois Counties of Cook, DuPage, Kane, Lake and Will, along with Lake County in Indiana. It is the third largest Metropolitan Area in the country. It is also interesting to look at uniqueness at a broader and narrower level. The NBI analysis following looked at these geographic areas:

- the United States
- Illinois
- the Chicago Metropolitan area (including Lake County, Indiana)
- Lake County, Illinois

The detailed analysis below shows the Buffalo Creek Bridge to be the only century old single lane steel thru truss bridge remaining in the Chicago Metropolitan area, and one of only 80 remaining in Illinois. The Buffalo Creek Bridge's incorporation of pedestrian access is one of only two left in Illinois and one of only 229 remaining in the United States.

	United States	Illinois	Chicago Metro	Lake County IL
Structures enumerated in NBI	725,589	30,827	5,699	342
- subset featuring a steel truss thru design	8447	209	24	1
- further subset known to be at least a century old	2069	85	2	1
- further subset which carries a single lane of traffic	1524	80	1	1
- further subset which incorporate a pedestrian walkway	229	2	1	1

For reference, the other Illinois bridge with an integrated walkway is the Third Street Bridge in Delevan, Illinois which was placed on the National Register in 1999.

It should be noted that the data contained in the NBI is not 100% accurate. For example, regarding the subject bridge, Buffalo Creek Bridge, the NBI database has the build date incorrectly listed as 1925. Additionally, a sampling of the data shows it to contain bridges which have been destroyed or replaced, yet still carry the original vintage build date. Regardless, the occasional inaccuracy does not materially affect the overall

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conclusion that bridges of this type are increasingly rare.

The NBI analysis was done using the 2016 edition of the database. The steel truss thru design subset was determined using TRAFFIC_LANES_ON_28A values greater than 0 to select vehicular bridges, STRUCTURE_TYPE_043B values of 10 denoting a thru truss design, and STRUCTURE_KIND_043A values of 3 or 4 denoting steel as the construction material. The century old subset was determined by using YEAR_BUILT_027 values less than 1917. The single lane of traffic subset was determined by using TRAFFIC_LANES_ON_28A values of 1. The pedestrian walkway subset was determined by using LEFT_CURB_MT_050A or RIGHT_CURB_MT_50B values greater than 0.

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9. Major Bibliographical References

Bibliography (Cite the books, articles, and other sources used in preparing this form.)

- Long Grove Lore and Legend, 1978, Virginia L. Park
- Real Photo Postcard Guide, Robert Bogdan & Todd Weseloh, 2006, Syracuse University Press
- U.S. Department of Transportation, National Bridge Inventory, www.fhwa.dot.gov/bridge/nbi.cfm
- State of Tennessee Survey Report For Historic Highway Bridges, www.tn.gov/tdot/article/historic-bridgebook
- Maryland State Highway Association, "The Pratt Truss", www.roads.maryland.gov/OPPEN/V-Pratt.pdf
- National Register of Historic Places Registration Form For SD DOT Br. No. 34-120-94, NRIS ID# 02000579
- National Register of Historic Places Registration Form For Northwood Bridge, NRIS ID# 97000175
- National Register of Historic Places Registration Form For Spencer's Crossing Bridge, NRIS ID# 89002177
- Historical American Engineering Record, National Park Service, Island Slough Bridge, HAER No. AR-82
- National Register of Historic Places Registration Form For Bolivia Road Bridge, NRIS ID# 03001464
- Long Grove Living Magazine, September 2013, Historic Long Grove Walking Tour
- Structural Engineers' Handbook, Milo Ketchum, Data for the Design and Construction of Steel Bridges and Buildings
- April 2009 Rural Historic Structural Survey of Channahon Township, Will County, Illinois, Will County Historic Preservation Commission
- BridgeHunter.com Database of Historic or Notable Bridges in the United States, James Baughn

Previous documentation on file (NPS):

- preliminary determination of individual listing (36 CFR 67 has been requested)
- previously listed in the National Register
- previously determined eligible by the National Register
- designated a National Historic Landmark
- recorded by Historic American Buildings Survey # _____
- recorded by Historic American Engineering Record # _____
- recorded by Historic American Landscape Survey # _____

Primary location of additional data:

- State Historic Preservation Office
- Other State agency
- Federal agency
- Local government
- University
- Other
- Name of repository: _____

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10. Geographical Data

Acreage of Property Less than one

(Do not include previously listed resource acreage; enter "Less than one" if the acreage is .99 or less)

Latitude/Longitude Coordinates

Datum if other than WGS84: _____

(enter coordinates to 6 decimal places)

1 42.1775 -87.9998
Latitude Longitude

3 _____
Latitude Longitude

2 _____
Latitude Longitude

4 _____
Latitude Longitude

Verbal Boundary Description (Describe the boundaries of the property.)

The Buffalo Creek Bridge is located on Robert Parker Coffin Road in Long Grove, Illinois, 500 feet southwest of downtown Long Grove as defined by the intersection of Robert Parker Coffin Road and Old McHenry Road. The UTM reference is Zone 16, E417429, N4668867. The road is a Village of Long Grove road which runs east - west and connects Illinois Route 53 with Illinois Route 83. The Illinois Route 53 intersection is in on the far south-eastern edge of Lake County's Ela Township (Section 25, Township 43 North, Range 10 East of the Principle Meridian). The Illinois Route 83 intersection is in the far south-western corner of Lake County's Vernon Township (Section 30, Township 43 North, Range 11 East of the 3rd Principle Meridian). The bridge is located in Vernon Township. The north and south boundary is defined by the physical bridge structure. The eastern and western boundary is defined by the point at which the abutment meets the road bed.

Boundary Justification (Explain why the boundaries were selected.)

The boundary includes only that portion of land which directly relates to the Buffalo Creek Bridge (its structural members and its approaches) including the abutments.

11. Form Prepared By

name/title Roy Underwood, President / Ryan Messner, President date 2/13/2017

organization Long Grove Historical Society / Historic Downtown
Long Grove Business Association telephone (847) 478-5140

street & number 338 Old McHenry Road email aaron@uinnovate.net

city or town Long Grove state IL zip code 60047

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Additional Documentation

Submit the following items with the completed form:

- **GIS Location Map (Google Earth or BING)**
- **Local Location Map**
- **Site Plan**
- **Floor Plans (As Applicable)**
- **Photo Location Map** (Include for historic districts and properties having large acreage or numerous resources. Key all photographs to this map and insert immediately after the photo log and before the list of figures).

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Photographs:

Submit clear and descriptive photographs. The size of each image must be 3000x2000 pixels, at 300 ppi (pixels per inch) or larger. Key all photographs to the sketch map. Each photograph must be numbered and that number must correspond to the photograph number on the photo log. For simplicity, the name of the photographer, photo date, etc. may be listed once on the photograph log and doesn't need to be labeled on every photograph.

Photo Log

Name of Property: Buffalo Creek Bridge
City or Vicinity: Long Grove
County: Lake **State:** Illinois
Photographer: Aaron Underwood and Lori Lyman
Dates Photographed: May 2015 and February 2017

Description of Photograph(s) and number, include description of view indicating direction of camera:

- Photo 1 of 11:** Front Side of bridge (north side)
- Photo 2 of 11:** West Pony Detail shown from front of bridge.
- Photo 3 of 11:** Hand Rail Detail shown from front side (north side)
- Photo 4 of 11:** East Side of Bridge (side with no walkway)
- Photo 5 of 11:** West Side of Bridge (side with walkway)
- Photo 6 of 11:** Bottom Support Beam and Foundation
- Photo 7 of 11:** East Interior as seen from front of bridge.
- Photo 8 of 11:** West Interior as seen from front of bridge.
- Photo 9 of 11:** Bridge Bottom
- Photo 10 of 11:** Pin Connection as viewed from pedestrian walkway
- Photo 11 of 11:** South Side of Bridge

Paperwork Reduction Act Statement: This information is being collected for applications to the National Register of Historic Places to nominate properties for listing or determine eligibility for listing, to list properties, and to amend existing listings. Response to this request is required to obtain a benefit in accordance with the National Historic Preservation Act, as amended (16 U.S.C.460 et seq.).

Estimated Burden Statement: Public reporting burden for this form is estimated to average 100 hours per response including time for reviewing instructions, gathering and maintaining data, and completing and reviewing the form. Direct comments regarding this burden estimate or any aspect of this form to the Office of Planning and Performance Management, U.S. Dept. of the Interior, 1849 C. Street, NW, Washington, DC.

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List of photos

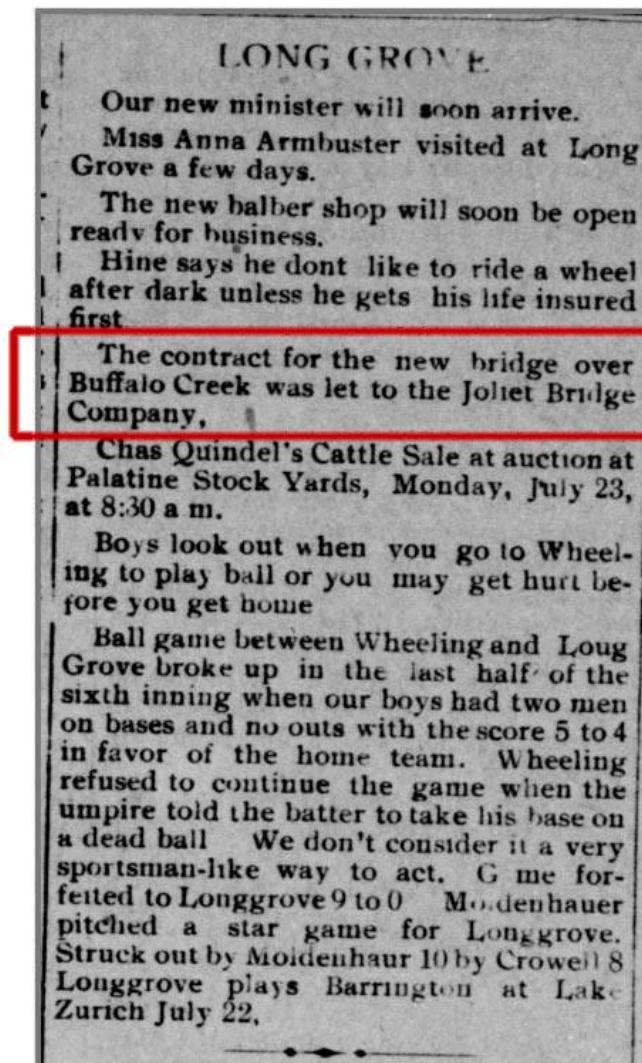
(Resize, compact, and paste images of maps and historic documents in this section. Place captions, with figure numbers above each image. Orient maps so that north is at the top of the page, all document should be inserted with the top toward the top of the page.)

Figure 1: 1906 Newspaper Article establishing bridge to be no older than July of 1906. It is our assertion that the bridge was built in the late summer or fall of 1906.

Newspapers.com

Cook County Herald (Arlington Heights, Illinois) · Thu, Jul 19, 1906 · Page 5

Downloaded on Mar 25, 2016



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Figure 2: 1908 Photo Post Card Photograph of Bridge establishing the bridge to have been built prior to July 1908.



CREEK BRIDGE
LONG GROVE ILL

POST CARD

CORRESPONDENCE HERE



NAME AND ADDRESS HERE

Just enjoying
a long sweet
smoke at the
corners. Note the
other side.
M.R.

Miss Evera Schmidt,
Palatine,
Illinois.

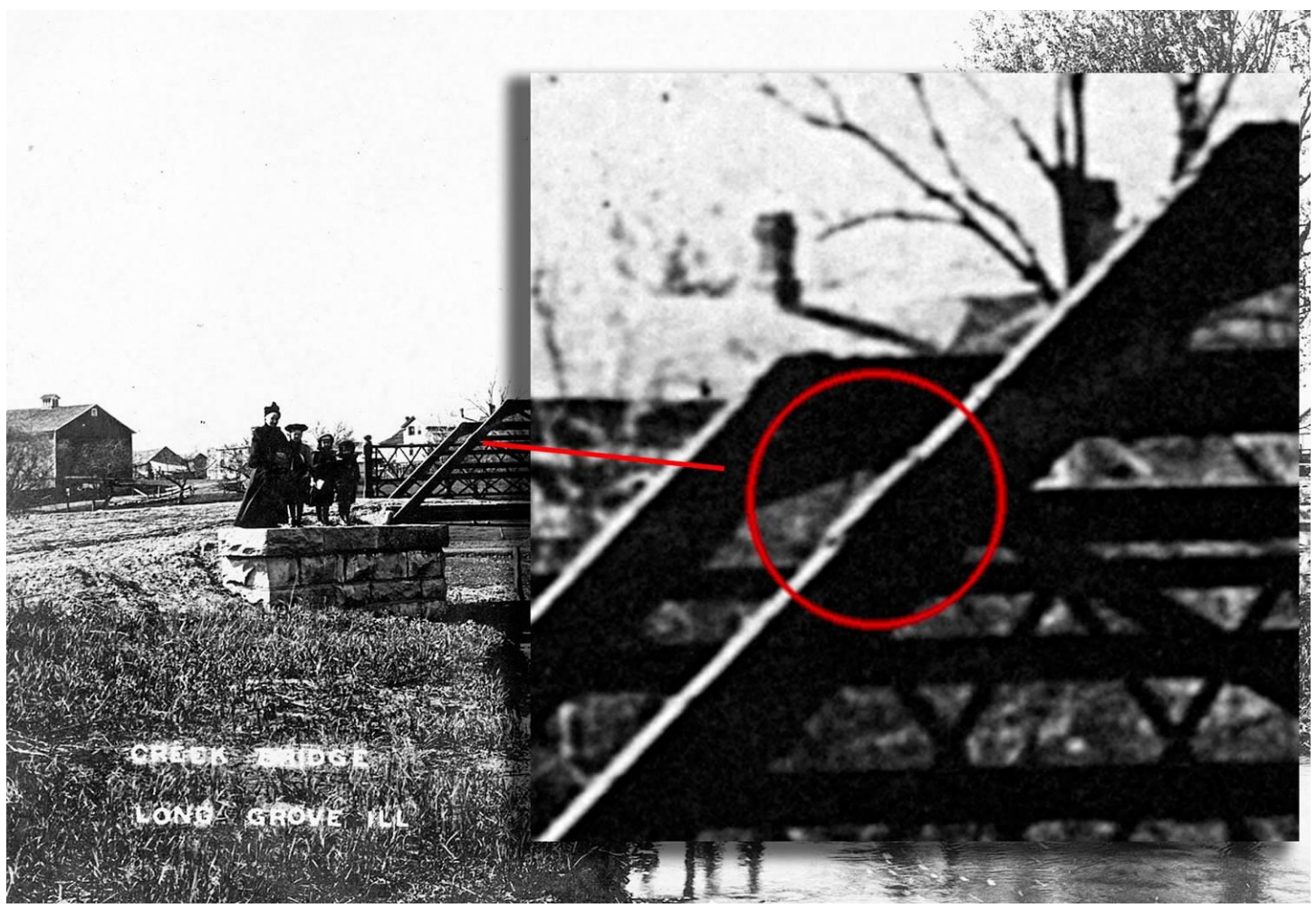
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Figure 3: 1908 Photo Post Card Bridge Plate – We believe the bridge plate (which would contain bridge date and manufacturer) can be seen in this picture. The plate appears to have been lost or removed many years ago, as no evidence of it exists today.



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Figure 4: Circa 1912 Photo Postcard (This Cyko postcard stock was in circulation between 1910 and 1915. Latest known postmark of this stock is 1936.



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Figure 5: Picture of unknown date showing pedestrian use of the bridge.



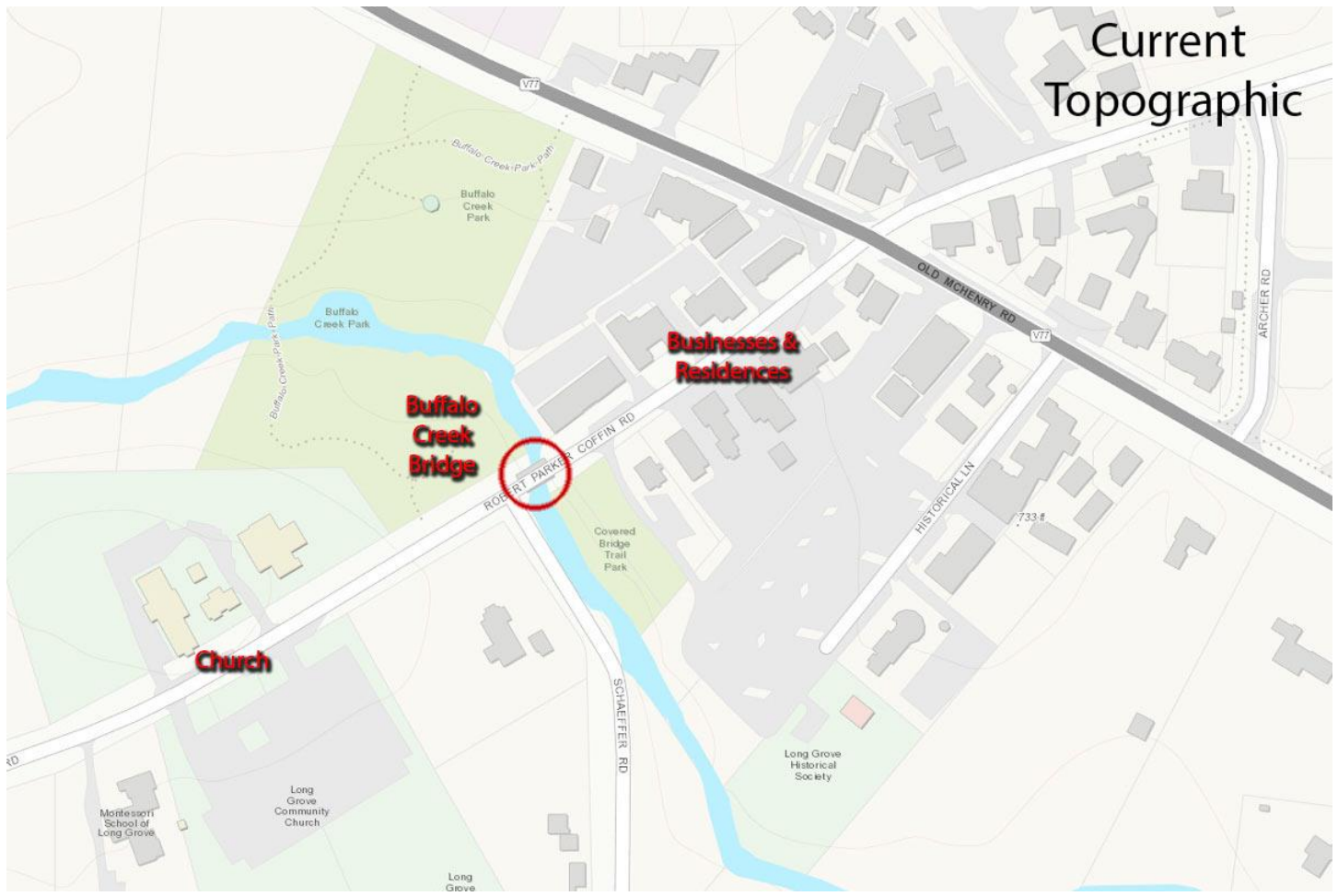
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Figure 6: Current Topographic Map



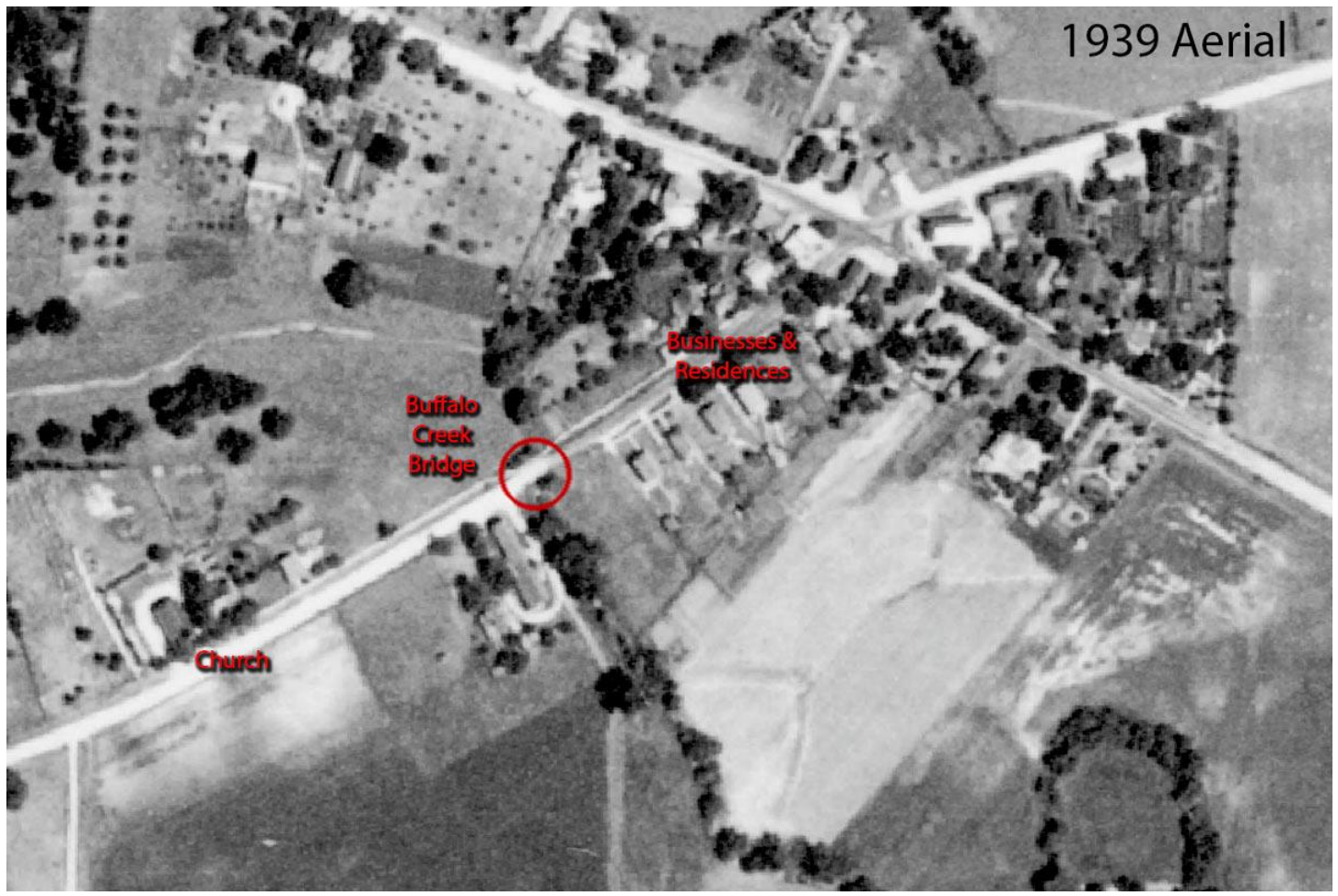
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Figure 7: 1939 Aerial



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Figure 8: 2015 Aerial



United States Department of the Interior
National Park Service

National Register of Historic Places Continuation Sheet

Buffalo Creek Bridge

Name of Property

Lake, Illinois

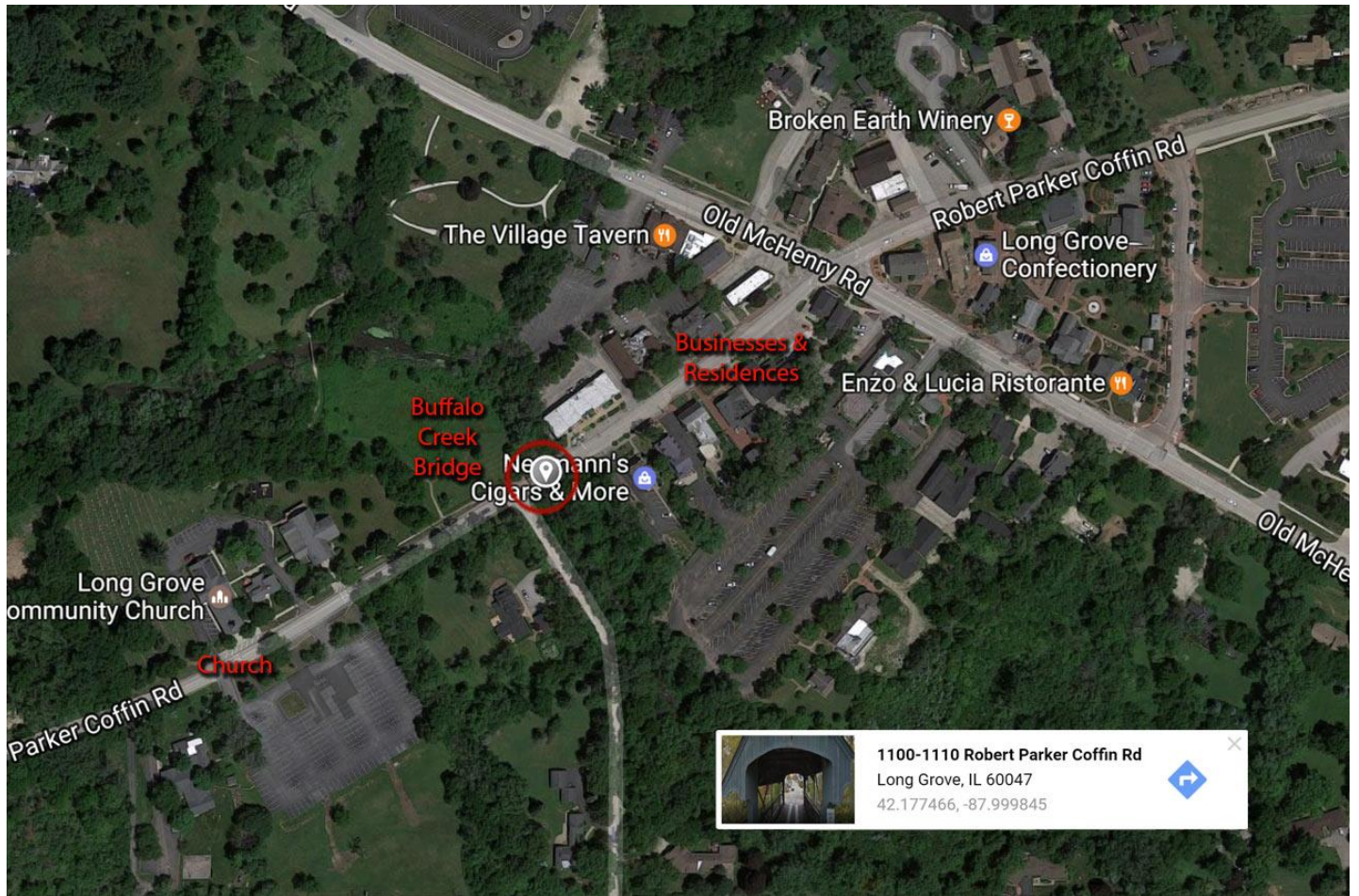
County and State

Name of multiple listing (if applicable)

Section number Additional Documentation

Page 23

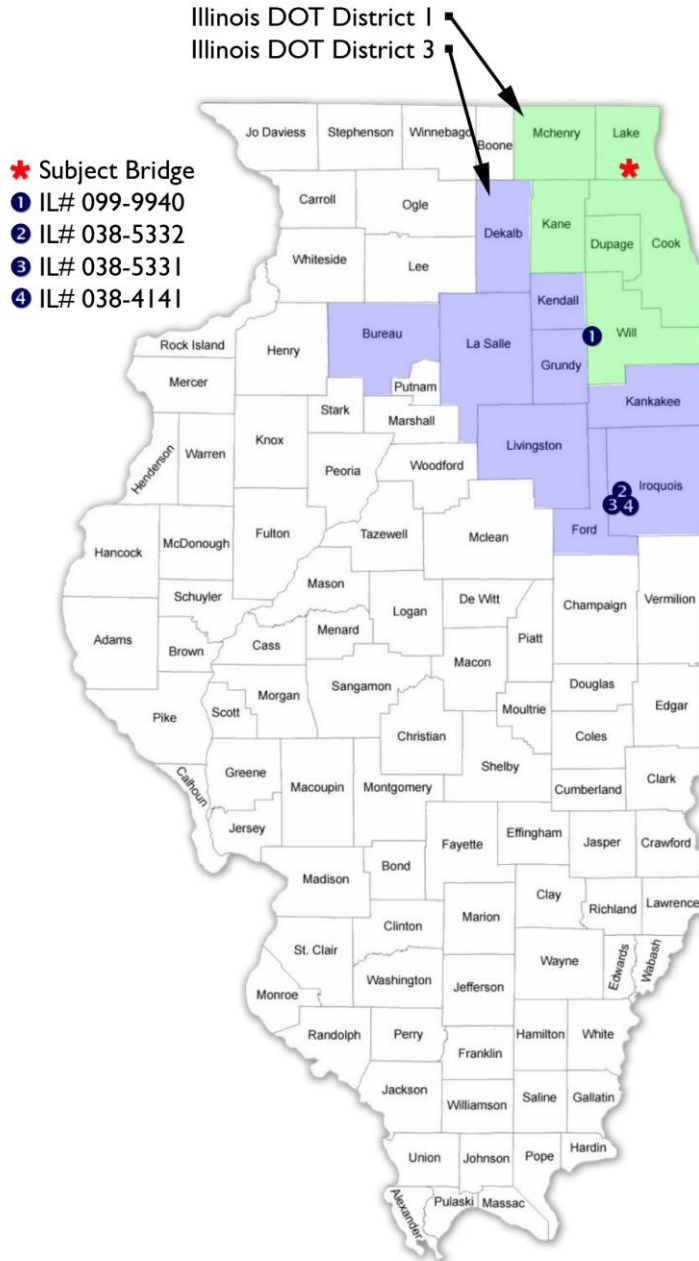
Figure 9: Google GIS Map



Property name:
 Illinois, County:

Figure 10: Illinois Department of Transportation Inventory of Pratt Pony Truss Eyebars Bridges

Area	Remaining Steel Pratt Pony Truss Eyebars Bridges
IDOT District 1	2
IDOT District 3	3
All Other IDOT Districts Statewide	28
Total Remaining in Illinois	33



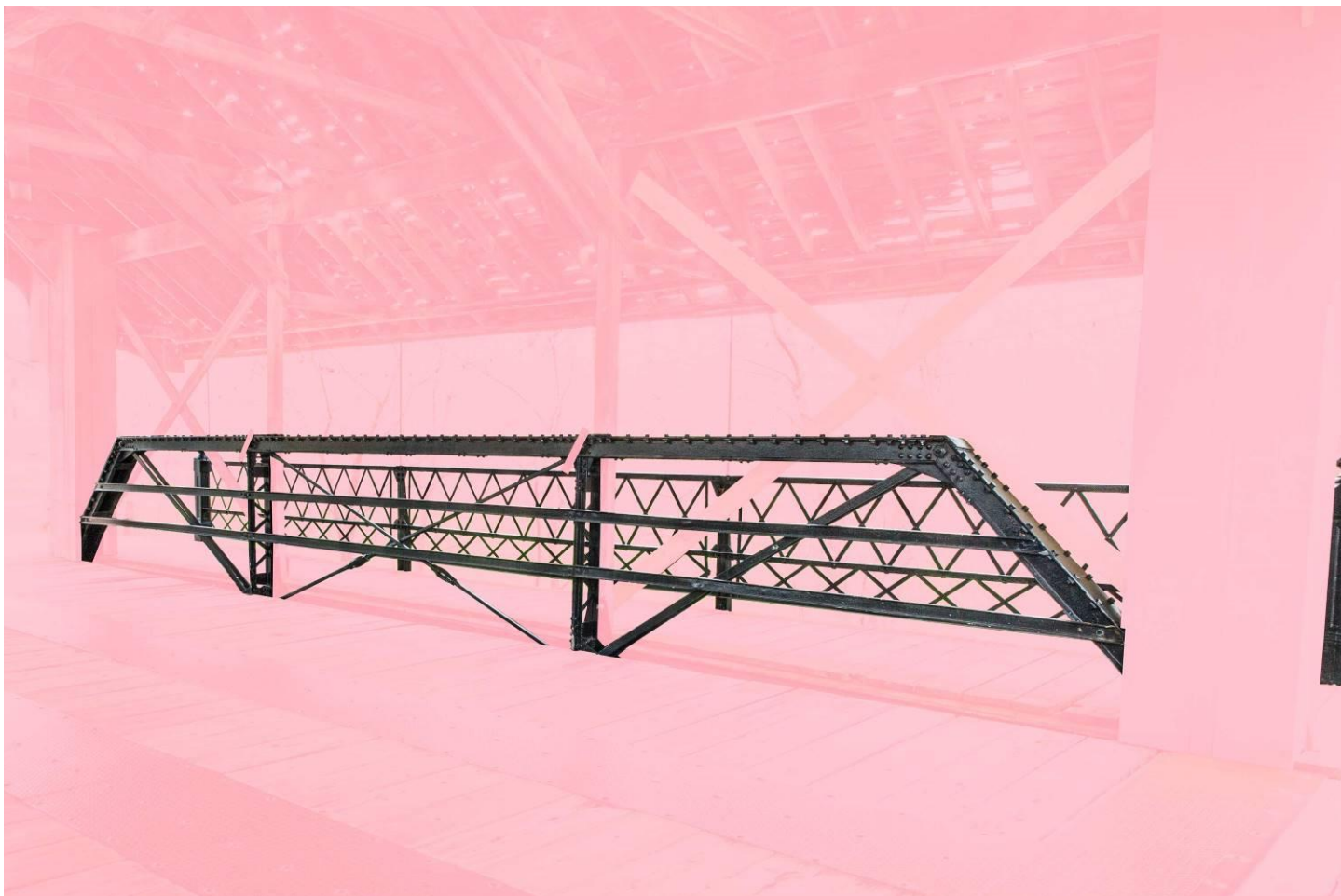
Property name:
Illinois, County:

Figure 11: East Interior (non-historic elements shown in red) as seen from front of bridge.



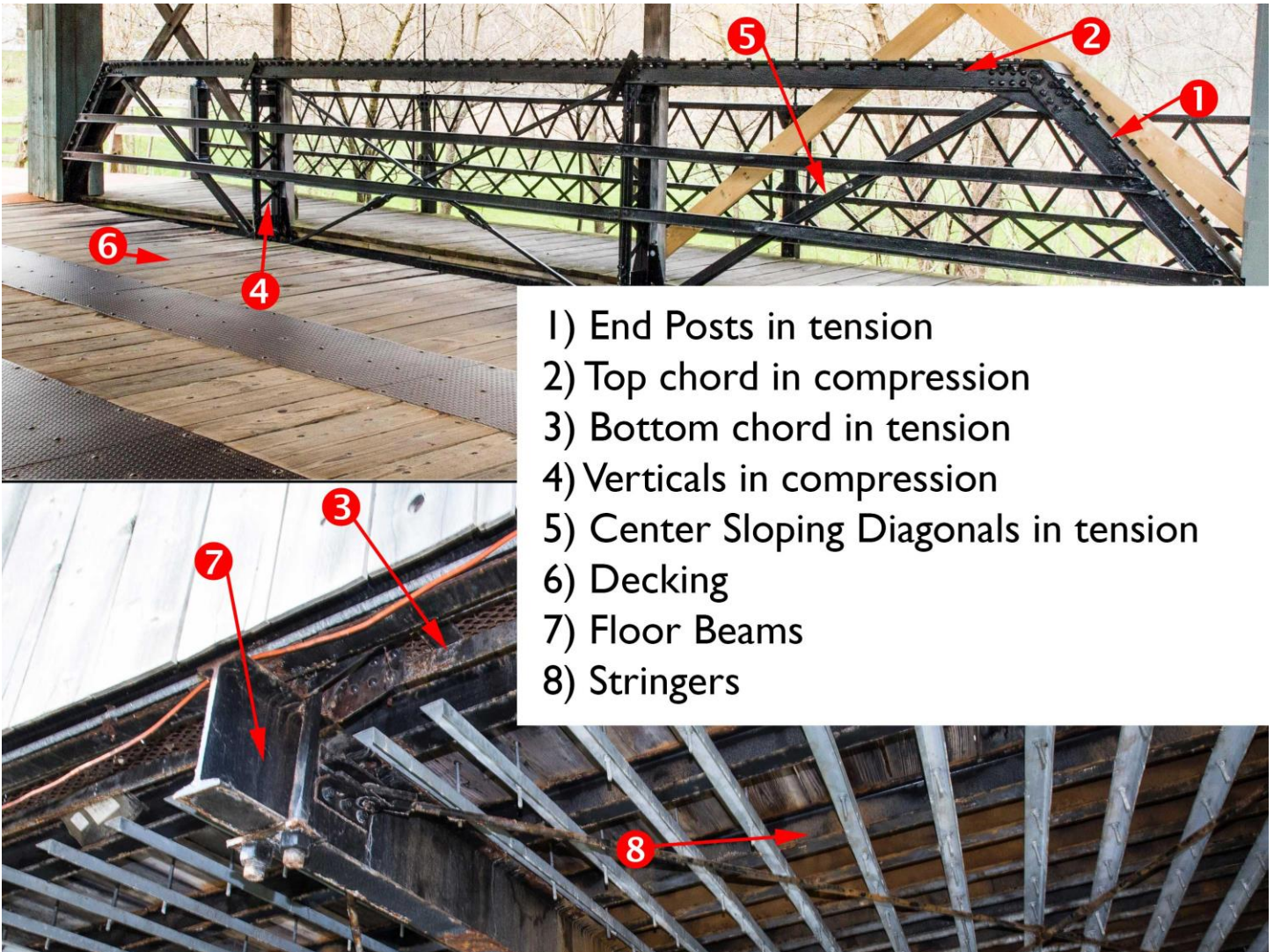
Property name:
Illinois, County:

Figure 12: West Interior (non-historic elements in red) as seen from front of bridge.



Property name:
Illinois, County:

Figure 13: Significant architectural elements of the Pratt Pony Truss Design



Long
Grove
Chocolate
Festival



Schaeffer

SPEED
LIMIT
25

ONE
WAY



WEIGHT
LIMIT
3
TONS

STOP

ONE LANE
BRIDGE
USE CENTER
TRACK

NO
TRUCKS
OR BUSES
\$500 FINE





















WEIGHT
LIMIT
3
TONS

NO
TRUCK
OR
BUSES
\$500 FINE



AHEAD



STOP

M

ps

RYAN MESSAGE

National Register of Historic Places
Memo to File

Correspondence

The Correspondence consists of communications from (and possibly to) the nominating authority, notes from the staff of the National Register of Historic Places, and/or other material the National Register of Historic Places received associated with the property.

Correspondence may also include information from other sources, drafts of the nomination, letters of support or objection, memorandums, and ephemera which document the efforts to recognize the property.

UNITED STATES DEPARTMENT OF THE INTERIOR
NATIONAL PARK SERVICE

NATIONAL REGISTER OF HISTORIC PLACES
EVALUATION/RETURN SHEET

Requested Action:

Property Name:

Multiple Name:

State & County:

Date Received: 4/25/2018 Date of Pending List: Date of 16th Day: Date of 45th Day: 6/11/2018 Date of Weekly List:

Reference number:

Nominator:

Reason For Review:

Accept Return Reject 6/11/2018 Date

Abstract/Summary
Comments:

Recommendation/
Criteria

Reviewer Barbara Wyatt Discipline Historian

Telephone (202)354-2252 Date _____

DOCUMENTATION: see attached comments : No see attached SLR : No

If a nomination is returned to the nomination authority, the nomination is no longer under consideration by the National Park Service.



Illinois Department of Natural Resources

One Natural Resources Way Springfield, Illinois 62702-1271
www.dnr.illinois.gov

Bruce Rauner, Governor

Wayne A. Rosenthal, Director

August 9, 2017

National Park Service
National Register of Historic Places
Attention: Ms. Barbara Wyatt
1849 C Street, NW, Mail Stop 7228
Washington, DC 20240



Dear Ms. Wyatt:

Enclosed for your review is one National Register Nomination Form. It has been recommended by the Illinois Historic Sites Advisory Council and signed by the Deputy State Historic Preservation Officer. It is being submitted in a digital format on the enclosed disk, and is the true and correct copy.

Buffalo Creek Bridge – Long Grove, Lake County

Please contact me at the address above, or by telephone at 217-785-4324. You can also email me at andrew.heckenkamp@illinois.gov if you need any additional information or clarification. Thank you for your attention to this matter.

Sincerely,

A handwritten signature in black ink, appearing to read "Andrew Heckenkamp".

Andrew Heckenkamp
Survey & National Register Coordinator
Illinois State Historic Preservation Office

Enclosures

56-1672



United States Department of the Interior
National Park Service

National Register of Historic Places Registration Form

This form is for use in nominating or requesting determinations for individual properties and districts. See instructions in National Register Bulletin, *How to Complete the National Register of Historic Places Registration Form*. If any item does not apply to the property being documented, enter "N/A" for "not applicable." For functions, architectural classification, materials, and areas of significance, enter only categories and subcategories from the instructions. Place additional certification comments, entries, and narrative items on continuation sheets if needed (NPS Form 10-900a).

1. Name of Property

historic name Buffalo Creek Bridge

other names/site number Long Grove Bridge

Name of Multiple Property Listing _____

(Enter "N/A" if property is not part of a multiple property listing)

2. Location

street & number Robert Parker Coffin Road over Buffalo Creek not for publication

city or town Long Grove vicinity

state Illinois county Lake zip code 60047

3. State/Federal Agency Certification

As the designated authority under the National Historic Preservation Act, as amended,

I hereby certify that this nomination request for determination of eligibility meets the documentation standards for registering properties in the National Register of Historic Places and meets the procedural and professional requirements set forth in 36 CFR Part 60.

In my opinion, the property meets does not meet the National Register Criteria. I recommend that this property be considered significant at the following level(s) of significance: national statewide local

Applicable National Register Criteria: A B C D

[Signature]
Signature of certifying official/Title: Deputy State Historic Preservation Officer

8/19/17
Date

Illinois State Historic Preservation Office
State or Federal agency/bureau or Tribal Government

In my opinion, the property meets does not meet the National Register criteria.

Signature of commenting official _____ Date _____

Title _____ State or Federal agency/bureau or Tribal Government _____

4. National Park Service Certification

I hereby certify that this property is:

entered in the National Register determined eligible for the National Register

determined not eligible for the National Register removed from the National Register

other (explain:) _____

Signature of the Keeper _____

Date of Action _____

Buffalo Creek Bridge
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County and State

5. Classification

Ownership of Property
(Check as many boxes as apply.)

Category of Property
(Check only **one** box.)

Number of Resources within Property
(Do not include previously listed resources in the count.)

- private
- public - Local
- public - State
- public - Federal

- building(s)
- district
- site
- structure
- object

<u>Contributing</u>	<u>Noncontributing</u>	
		buildings
		site
1		structure
		object
1		Total

Number of contributing resources previously listed in the National Register

N/A

6. Function or Use

Historic Functions

(Enter categories from instructions.)

Transportation - Road Related (vehicular)

Transportation - Pedestrian Related

7. Description

Architectural Classification

(Enter categories from instructions.)

Other: Half-Hip Pin-Connected Pony Pratt

Truss Bridge

Current Functions

(Enter categories from instructions.)

Transportation - Road Related (vehicular)

Transportation - Pedestrian Related

Materials

(Enter categories from instructions.)

foundation: Stone

walls: Steel

roof: Wood

other: _____

Returned

Buffalo Creek Bridge
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Narrative Description

(Describe the historic and current physical appearance and condition of the property. Describe contributing and noncontributing resources if applicable. Begin with a **summary paragraph** that briefly describes the general characteristics of the property, such as its location, type, style, method of construction, setting, size, and significant features. Indicate whether the property has historic integrity).

Summary Paragraph

Buffalo Creek Bridge carries a paved single lane road and a single pedestrian walkway across Buffalo Creek in Long Grove, Illinois. It sits between the businesses and residences in the Long Grove Business District and the major church in the area, the Long Grove Community Church. The 41 foot structure consists of one steel Truss Bridge. It is in an area of suburban Chicago where local zoning has maintained low population density and a pastoral environment.

Narrative Description

The bridge was constructed in 1888 by the Joliet Bridge and Iron Company of Joliet, Illinois, and is a rare surviving example of a pin-connected truss bridge built for an urban setting. A low height nose-algic covering was added in 1972 as a life extension measure. The function of the covering was to limit truck traffic and heavy vehicles and shield the steel from the elements.

The covering is composed of common dimensional lumber. The roof portion is supported by wooden beams that are attached with a steel bracket to the bridge's vertical steel beams. The roof on the covering is wood shake. On the east side (opposite side of the walkway), a short panel constructed of wooden slats shields the steel infrastructure from the weather.

The bridge supports load via a series of steel elements connected into triangular units - a Truss bridge. The bridge features short vertical beams which are able to sustain load by depending on diagonal beams for support. This design is referred to as a Pratt Truss Bridge. The truss elements are connected via pins rather than rivets, so a further classification can be added to the bridge type - Pin-Connected. The steel elements support the load from above, and there is no top connection between the left and right sides of the bridge - a design style referred to as a "Pony". The end posts of the bridge are angled such that they extend approximately a half-panel in width, referred to as a "half-hip" design. Thus the full typing of the Buffalo Creek Bridge can be expressed as a "Half-Hip Pin-Connected Pony Pratt Truss Bridge".

The major components of the sub-structure are original, and are easily viewed from the shore adjacent to the abutments. The abutments are the original limestone block. The floor beams are the original twin steel I-beams. The bottom 6 laterals are the steel originals. The 13 stringers are also all original.

The major components of the top structure are also original, and are easily viewed from the pedestrian portion of the bridge. The 4 steel intermediate posts are original. The 8 steel counters and diagonals are original. The 2 steel top cords and 4 end posts are original. The steel pins connecting the chords and posts are original. The pedestrian walkway steel elements are also original.

Over the years there have been a few repairs and supplementation to the original materials. The abutment ledge on one side was reinforced with concrete. Many of the original rivets in the steel have been replaced with bolts. The wooden bridge decking has

Buffalo Creek Bridge
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been replaced several times. Angle iron has been added to function as supplementary floor beams. The wooden covering has been replaced completely on at least one occasion.

Historic Integrity

The original foundation and steel structure of the bridge maintains its original function and appearance. While the cover is not historic, it does not detract or hide the bridge's historic elements, which are clearly visible to drivers and pedestrians using the bridge.

Returned

Buffalo Creek Bridge
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8. Statement of Significance

Applicable National Register Criteria

(Mark "x" in one or more boxes for the criteria qualifying the property for National Register listing.)

- A Property is associated with events that have made a significant contribution to the broad patterns of our history.
- B Property is associated with the lives of persons significant in our past.
- C Property embodies the distinctive characteristics of a type, period, or method of construction or represents the work of a master, possesses high artistic values, or represents a significant and distinguishable entity whose components lack individual distinction.
- D Property has yielded, or is likely to yield, information important in prehistory or history.

Areas of Significance

(Enter categories from instructions.)

Engineering

Period of Significance

1906

Significant Dates

1906

Significant Person

(Complete only if criterion B is marked above.)

n/a

Cultural Affiliation (if applicable)

n/a

Architect/Builder

Joliet Bridge & Iron Company, Joliet, Illinois

Criteria Considerations

(Mark "x" in all the boxes that apply.)

Property is:

- A Owned by a religious institution or used for religious purposes.
- B removed from its original location.
- C a birthplace or grave.
- D a cemetery.
- E a reconstructed building, object, or structure.
- F a commemorative property.
- G less than 50 years old or achieving significance within the past 50 years.

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Statement of Significance Summary Paragraph (Provide a summary paragraph that includes level of significance, applicable criteria, justification for the period of significance, and any applicable criteria considerations).

Buffalo Creek Bridge is being nominated to the National Register of Historic Places under Category C as Pony Trusses are rapidly disappearing from the Illinois landscape and this bridge maintains good integrity. The bridge was identified by the Illinois Department of Transportation as being one of only 2 remaining Steel Pratt Pony Truss Eyebar bridges in the Chicago Metro area (IDOT District 1), and one of only 33 remaining in Illinois. The majority of these remaining bridges are no longer in service, and few, if any, have an original pedestrian walkway feature. The period of significance is 1906, the year the bridge was constructed.

Narrative Statement of Significance (Provide at least one paragraph for each area of significance.)

Brief History of Long Grove

The Buffalo Creek Bridge carries Robert Parker Coffin road over Buffalo Creek. This road was originally a trail used by local Native Americans to traverse between villages located near where Barrington and Lincolnshire are today. Another trail, now known as Old McHenry Road connected villages near present day Wauconda and Des Plaines. The intersection of these two trails comprises the crossroads that anchor the Long Grove Business District.

The first bridge over Buffalo Creek in downtown Long Grove was said to be a simple wooden affair, created in the late 1840's as a "less messy" way for area people to get to the church and cemetery - riding your horse through Buffalo Creek would have been the earlier "messy" way. It was likely a simple structure, but adequate for the occasional horse and wagon. As the population in the Long Grove area increased, the crossroads became a strategic central location for goods and services - by 1900 it had become a true commerce center for area farmers, featuring two corner stores, a grocery, a hotel, a tavern, and even a town hall.

In 1902, the automobile was just starting to take hold - one automobile for every 1000 people in the US. Where does one drive their car in a horse and wagon world? Not very far on dirt roads and certainly not alone - AAA was formed in Chicago in 1902 to advocate for road improvements. Clubs were formed for people to take excursions together. Long Grove was a popular early destination for these early adopter auto enthusiasts. Perhaps in a related response, the old wooden bridge was replaced in 1906 with a "modern" steel truss bridge.

Ironically, this burgeoning mobility would be the downfall of early downtown Long Grove, and later its revival. With the advent of the automobile and tractor, Long Grove lost its strategic location advantage - area residents no longer needed local horse and buggy shops, and were able to drive farther to larger towns to do their weekly marketing and shopping. Some Long Grove shops closed up and sat empty, while others tried to adapt - the Smithy put in an auto garage and gas pump. The hotel was converted to a private residence. A chance for commercial redemption came and went in 1930, when the State routed Highways 53 and 83 around Long Grove, bypassing the central crossroads. The rationing and economics of World War II pretty much finished off what was left. By 1945, Long Grove had become a ghost town - two dirt roads crossing with shuttered buildings all around.

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When the economy improved after the war, so did the roads - Old McHenry Road was blacktopped and the corner stores were acquired by the wives of two local farmers who began selling antiques, handmade dresses, ice cream, and homemade pies. Long Grove again became an interesting and fun place to drive on a nice afternoon. Its growing popularity attracted the attention of a few other antique dealers and adjacent downtown residences were acquired and repurposed as additional shops. Recognizing that expansion could hurt the charm of the quaint crossroads, action was taken in 1962 - the Long Grove Village Board passed the "Historic Landmark Ordinance", one of the first of its kind in the country. The ordinance required new construction and remodeling to conform to the historic style already present near the crossroads.

The crossroads that anchor the Long Grove Business district (Robert Parker Coffin Road and Old McHenry Road) have contained businesses since the 1840's and a mixture of homes and businesses since at least the 1860's. That mixture continues to exist today. The business district is the most immediate feature served by one side of the bridge. On the opposite side, the Long Grove Community Church, which dates to 1847 is the most immediate feature served by the bridge. The flow of pedestrians and wheeled traffic between these two features exists today, much the same as it existed when the bridge was created in 1906. The local oral history is that there was a wooden structure that served as a bridge over Buffalo Creek prior to 1906, but no records or photographs are known to exist that document this.

Dating Buffalo Creek Bridge

The 1906 construction date of the bridge has been established using a July 1906 newspaper account of the bridge contract being awarded. The same newspaper account is used to establish the builder as Joliet Bridge and Iron Company. Additionally, a photo postcard of the bridge which is postmarked 1908 establishes that the bridge must have been constructed prior to 1908. Circumstantially, a Steel Half-Hip Pratt Pony Truss Bridge would be most commonly found to have been constructed between 1890 and 1910.

Joliet Bridge and Iron Company Background

The Joliet Bridge and Iron Company was formed in 1896. Robert C. Morrison served as President of the company. The plant was located adjacent to the Elgin, Joliet and Eastern Railroad (EJ & E) and employed about 75 workers to design and fabricate "steel bridges of every description". This rail line (the EJ & E railroad) runs through the village of Long Grove about four miles north of the Buffalo Creek Bridge, and about 50 miles distant from Joliet. Morrison died in 1913 and the company was shortly thereafter reorganized and renamed The Joliet Bridge and Construction Company.

Buffalo Creek Bridge Design

The Pratt bridge design was patented in 1844 by Thomas and Caleb Pratt, and featured vertical members in compression and diagonal members in tension. Favored for its simplicity, by 1870 it had become the standard for steel Truss design and remained so until around 1910. The Pratt Pony Truss is a variety in which the roadway passes through or between the trusses, but the Pony Trusses are small enough not to require overhead bracing to resist the force of winds. Pratt Half-Hip Trusses are a variety characterized by inclined end posts that do not extend the length of a full panel. The Pratt Half-Hip variation was popular from the 1890's onward. The integration of a pedestrian walkway is a unique feature of the Buffalo Creek Bridge design, and underscores the bridges dual function for both vehicular and pedestrian traffic.

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Illinois Context of Buffalo Creek Bridge

Illinois Department of Transportation records classify the Buffalo Creek Bridge as a "Steel Pratt Pony Truss Eyebar" bridge, and establish that it is one of two remaining in the Chicago area, and one of only 33 remaining in the state. See Figure 10 for a graphic illustrating Illinois Department of Transportation records.

The National Bridge Inventory (NBI) is a different database which compiles information on all bridges and tunnels which have roads passing above or below. Any bridge longer than 20 feet is included in this database. The NBI data provides a few additional bridge characteristics which offer an alternative objective base to assess the uniqueness of the Buffalo Creek Bridge.

Unique aspects of the Buffalo Creek Bridge that can be discerned using the NBI data are as follows:

- features a steel truss thru design
- over a century old (built before 1917 - the Buffalo Creek Bridge was built in 1906)
- carries a single lane of traffic
- carries a pedestrian walkway

The most characteristic geographic area relevant to the Buffalo Creek Bridge is the Chicago Metropolitan Area. This US Census Bureau defines this area as being comprised of the Illinois Counties of Cook, DuPage, Kane, Lake and Will, along with Lake County in Indiana. It is the third largest Metropolitan Area in the country. It is also interesting to look at uniqueness at a broader and narrower level. The NBI analysis following looked at these geographic areas:

- the United States
- Illinois
- the Chicago Metropolitan area (including Lake County, Indiana)
- Lake County, Illinois

The detailed analysis below shows the Buffalo Creek Bridge to be the only century old single lane steel thru truss bridge remaining in the Chicago Metropolitan area, and one of only 80 remaining in Illinois. The Buffalo Creek Bridge's incorporation of pedestrian access is one of only two left in Illinois and one of only 29 remaining in the United States.

	United States	Illinois	Chicago Metro	Lake County IL
Structures enumerated in NBI	725,589	30,827	5,699	342
- subset featuring a steel truss thru design	8447	209	24	1
- further subset known to be at least a century old	2069	85	2	1
- further subset which carries a single lane of traffic	1524	80	1	1
- further subset which incorporate a pedestrian walkway	229	2	1	1

For reference, the other Illinois bridge with an integrated walkway is the Third Street Bridge in Delevan, Illinois which was placed on the National Register in 1999.

It should be noted that the data contained in the NBI is not 100% accurate. For example, regarding the subject bridge, Buffalo Creek Bridge, the NBI database has the build date incorrectly listed as 1925. Additionally, a sampling of the data shows it to contain bridges which have been destroyed or replaced, yet still carry the original vintage build date. Regardless, the occasional inaccuracy does not materially affect the overall

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conclusion that bridges of this type are increasingly rare.

The NBI analysis was done using the 2016 edition of the database. The steel truss thru design subset was determined using TRAFFIC_LANES_ON_28A values greater than 0 to select vehicular bridges, STRUCTURE_TYPE_043B values of 10 denoting a thru truss design, and STRUCTURE_KIND_043A values of 3 or 4 denoting steel as the construction material. The century old subset was determined by using YEAR_BUILT_027 values less than 1917. The single lane of traffic subset was determined by using TRAFFIC_LANES_ON_28A values of 1. The pedestrian walkway subset was determined by using LEFT_CURB_MT_050A or RIGHT_CURB_MT_50B values greater than 0.

Returned

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9. Major Bibliographical References

Bibliography (Cite the books, articles, and other sources used in preparing this form.)

Long Grove Lore and Legend, 1978, Virginia L. Park

Real Photo Postcard Guide, Robert Bogdan & Todd Weseloh, 2006, Syracuse University Press

U.S. Department of Transportation, National Bridge Inventory, www.fhwa.dot.gov/bridge/nbi.cfm

State of Tennessee Survey Report For Historic Highway Bridges, www.tn.gov/tdot/article/historic-bridgebook

Maryland State Highway Association, "The Pratt Truss", www.roads.maryland.gov/OPPEN/V-Pratt.pdf

National Register of Historic Places Registration Form For SD DOT Br. No. 34-120-94, NRIS ID# 02000579

National Register of Historic Places Registration Form For Northwood Bridge, NRIS ID# 97000175

National Register of Historic Places Registration Form For Spencer's Crossing Bridge, NRIS ID# 89002177

Historical American Engineering Record, National Park Service, Island Slough Bridge, HAER No. AR-82

National Register of Historic Places Registration Form For Bolivia Road Bridge, NRIS ID# 03001464

Long Grove Living Magazine, September 2013, Historic Long Grove Walking Tour

Structural Engineers' Handbook, Milo Ketchum, Data for the Design and Construction of Steel Bridges and Buildings

Previous documentation on file (NPS):

- preliminary determination of individual listing (36 CFR 67 has been requested)
- previously listed in the National Register
- previously determined eligible by the National Register
- designated a National Historic Landmark
- recorded by Historic American Buildings Survey # _____
- recorded by Historic American Engineering Record # _____
- recorded by Historic American Landscape Survey # _____

Primary location of additional data:

- State Historic Preservation Office
 - Other State agency
 - Federal agency
 - Local government
 - University
 - Other
- Name of repository: _____

Historic Resources Survey Number (if assigned): _____

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10. Geographical Data

Acreage of Property Less than one

(Do not include previously listed resource acreage; enter "Less than one" if the acreage is .99 or less)

Latitude/Longitude Coordinates

Datum if other than WGS84: _____
(enter coordinates to 6 decimal places)

1	<u>42.1775</u>	<u>-87.9998</u>	3	_____	_____
	Latitude	Longitude		Latitude	Longitude
2	_____	_____	4	_____	_____
	Latitude	Longitude		Latitude	Longitude

Verbal Boundary Description (Describe the boundaries of the property.)

The Buffalo Creek Bridge is located on Robert Parker Coffin Road in Long Grove, Illinois, 500 feet southwest of downtown Long Grove as defined by the intersection of Robert Parker Coffin Road and Old McHenry Road. The UTM reference is Zone 16, E417429, N4667867. The road is a Village of Long Grove road which runs east - west and connects Illinois Route 53 with Illinois Route 83. The Illinois Route 53 intersection is in on the far south-eastern edge of Lake County's Ela Township (Section 25, Township 43 North, Range 10 East of the Principle Meridian). The Illinois Route 83 intersection is in the far south-western corner of Lake County's Vernon Township (Section 30, Township 43 North, Range 11 East of the 3rd Principle Meridian). The bridge is located in Vernon Township. The north and south boundary is defined by the physical bridge structure. The eastern and western boundary is defined by the point at which the abutment meets the road bed.

Boundary Justification (Explain why the boundaries were selected.)

The boundary includes only that portion of land which directly relates to the Buffalo Creek Bridge (its structural members and its approaches) including the abutments.

11. Form Prepared By

name/title Roy Underwood, President / Ryan Messner, President date 2/13/2017
organization Long Grove Historical Society / Historic Downtown
Long Grove Business Association telephone (847) 478-5140
street & number 338 Old McHenry Road email aaron@uinnovate.net
city or town Long Grove state IL zip code 60047

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Additional Documentation

Submit the following items with the completed form:

- **GIS Location Map (Google Earth or BING)**
- **Local Location Map**
- **Site Plan**
- **Floor Plans (As Applicable)**
- **Photo Location Map** (Include for historic districts and properties having large acreage or numerous resources. Key all photographs to this map and insert immediately after the photo log and before the list of figures).

Returned

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Photographs:

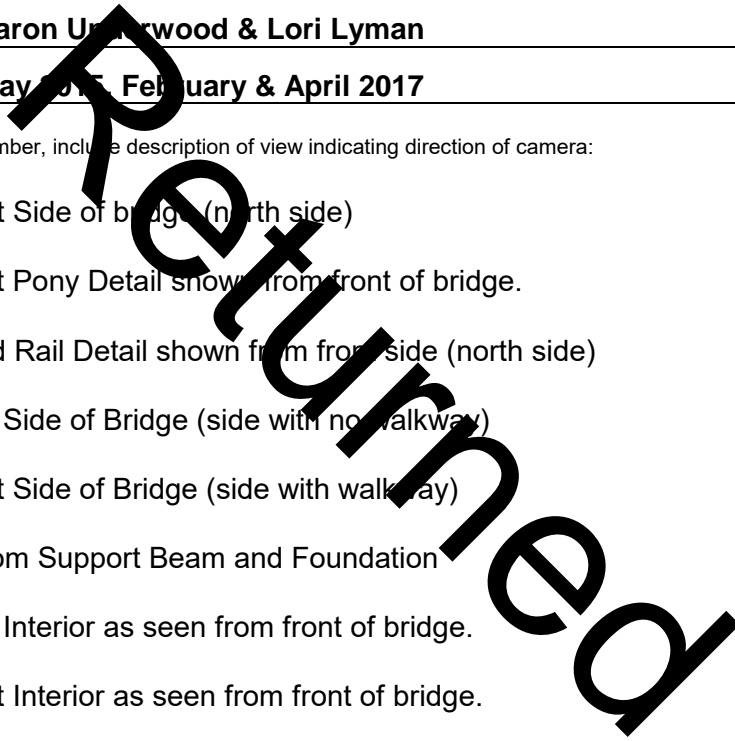
Submit clear and descriptive photographs. The size of each image must be 3000x2000 pixels, at 300 ppi (pixels per inch) or larger. Key all photographs to the sketch map. Each photograph must be numbered and that number must correspond to the photograph number on the photo log. For simplicity, the name of the photographer, photo date, etc. may be listed once on the photograph log and doesn't need to be labeled on every photograph.

Photo Log

Name of Property: Buffalo Creek Bridge
City or Vicinity: Long Grove
County: Lake **State:** Illinois
Photographer: Aaron Underwood & Lori Lyman
Date Photographed: May 2015, February & April 2017

Description of Photograph(s) and number, include description of view indicating direction of camera:

- Photo 1 of 11:** Front Side of bridge (north side)
- Photo 2 of 11:** West Pony Detail shown from front of bridge.
- Photo 3 of 11:** Hand Rail Detail shown from front side (north side)
- Photo 4 of 11:** East Side of Bridge (side with no walkway)
- Photo 5 of 11:** West Side of Bridge (side with walkway)
- Photo 6 of 11:** Bottom Support Beam and Foundation
- Photo 7 of 11:** East Interior as seen from front of bridge.
- Photo 8 of 11:** West Interior as seen from front of bridge.
- Photo 9 of 11:** Bridge Bottom
- Photo 10 of 11:** Pin Connection as viewed from pedestrian walkway
- Photo 11 of 11:** South Side of Bridge



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Paperwork Reduction Act Statement: This information is being collected for applications to the National Register of Historic Places to nominate properties for listing or determine eligibility for listing, to list properties, and to amend existing listings. Response to this request is required to obtain a benefit in accordance with the National Historic Preservation Act, as amended (16 U.S.C.460 et seq.).

Estimated Burden Statement: Public reporting burden for this form is estimated to average 100 hours per response including time for reviewing instructions, gathering and maintaining data, and completing and reviewing the form. Direct comments regarding this burden estimate or any aspect of this form to the Office of Planning and Performance Management, U.S. Dept. of the Interior, 1849 C. Street, NW, Washington, DC.

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National Park Service

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List of photos

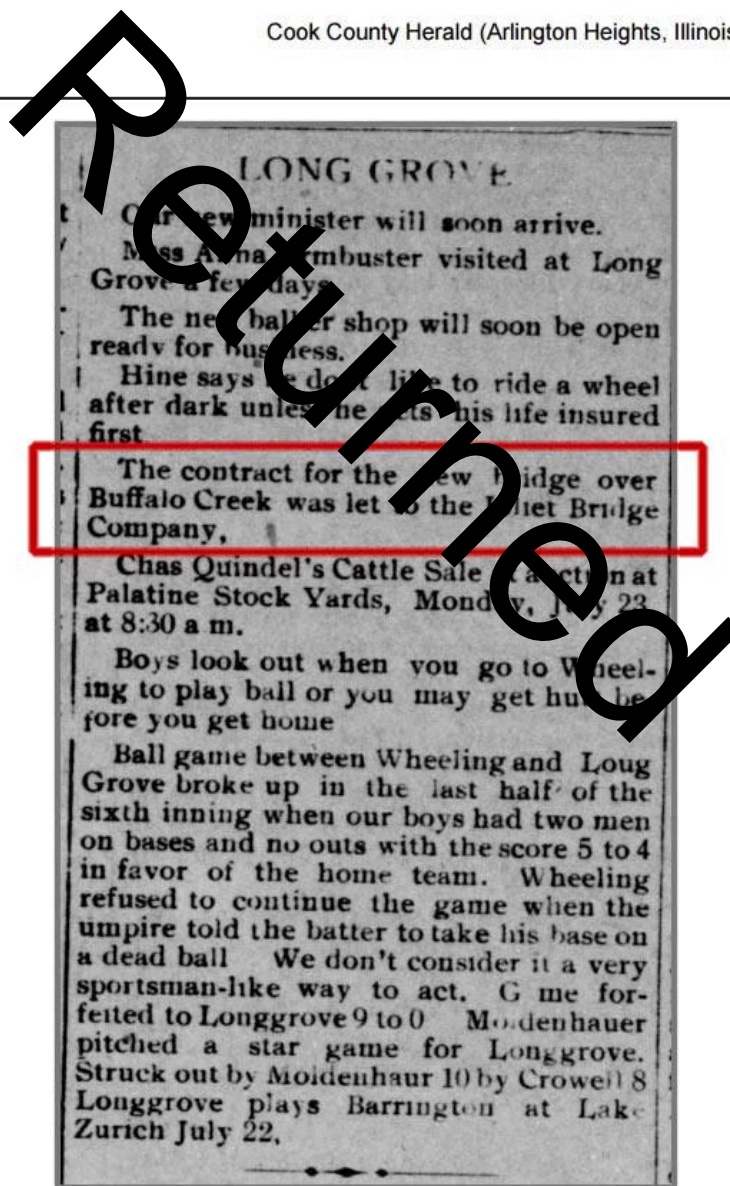
(Resize, compact, and paste images of maps and historic documents in this section. Place captions, with figure numbers above each image. Orient maps so that north is at the top of the page, all document should be inserted with the top toward the top of the page.)

Figure 1: 1906 Newspaper Article establishing bridge to be no older than July of 1906. It is our assertion that the bridge was built in the late summer or fall of 1906.

Newspapers.com

Cook County Herald (Arlington Heights, Illinois) · Thu, Jul 19, 1906 · Page 5

Downloaded on Mar 25, 2016



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National Park Service

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Figure 2: 1908 Photo Post Card Photograph of Bridge establishing the bridge to have been built prior to July 1908.



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National Park Service

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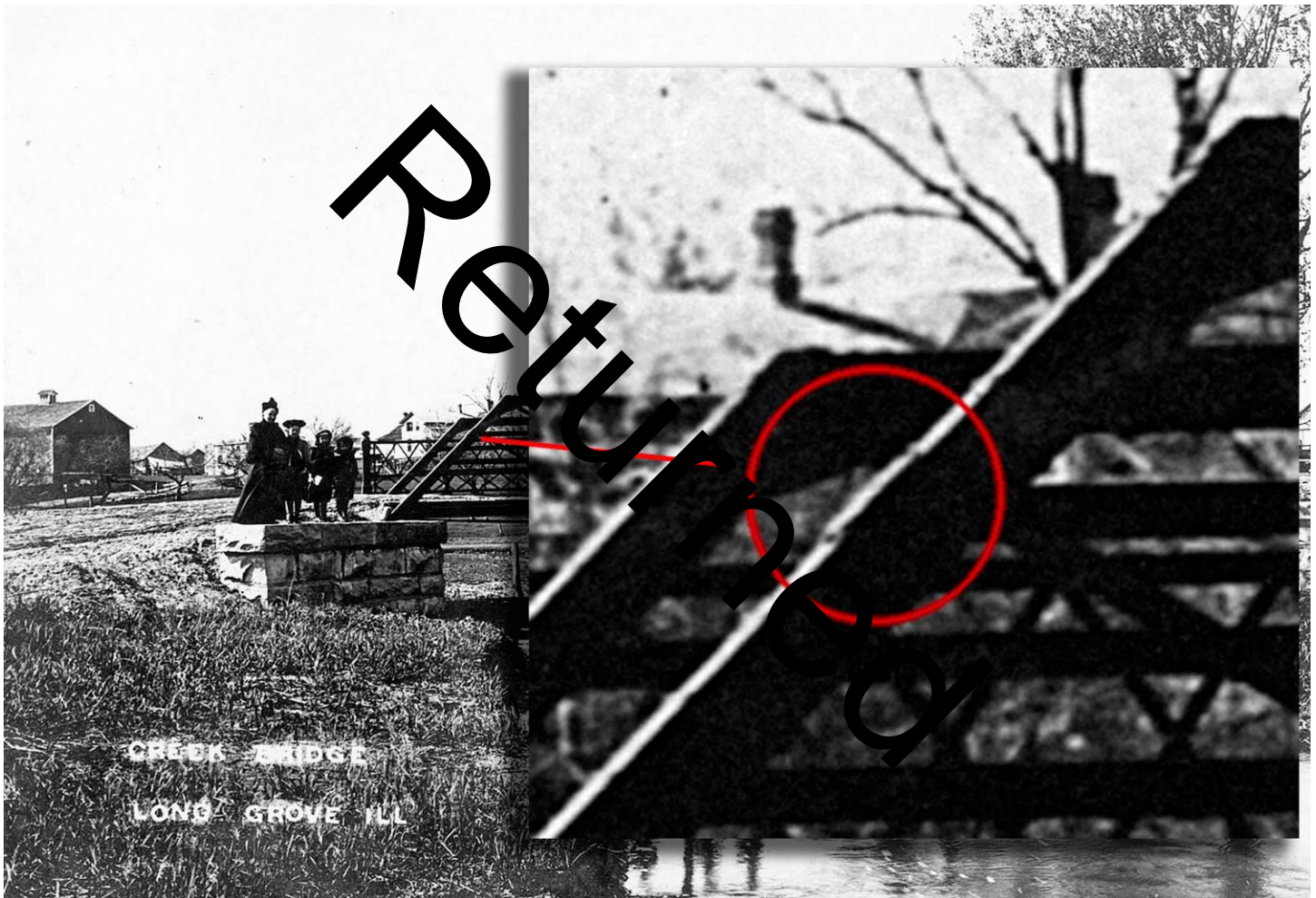
Name of multiple listing (if applicable)

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Figure 3: 1908 Photo Post Card Bridge Plate – We believe the bridge plate (which would contain bridge date and manufacturer) can be seen in this picture. The plate appears to have been lost or removed many years ago, as no evidence of it exists today.



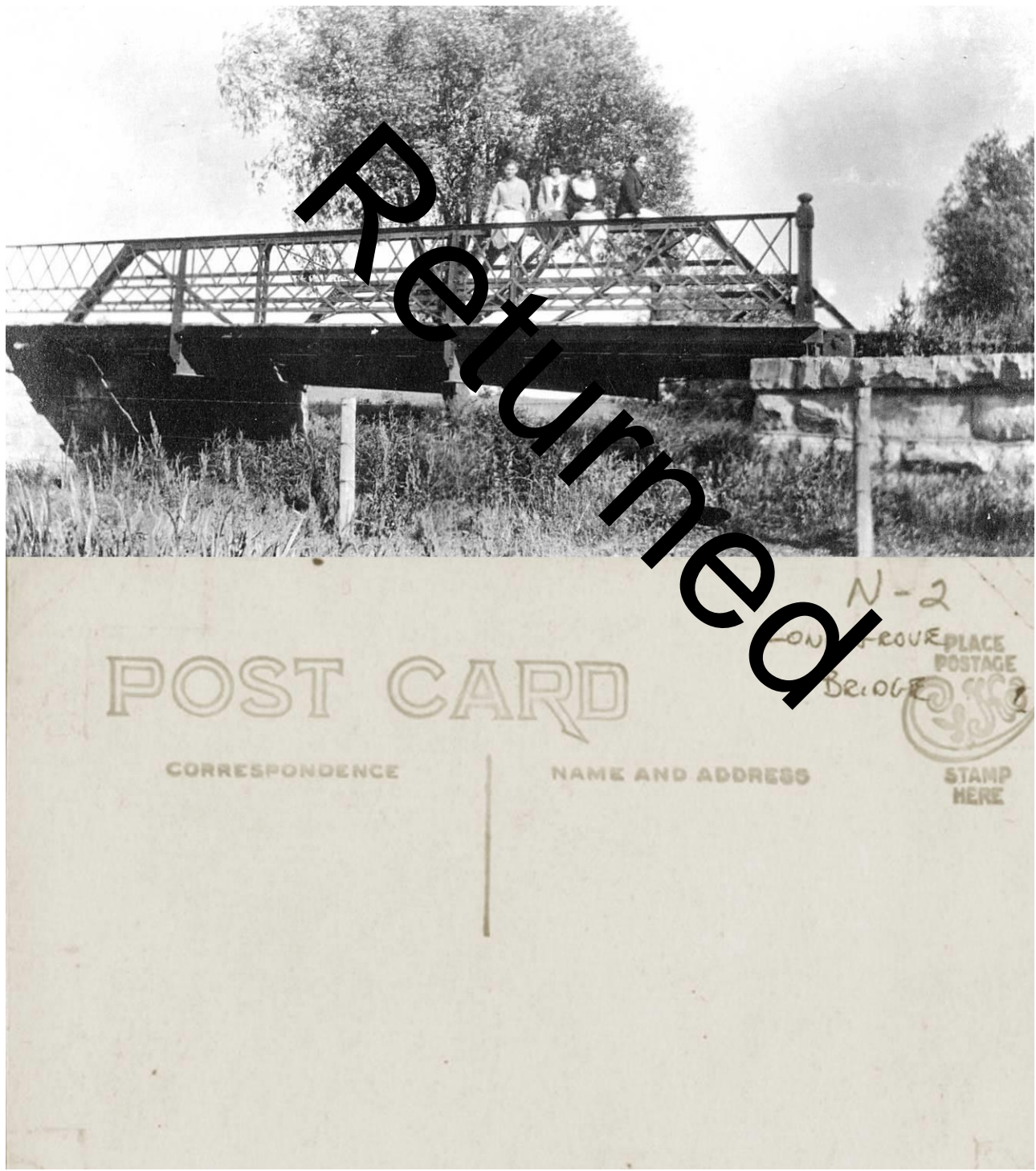
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Figure 4: Circa 1912 Photo Postcard (This Cyko postcard stock was in circulation between 1910 and 1915. Latest known postmark of this stock is 1936.



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Figure 5: Picture of unknown date showing pedestrian use of the bridge.



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Figure 6: Current Topographic Map



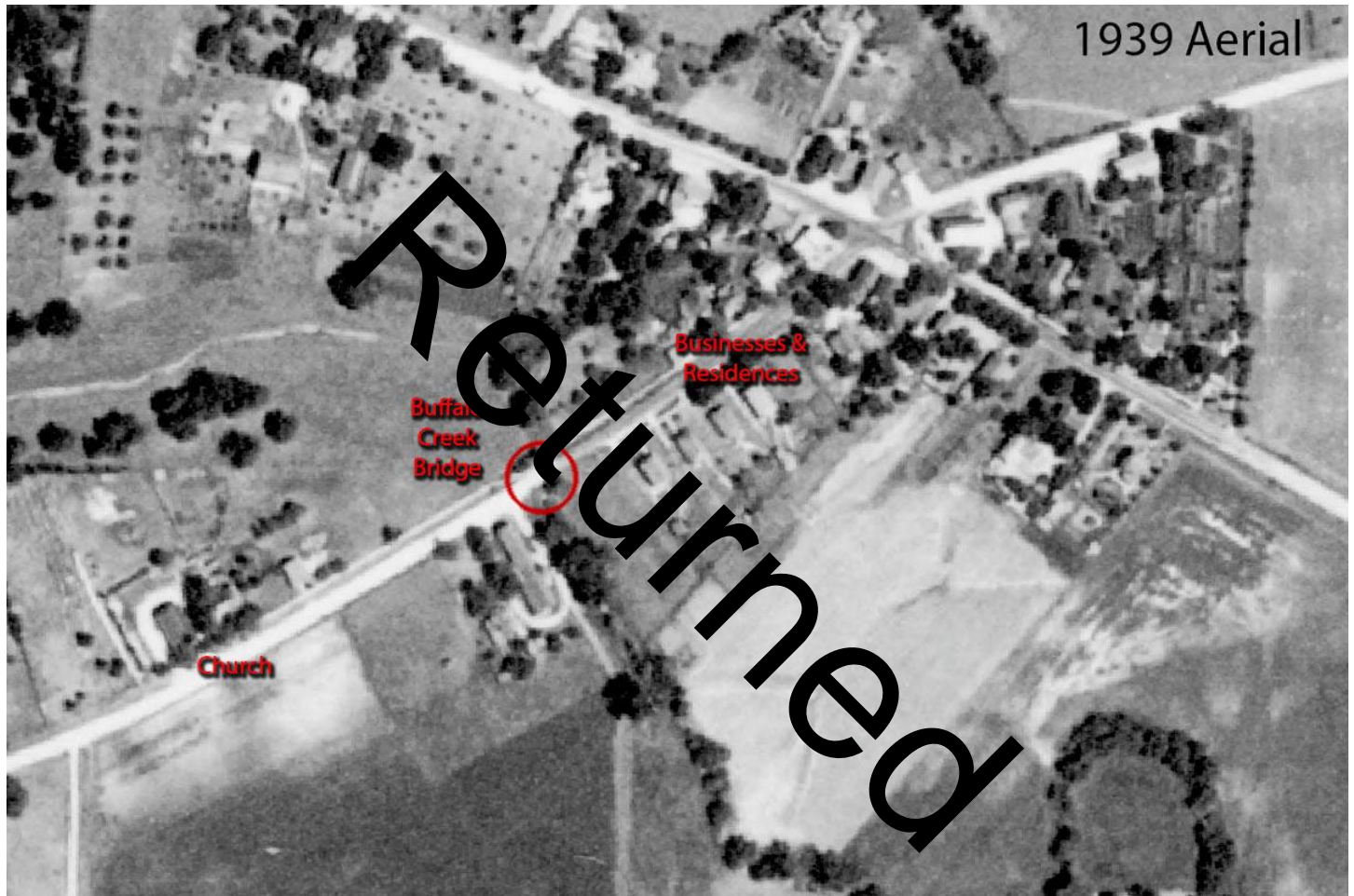
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Figure 7: 1939 Aerial



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Figure 8: 2015 Aerial



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Figure 9: Google GIS Map

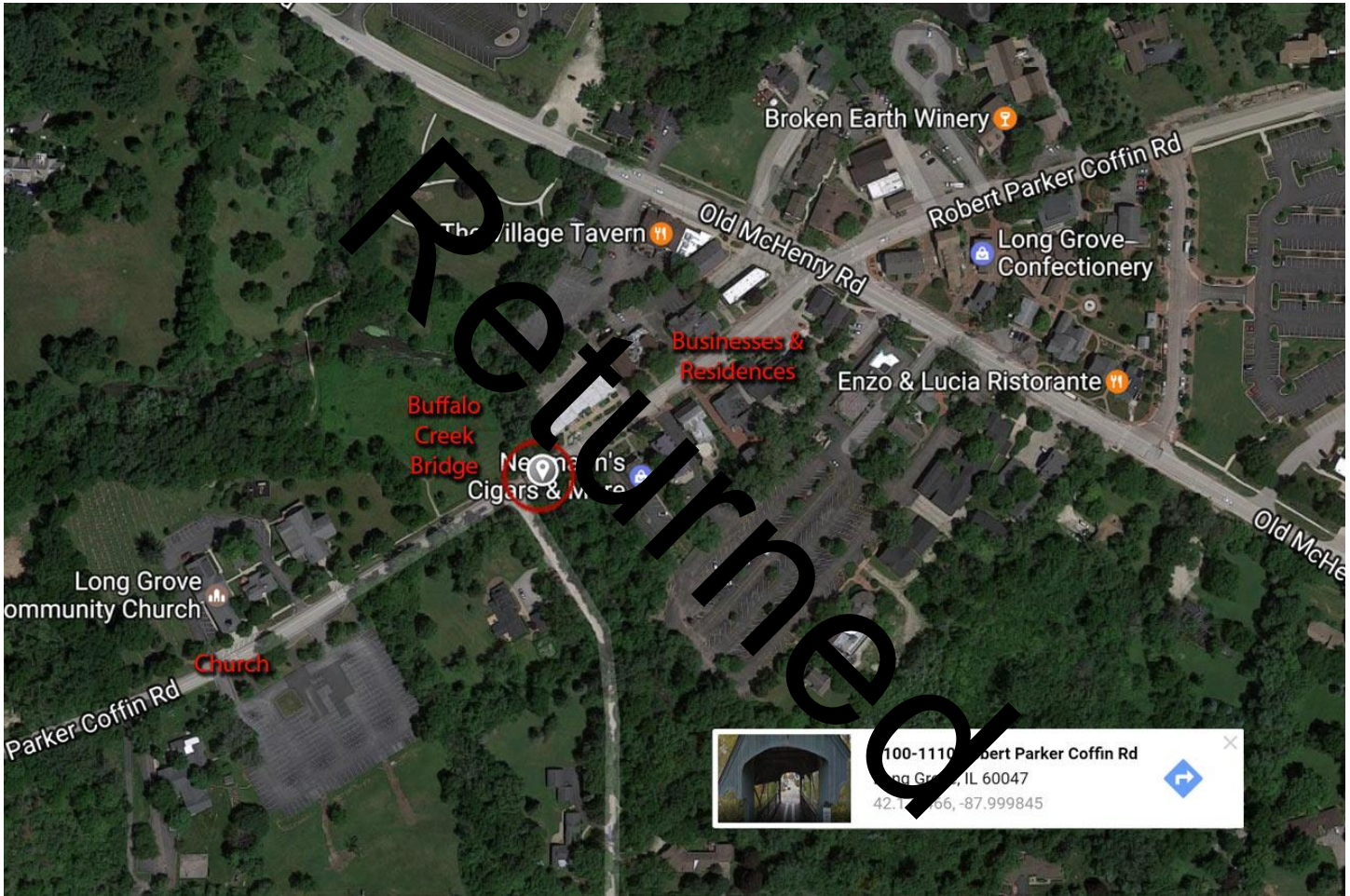


Figure 10: Illinois Department of Transportation Records

Area	Remaining Steel Pratt Pony Truss Eyebar Bridges
IDOT District 1 (green)	2
IDOT District 3 (blue)	3
Statewide	33

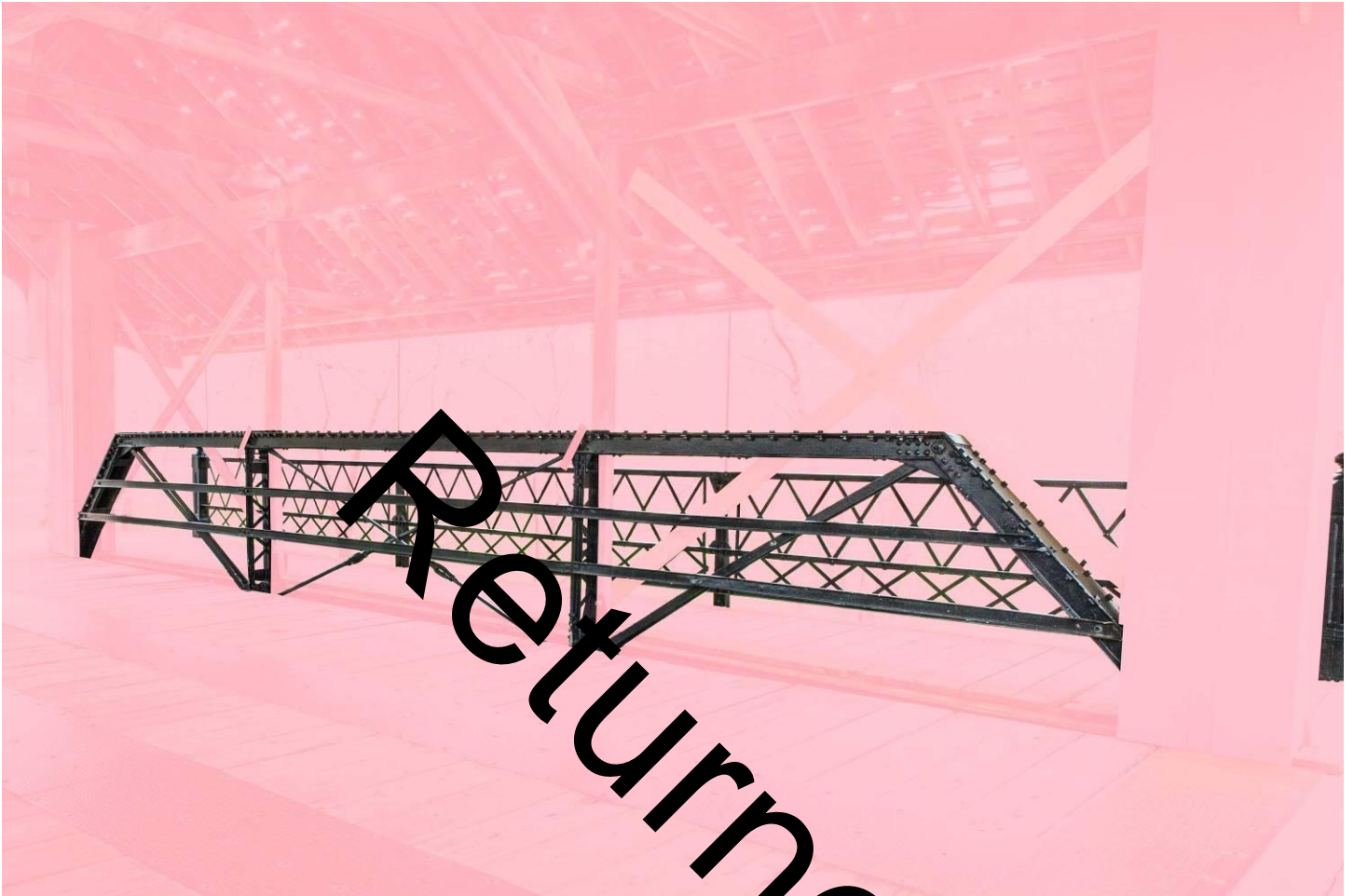


(subject bridge marked with a star)

Figure 11: East Interior (non-historic elements shown in red) as seen from front of bridge.



Figure 12: West Interior (non-historic elements in red) as seen from front of bridge.



UNITED STATES DEPARTMENT OF THE INTERIOR
NATIONAL PARK SERVICE

NATIONAL REGISTER OF HISTORIC PLACES
EVALUATION/RETURN SHEET

Requested Action:

Property Name:

Multiple Name:

State & County:

Date Received: 8/14/2017 Date of Pending List: 9/19/2017 Date of 16th Day: 10/4/2017 Date of 45th Day: 9/28/2017 Date of Weekly List:

Reference number:

Nominator:

Reason For Review:

- | | | |
|---|--|---|
| <input type="checkbox"/> Appeal | <input type="checkbox"/> PDIL | <input type="checkbox"/> Text/Data Issue |
| <input type="checkbox"/> SHPO Request | <input type="checkbox"/> Landscape | <input type="checkbox"/> Photo |
| <input type="checkbox"/> Waiver | <input type="checkbox"/> National | <input type="checkbox"/> Map/Boundary |
| <input type="checkbox"/> Resubmission | <input type="checkbox"/> Mobile Resource | <input type="checkbox"/> Period |
| <input checked="" type="checkbox"/> Other | <input type="checkbox"/> TCP | <input type="checkbox"/> Less than 50 years |
| | <input type="checkbox"/> CLG | |

Accept Return Reject 9/28/2017 Date

Abstract/Summary Comments:

Recommendation/ Criteria

Reviewer Barbara Wyatt Discipline Historian

Telephone (202)354-2252 Date _____

DOCUMENTATION: see attached comments : No see attached SLR : No

If a nomination is returned to the nomination authority, the nomination is no longer under consideration by the National Park Service.

**United States Department of the Interior
National Park Service
National Register of Historic Places**

**Comments
Evaluation/Return Sheet**

Property Name: Buffalo Creek Bridge
Property Location: Long Grove, Lake County, Illinois
Reference Number: 16000897
Date of Return: 10-2-17

Reason for Return

This nomination is being returned for the additional information described below.

The Buffalo Creek Bridge was nominated at the state level of significance as an excellent example of a pony truss bridge in Illinois. The nomination classifies it as "a rare surviving example of a pin-connected truss bridge built for an urban setting." The incidence of such bridges in the state is well presented by the IDOT-prepared nomination, but the integrity of the nominated bridge, especially compared to other examples, is not well explained.

At issue is the gable roof that has sheltered the bridge since 1972, well after the 1906 period of significance. The bridge is in Long Grove, Illinois, a historic community with a long history of preservation activity. The nomination states the following about the roof:

A low height nostalgic covering was added in 1972 as a life extension measure. The function of the covering was to limit truck traffic and heavy vehicles and shield the steel from the elements.

The covering is composed of common dimensional lumber. The roof portion is supported by wooden beams that are attached with a steel bracket to the bridge's vertical steel beams. The roof on the covering is wood shake. On the east side (opposite side of the walkway), a short panel constructed of wooden slats shields the steel infrastructure from the weather.

Please resubmit this nomination with a better explanation of how this roof affects the overall integrity of the bridge. If possible, include information about the prevalence of the post-construction covered truss bridges in Illinois. If this is a common practice for the reasons stated in the nomination, this could be relevant to the explanation.

Another issue is the state level of significance, given the modification of the roof. Could this be more reasonably nominated at the local level? Please better justify the state level of significance.

Please call me at 202-354-2252 or send an email to barbara_wyatt@nps.gov if you have any questions.

A handwritten signature in black ink that reads "Barbara Wyatt". The signature is written in a cursive style with a long horizontal flourish extending to the right.

Barbara Wyatt
National Register of Historic Places



Illinois Department of Natural Resources

One Natural Resources Way Springfield, Illinois 62702-1271
www.dnr.illinois.gov

Bruce Rauner, Governor

Wayne A. Rosenthal, Director

April 19, 2018

Ms. Barbara Wyatt
National Park Service
National Register of Historic Places
1849 C Street, NW, Mail Stop 7228
Washington, DC 20240

Dear Ms. Wyatt:

Enclosed are the disks that contain the true and correct copies of the National Register nominations recommended for nomination by the Illinois Historic Sites Advisory Council and signed by the Deputy State Historic Preservation Officer:

- **SLR for the Emmanuel Episcopal Church, LaGrange, Cook County**
- **Requested corrections for the Kankakee Downtown Historic District (including confirmation of federal notification)**
- **Requested corrections for the Buffalo Creek Bridge, Long Grove, Lake County**

Please contact me at 217/785-4324 if you need any additional information. Thank you for your attention to this matter.

Sincerely,

Andrew Heckenkamp, Coordinator
Survey and National Register program
Illinois State Historic Preservation Office
Illinois Department of Natural Resources

enclosures